

1212-16 N. DELAWARE AVE

CIVIC DESIGN REVIEW
GNOME ARCHITECTS
12.05.2023



PROJECT DESCRIPTION:

1212 N. Delaware Ave. is a 7 story mixed-use building which is composed of 62 residential units and one ground floor commercial space. The site is located directly across from Penn Treaty Park along Delaware Ave, and has been designed by-right to meet CMX-3 zoning requirements, and is in full compliance with all applicable overlays.

As part of the zoning requirements, off site parking has been secured at 950 Marlborough St and has been submitted to L&I under \ZP-2023-009819. In an effort to consider feedback from the RCO and PCPC as part of our pre-CDR meeting, the residential unit count has been reduced from the originally proposed 63 units to 62 units, making room for an expanded commercial space to have frontage on multiple streets and have access to the public plaza. 31 additional bicycle parking stalls have also been added to the required 21 required bicycle parking stalls based on preliminary feedback.

Owner

Philly Capital Group
Philadelphia, PA 19107

Architect

Gnome Architects LLC
1901 S. 9th st Rm 310
Philadelphia, PA 19148

Civil Engineer

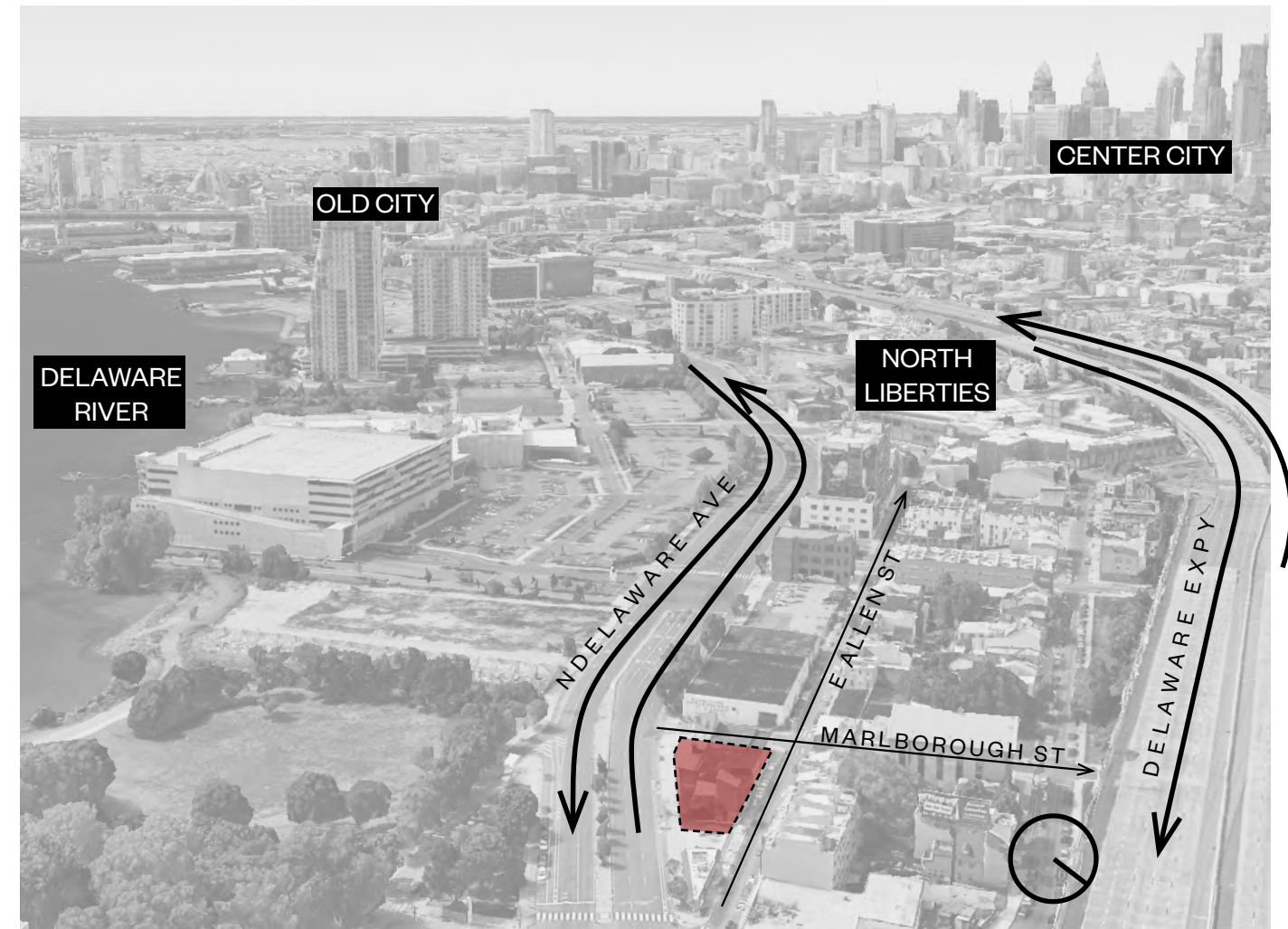
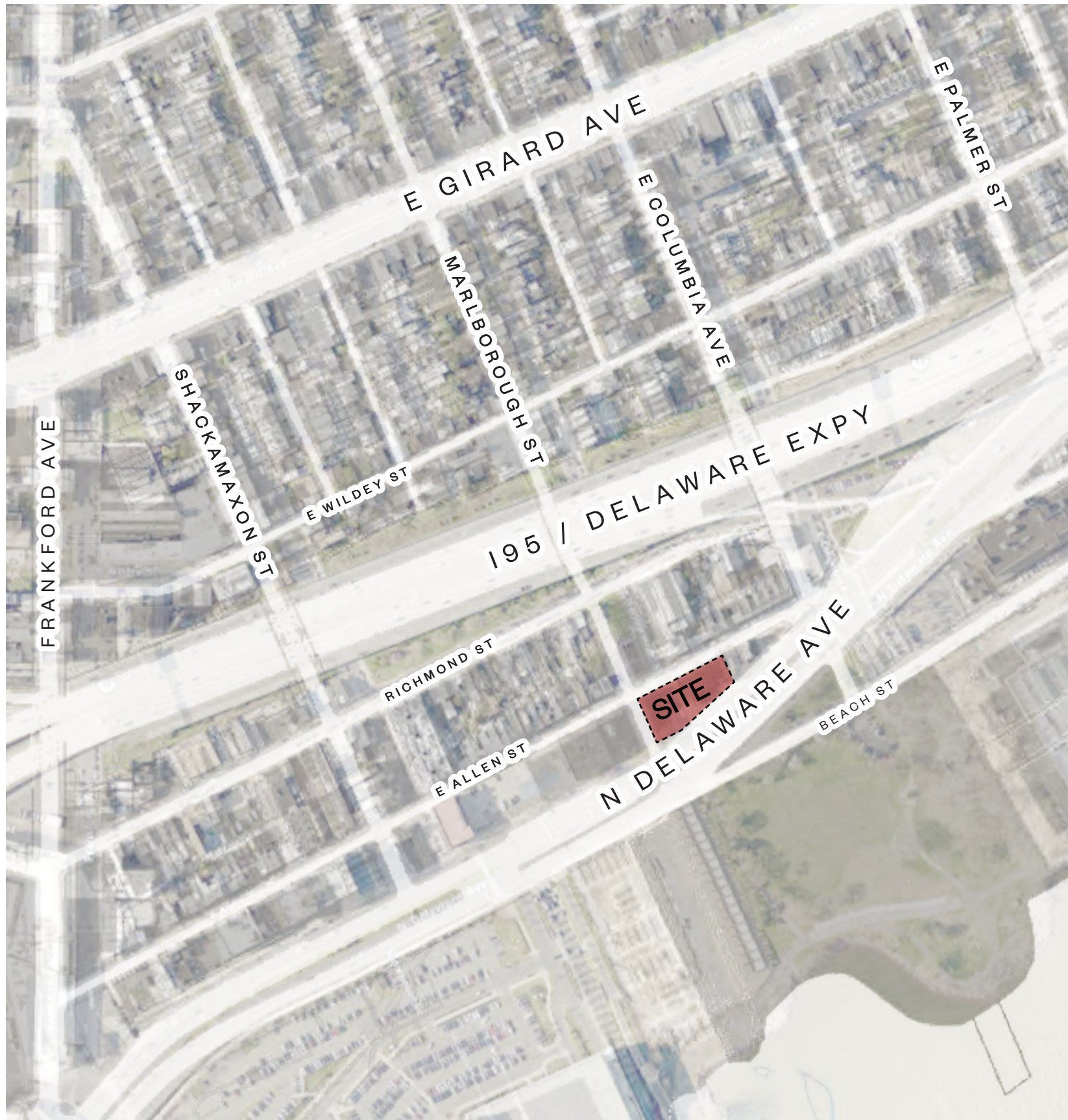
Ruggiero Plante Land Design
5900 Ridge Avenue
Philadelphia, PA 19128

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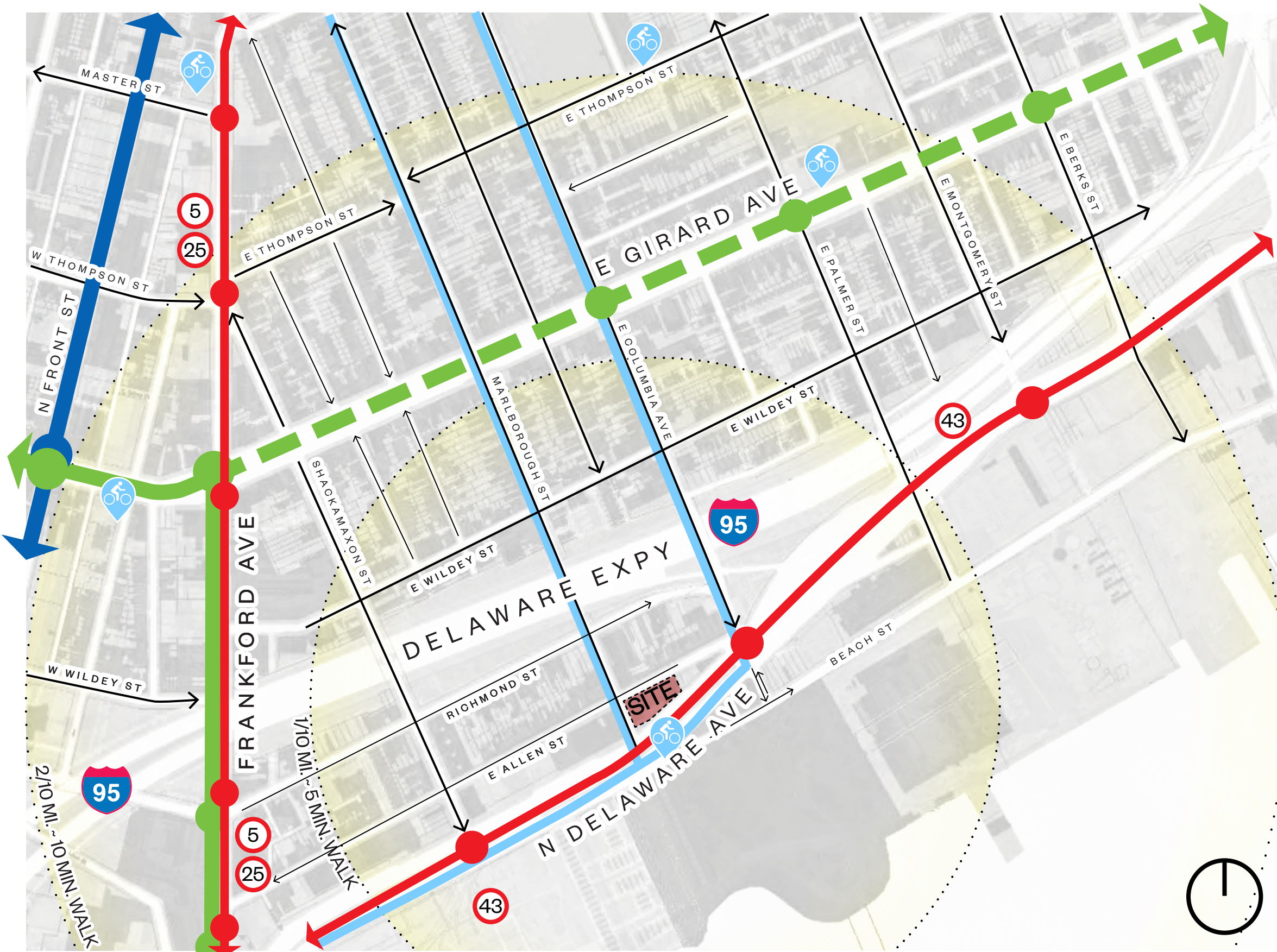
STREET PHOTOS






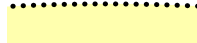


STREET PHOTOS



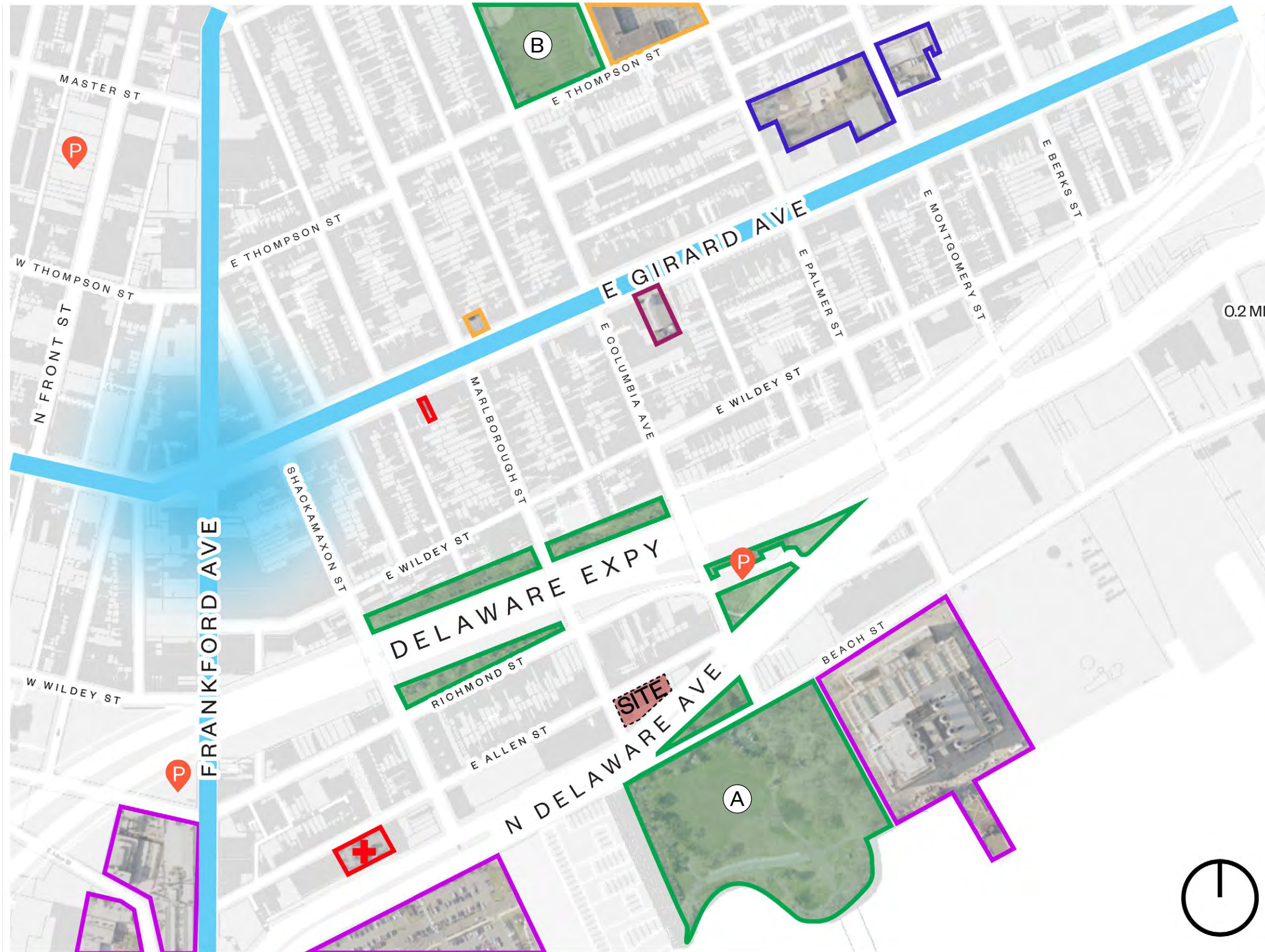
- RM-1, RM-2
- RSA-5
- CMX-2, CMX-2.5
- CMX-3
- ICMX
- SP-PO-A



-  BIKE ROUTE & SHARE LOCATION
-  I95 HIGHWAY
-  SEPTA TROLLEY LINES & STOPS
-  SEPTA MFL LINES & STOPS
-  SEPTA BUS ROUTE & STOPS
-  WALKING RADIUS



TRANSIT MAP



A - PENN TREATY PARK

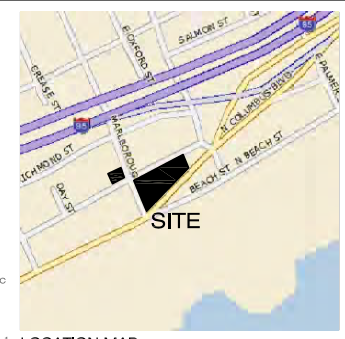
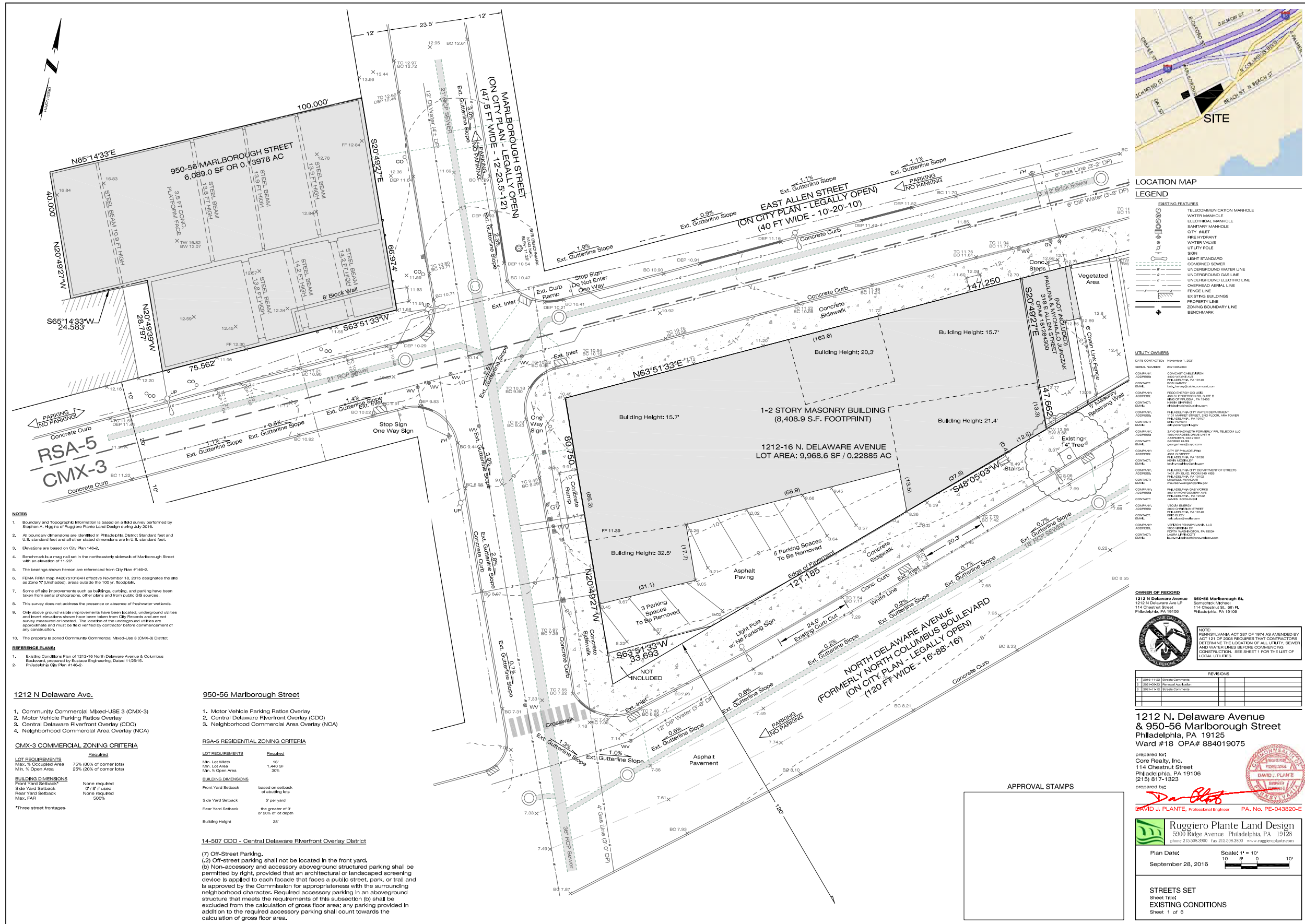


B - HETZELL PLAYGROUND

-  PUBLIC PARKING
-  ENTERTAINING / MIXED USE
-  COMMUNITY ORGANIZATIONS
-  SCHOOLS
-  RELIGIOUS INSTITUTIONS
-  PARKS / GREENSPACE
-  MEDICAL
-  COMMERCIAL CORRIDOR
-  WALKING RADIUS



AMENITIES MAP



LEGEND

EXISTING FEATURES

- TELECOMMUNICATION MANHOLE
- WATER MANHOLE
- ELECTRICAL MANHOLE
- SANITARY MANHOLE
- CITY INLET
- FIRE HYDRANT
- WATER VALVE
- UTILITY POLE
- SEWER
- LIGHT STANDARD
- COMBINED SEWER
- UNDERGROUND WATER LINE
- UNDERGROUND GAS LINE
- UNDERGROUND ELECTRIC LINE
- OVERHEAD AERIAL LINE
- FENCE LINE
- EXISTING BUILDINGS
- PROPERTY LINE
- ZONING BOUNDARY LINE
- BENCHMARK

UTILITY OWNERS

DATE CONTRACTED: November 1, 2021

SEWER: CONQUEST ENGINEERING
 ADDRESS: PHILADELPHIA, PA 19104
 CONTACT: [Redacted]
 EMAIL: [Redacted]

COMBINED: [Redacted]
 ADDRESS: [Redacted]
 CONTACT: [Redacted]
 EMAIL: [Redacted]

GAS: [Redacted]
 ADDRESS: [Redacted]
 CONTACT: [Redacted]
 EMAIL: [Redacted]

ELECTRIC: [Redacted]
 ADDRESS: [Redacted]
 CONTACT: [Redacted]
 EMAIL: [Redacted]

TELECOM: [Redacted]
 ADDRESS: [Redacted]
 CONTACT: [Redacted]
 EMAIL: [Redacted]

OWNER OF RECORD

1212 N Delaware Avenue
 1212 N Delaware Ave LP
 114 Chestnut Street
 Philadelphia, PA 19106

950-56 Marlborough St
 Superior Market
 114 Chestnut St, 6th Fl
 Philadelphia, PA 19106

NOTE: PENNSYLVANIA ACT 287 OF 1974 AS AMENDED BY ACT 51 OF 2008 REQUIRES THAT CONTRACTORS DETERMINE THE LOCATION OF ALL UTILITY, SEWER AND WATER LINES BEFORE COMMENCING CONSTRUCTION. SEE SHEET 1 FOR THE LIST OF LOCAL UTILITIES.

REVISIONS

NO.	DATE	DESCRIPTION
1	2021-11-01	Issue Comments
2	2021-11-01	Revised App Number
3	2021-11-01	Issue Comments

1212 N. Delaware Avenue & 950-56 Marlborough Street
 Philadelphia, PA 19125
 Ward #18 OPA# 884019075

prepared for:
 Core Realty, Inc.
 114 Chestnut Street
 Philadelphia, PA 19106
 (215) 617-1323
 prepared by:
 DAVID J. PLANTE, Professional Engineer PA, No. PE-043820-E

Ruggiero Plante Land Design
 5900 Ridge Avenue Philadelphia, PA 19128
 phone 215-208-2000 fax 215-208-2600 www.ruggieroplanteland.com

Plan Date: September 28, 2016
 Scale: 1" = 10'
 10' 0" 0' 10'

STREETS SET
 Sheet Title:
 EXISTING CONDITIONS
 Sheet 1 of 6

- NOTES**
- Boundary and Topographic Information is based on a field survey performed by Stephen A. Hoggis of Ruggiero Plante Land Design during July, 2016.
 - All boundary dimensions are identified in Philadelphia District Standard feet and U.S. standard feet and all other stated dimensions are in U.S. standard feet.
 - Elevations are based on City Plan 148-2.
 - Benchmark is a mag nail set in the northeastern sidewalk of Marlborough Street with an elevation of 11.267.
 - The bearings shown herein are referenced from City Plan #148-2.
 - ES&M #9941 map #A0207201 dated November 18, 2015 designates the site as Zone 7 (unshaded), areas outside the 100 y. floodplain.
 - Some of the site improvements such as buildings, curbing, and zoning have been taken from aerial photographs, other plans and from public GIS sources.
 - This survey does not address the presence or absence of freshwater wetlands.
 - Only above ground (H&H) improvements have been located, underground utilities and invert elevations shown have been taken from City Records and are not survey measured or located. The location of the underground utilities are approximate and must be field verified by contractor before commencement of any construction.
 - The property is zoned Community Commercial Mixed-Use 3 (CMX-3) District.
- REFERENCE PLANS**
- Existing Conditions Plan of 1212-16 North Delaware Avenue & Columbus Boulevard, prepared by Eustace Engineering, Dated 11/25/16.
 - Philadelphia City Plan #148-2.

1212 N Delaware Ave.

- Community Commercial Mixed-USE 3 (CMX-3)
- Motor Vehicle Parking Ratios Overlay
- Central Delaware Riverfront Overlay (CDO)
- Neighborhood Commercial Area Overlay (NCA)

CMX-3 COMMERCIAL ZONING CRITERIA

LOT REQUIREMENTS	Required
Min. % Open Area	75% (80% of corner lots)
Min. % Open Area	25% (20% of corner lots)

BUILDING DIMENSIONS

Front Yard Setback	None required
Side Yard Setback	0' / If used
Rear Yard Setback	None required
Max. FAR	500%

*Three street frontages.

950-56 Marlborough Street

- Motor Vehicle Parking Ratios Overlay
- Central Delaware Riverfront Overlay (CDO)
- Neighborhood Commercial Area Overlay (NCA)

RSA-5 RESIDENTIAL ZONING CRITERIA

LOT REQUIREMENTS	Required
Min. Lot Area	18'
Min. Lot Area	1,440 SF
Min. % Open Area	30%

BUILDING DIMENSIONS

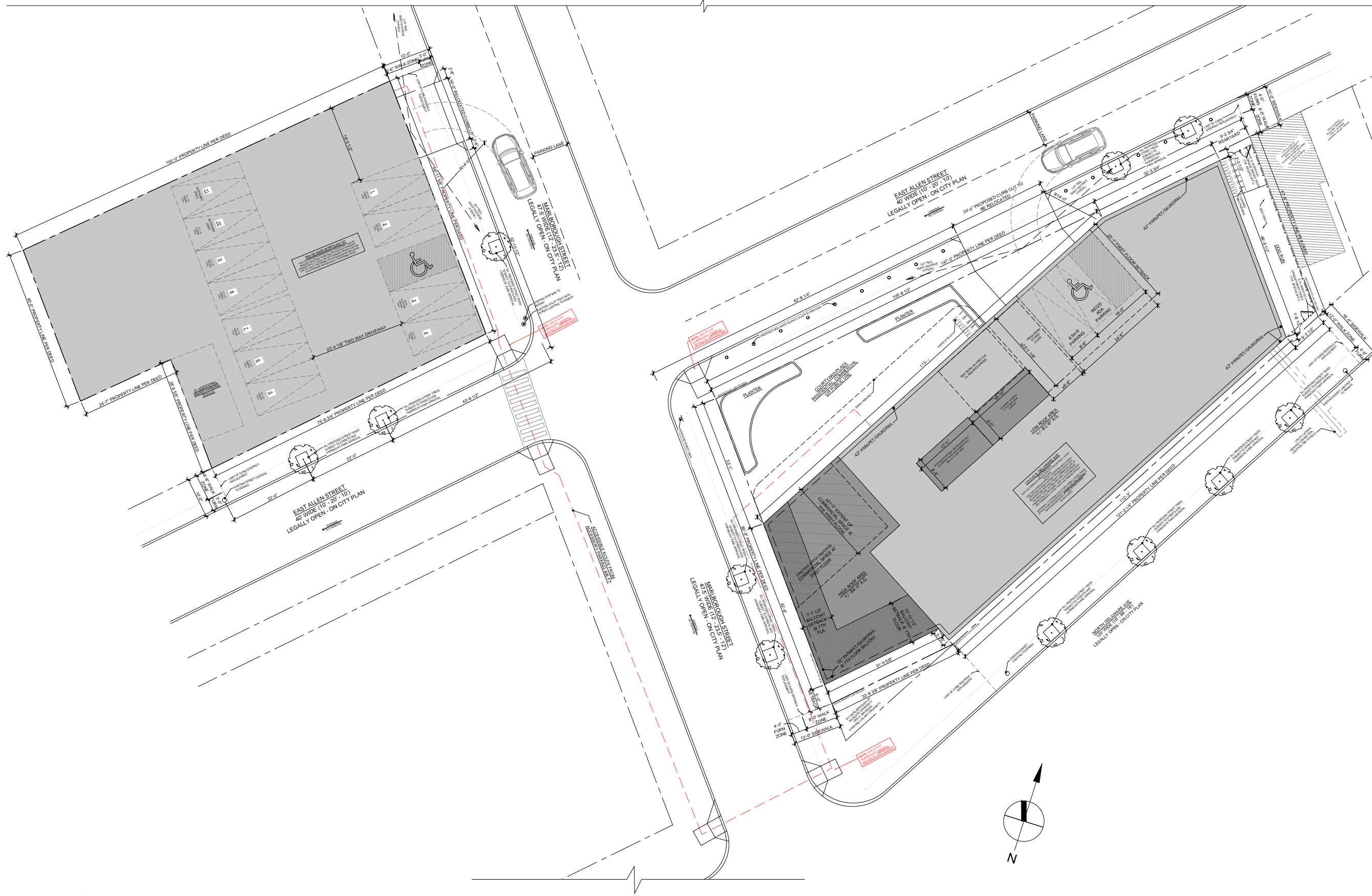
Front Yard Setback	based on setback of adjoining lots
Side Yard Setback	0' per yard
Rear Yard Setback	the greater of 0' or 20% of lot depth
Building Height	30'

14-507 CDO - Central Delaware Riverfront Overlay District

(7) Off-Street Parking.
 (a) Off-street parking shall not be located in the front yard.
 (b) Non-accessory and accessory aboveground structured parking shall be permitted by right, provided that an architectural or landscaped screening device is applied to each facade that faces a public street, park, or trail and is approved by the Commission for appropriateness with the surrounding neighborhood character. Required accessory parking in an aboveground structure that meets the requirements of this subsection (b) shall be excluded from the calculation of gross floor area; any parking provided in addition to the required accessory parking shall count towards the calculation of gross floor area.

APPROVAL STAMPS





SCOPE OF WORK

FOR THE COMPLETE DEMOLITION OF THE EXISTING 1 STORY BUILDING.
 FOR THE CONSTRUCTION OF A PROPOSED NEW 7-STORY DETACHED STRUCTURE W/
 ROOF ACCESS FOR MAINTANACE ONLY.
 PROPOSED USE INCLUDED VACANT COMMERCIAL USE AT THE FIRST FLOOR, 63
 DWELLING UNITS, 2 ACCESSORY PARKING STALLS (INCLUDING 1 ADA VA ACCESSIBLE
 SPOT) AND A MINIMUM 21 CLASS 1A BICYCLE PARKING SPACES

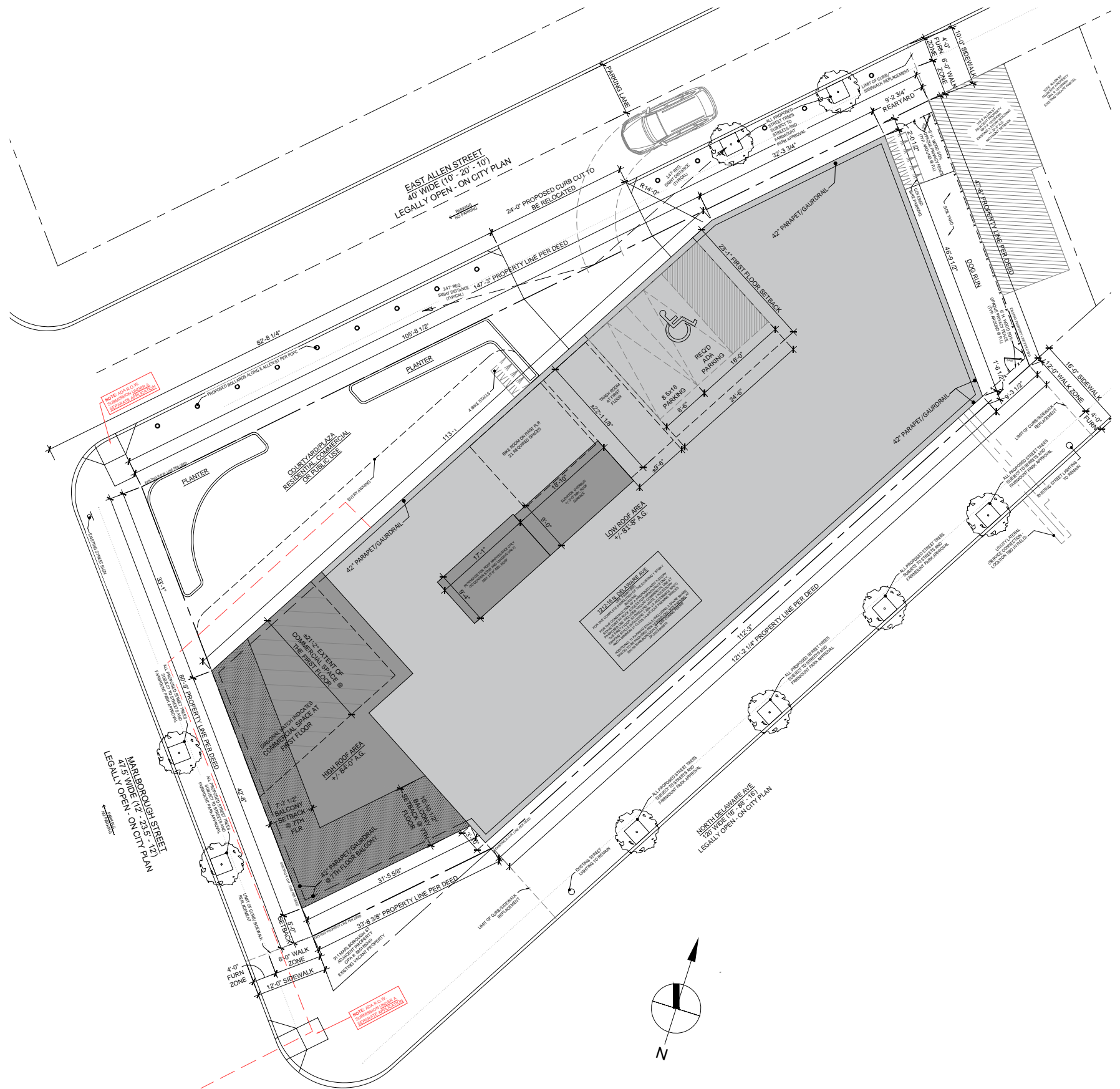
ADDITIONAL 11 PARKING STALLS (INCLUDING 2 SHARE SHARE SPACE) TO BE PROVIDED
 PER 14-802(9) OFF-SITE PARKING AT 950-56 MARLBOROUGH ST. SEE ZONING PERMIT
 ZP-2023-009819

ZONING DATA - 1212-16 N. DELAWARE AVE

CMX-3 ZONING	REQUIRED/ ALLOWED	PROPOSED
LOT AREA	-	9968.60 SF
COVERAGE	MAX. 80%	7283.00 SF (73.1%)
OPEN AREA	MIN. 20%	2685.60 SF (26.9%)
SIDE YARD	8FT IF USED	NONE
REAR YARD	NOT REQUIRED	9'-2 3/4"
HEIGHT	-	84'-0"
MAX FAR	500	500 (49,840 SF)

PARKING REQUIREMENTS

TOTAL PARKING	((63 DWELLING UNIT/10)*3) = 21 SPACES AUTO-SHARE REDUCTION = 2 AUTO-SHARE SPACE *4 = 8 SPACE REDUCTION TOTAL PARKING REQUIRED = 21 - 8 = 13 PARKING SPACES	13
ADA PARKING	1-25 PARKING SPACES = 1	1
BIKE PARKING	63 DWELLING UNITS/3 = 21	21



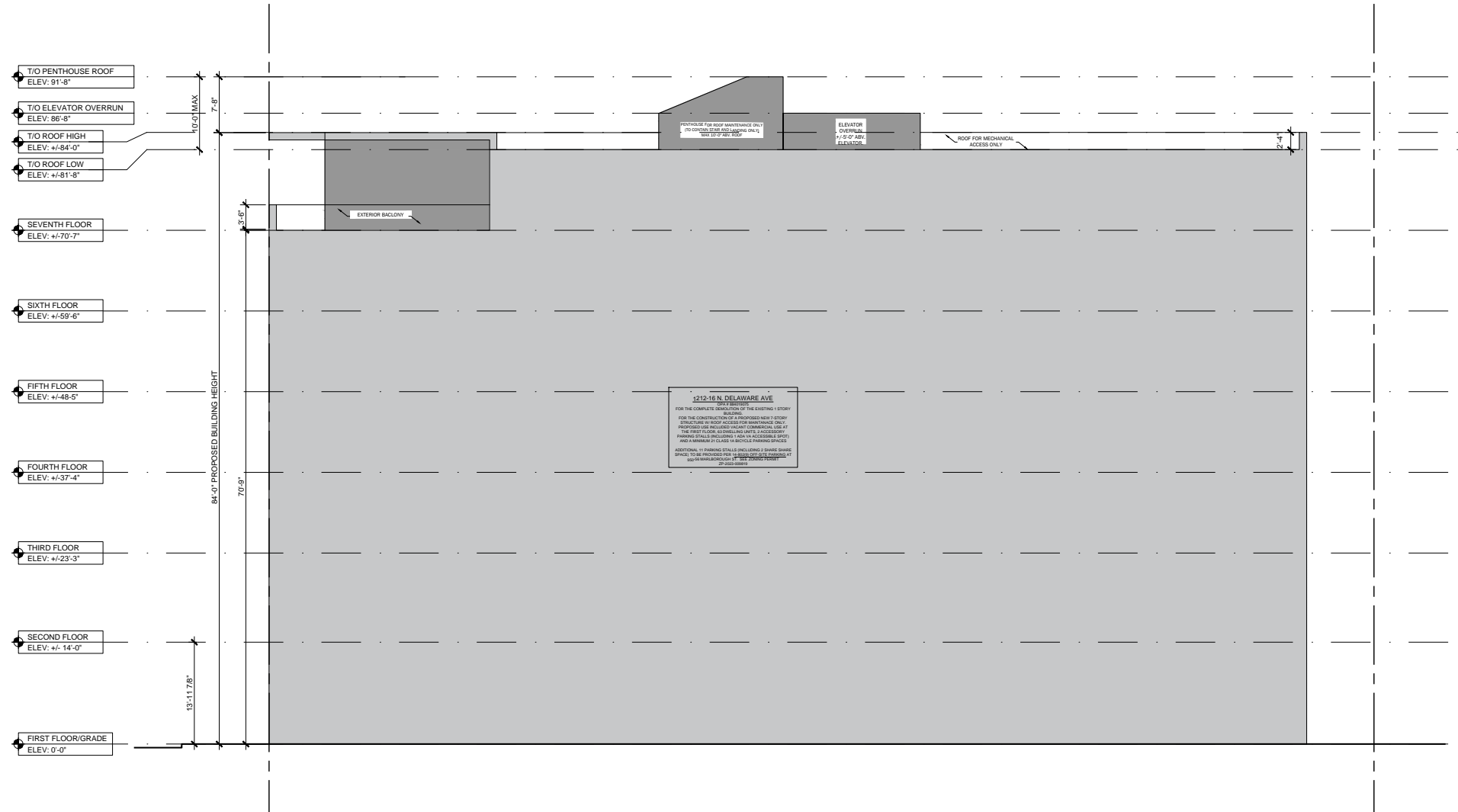
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 ZP-2023-009819

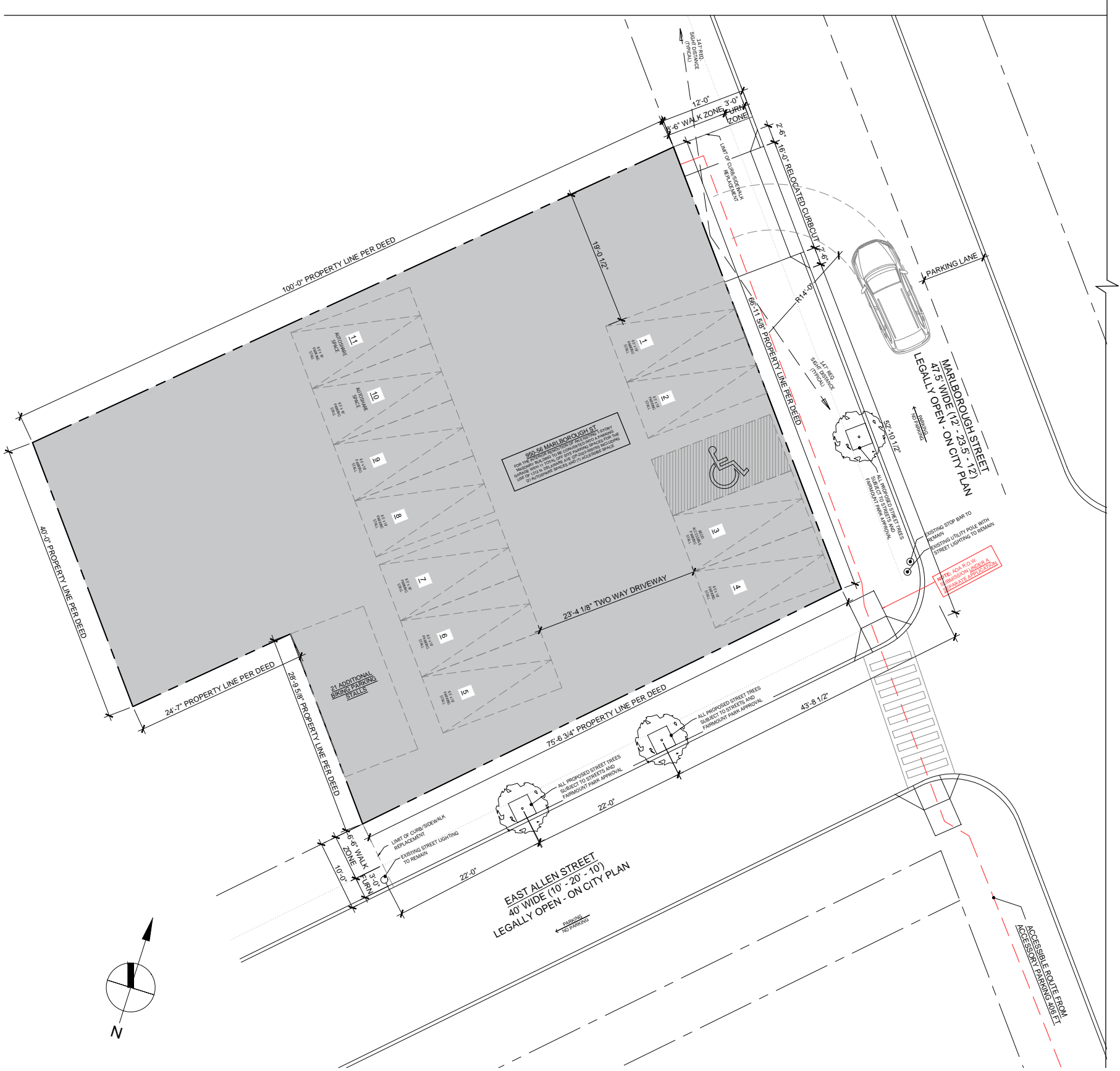
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CMX-3 ZONING	REQUIRED/ ALLOWED	PROPOSED	
LOT AREA	-	9968.60 SF	
COVERAGE	MAX. 80%	7283.00 SF	(73.1%)
OPEN AREA	MIN. 20%	2685.60 SF	(26.9%)
SIDE YARD	8FT IF USED	NONE	
REAR YARD	NOT REQUIRED	9'-2 ³ / ₄ "	
HEIGHT	-	84'-0"	
MAX FAR	500	500 (49,840 SF)	
PARKING REQUIREMENTS			
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ADA PARKING	1-25 PARKING SPACES = 1	1	
BIKE PARKING	63 DWELLING UNITS/3 = 21	21	



SCOPE OF WORK				
FOR THE INTERIOR RENOVATION OF AN EXISTING 1-STORY MASONRY BUILDING TO BE CONVERTED INTO A PARKING GARAGE WITH 11 TOTAL OFF SITE PARKING SPACES FOR THE USE OF 1212 N. DELAWARE AVE (ZP-2023-009750) INCLUDING (2) AUTOSHARE SPACES AND (1) ACCESSIBLE SPACE				
ZONING DATA - 950-56 MARLBOROUGH ST				
RSA-5 ZONING	REQUIRED/ ALLOWED	EXISTING	PROPOSED	
LOT AREA	960 SF	6089.0 SF	6089.00 SF	
COVERAGE	MAX. 80%	6089.0 SF	6089.00 SF	(100.0%)
OPEN AREA	MIN. 20%	0.00 SF	0.00 SF	(0.0%)
REAR YARD	9'-0"	NONE	NONE	
HEIGHT	38'-0"	38'-0"	38'-0"	38'-0"

NOTE:
OFF-SITE PARKING IS UNDER A SEPARATE ZONING APPLICATION. ZP-2023-009819



PROPOSED OFF SITE PARKING ZONING PLAN

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PROPOSED MIXED-USE BUILDING

- 7- STORIES
- 49,869 TOTAL GSF
- 62 RESIDENTIAL UNITS + 1 COMMERCIAL SPACE
- 13 TOTAL PARKING STALLS
 - 2 ON-SITE PARKING STALLS
 - 11 OFF-SITE PARKING STALLS INCLUDING 2 AUTO-SHARE STALLS
- 52 TOTAL BICYCLE STALLS
 - 21 ON-SITE TYPE 1A BICYCLE PARKING
 - 10 EXTERIOR ON-SITE BICYCLE PARKING
 - 21 OFF-SITE TYPE 1A BICYCLE PARKING

- COMMERCIAL
- RESIDENTIAL AMENITY
- RESIDENTIAL UNIT
- CIRCULATION/UTILITY
- BALCONY

- (A) PROPOSED STREET TREE
- (B) BOLLARDS ADJACENT TO E ALLEN ST
- (C) RAISED BRICK PLANTER
- (D) CURVED DINING BENCH
- (E) BUILT-IN METAL PLANTERS
- (F) ENTRY PLANTERS
- (G) DOG RUN TURF
- (H) ADA CORNER RAMP
- (I) PROPOSED BICYCLE PARKING
- (J) EXISTING STREET SIGNAGE TO REMAIN
- (K) EXISTING STREET LIGHTING TO REMAIN
- (L) PAVED PLAZA



GROUND FLOOR PLAN

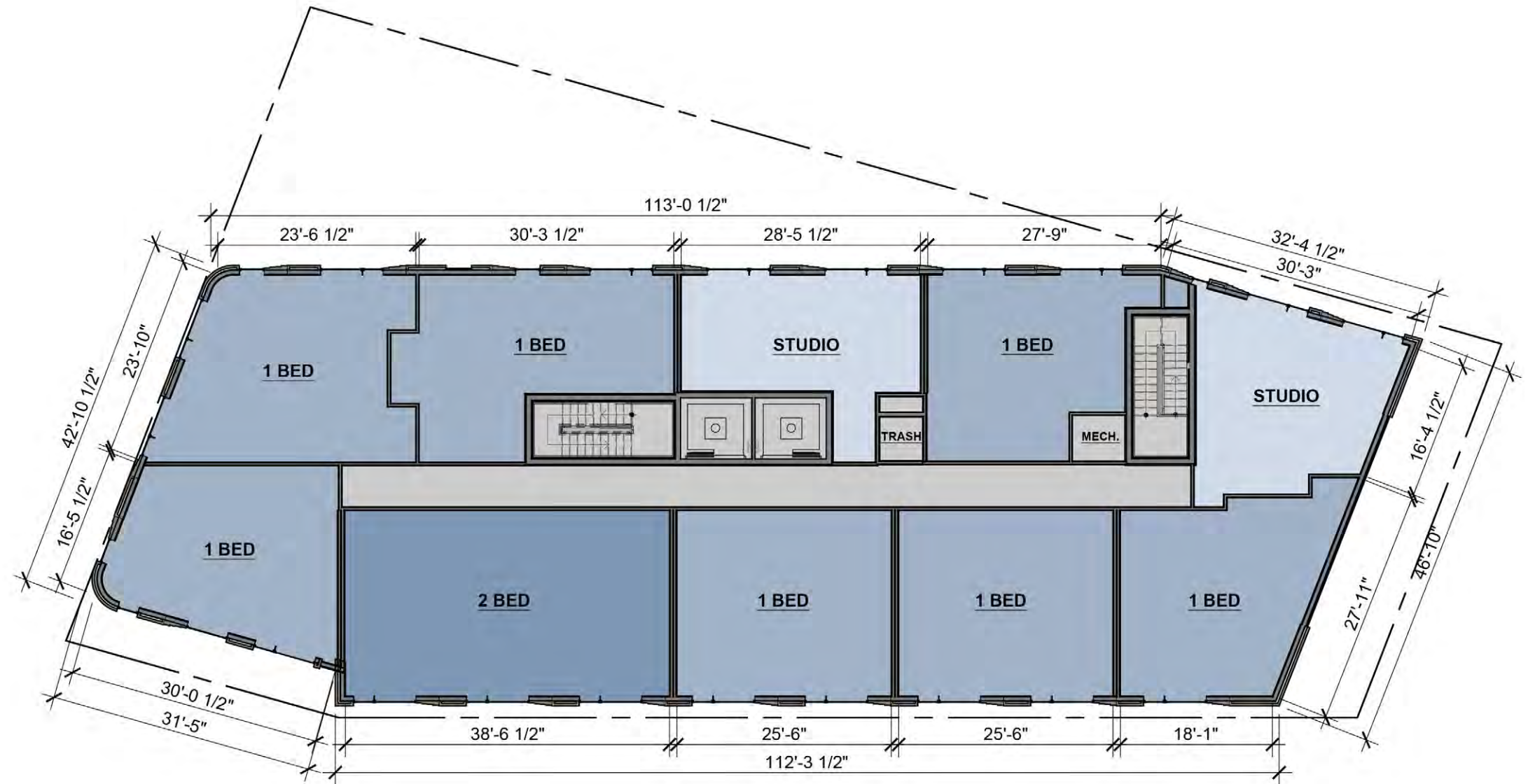


PROPOSED MIXED-USE BUILDING

- 7- STORIES
- 49,869 TOTAL GSF
- 62 RESIDENTIAL UNITS + 1 COMMERCIAL SPACE
- 13 TOTAL PARKING STALLS
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- 52 TOTAL BICYCLE STALLS
 - 21 ON-SITE TYPE 1A BICYCLE PARKING
 - 10 EXTERIOR ON-SITE BICYCLE PARKING
 - 21 OFF-SITE TYPE 1A BICYCLE PARKING

- COMMERCIAL
- RESIDENTIAL AMENITY
- RESIDENTIAL UNIT
- CIRCULATION/UTILITY
- BALCONY

UNIT MATRIX				
FLOOR	STUDIO	1 BED	2 BED	UNITS
FIRST FLOOR	0	4	0	4
SECOND FLOOR	2	7	1	10
THIRD FLOOR	2	7	1	10
FOURTH FLOOR	2	7	1	10
FIFTH FLOOR	2	7	1	10
SIXTH FLOOR	2	7	1	10
SEVENTH FLOOR	2	5	1	8
PROJECT TOTALS	12	44	6	62

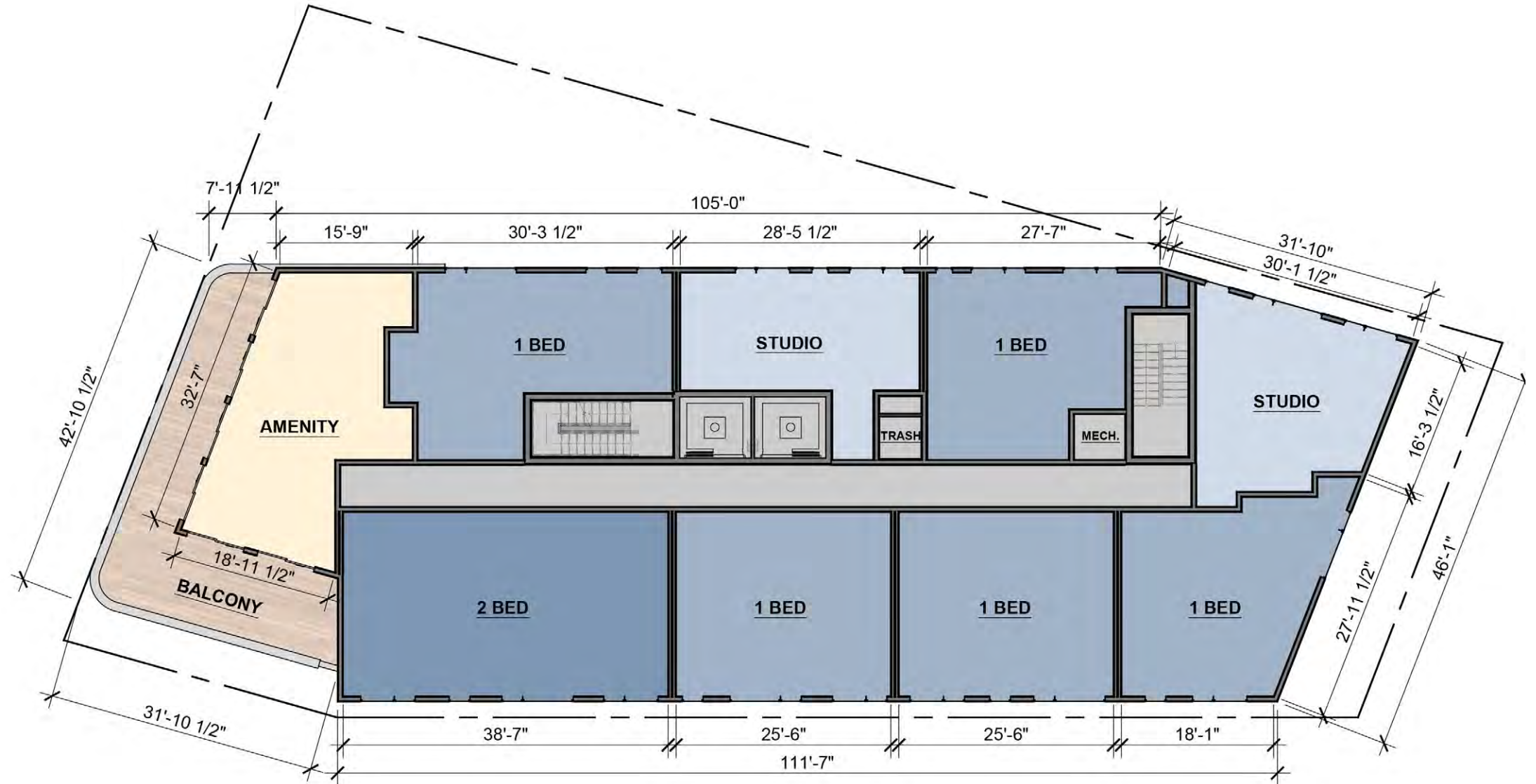


PROPOSED MIXED-USE BUILDING

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- 49,869 TOTAL GSF
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- 13 TOTAL PARKING STALLS
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 - 21 ON-SITE TYPE 1A BICYCLE PARKING
 - 10 EXTERIOR ON-SITE BICYCLE PARKING
 - 21 OFF-SITE TYPE 1A BICYCLE PARKING

- COMMERCIAL
- RESIDENTIAL AMENITY
- RESIDENTIAL UNIT
- CIRCULATION/UTILITY
- BALCONY

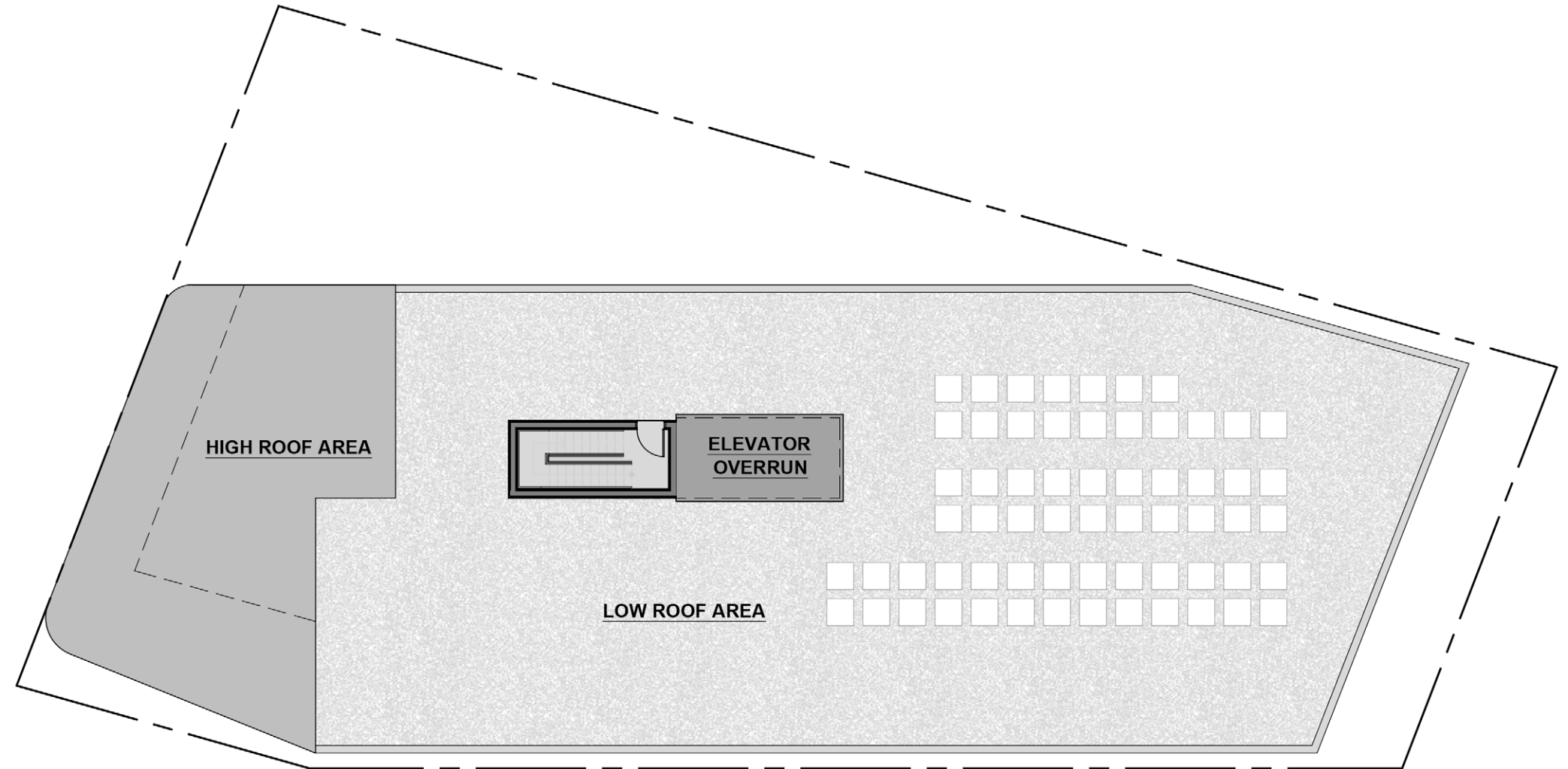
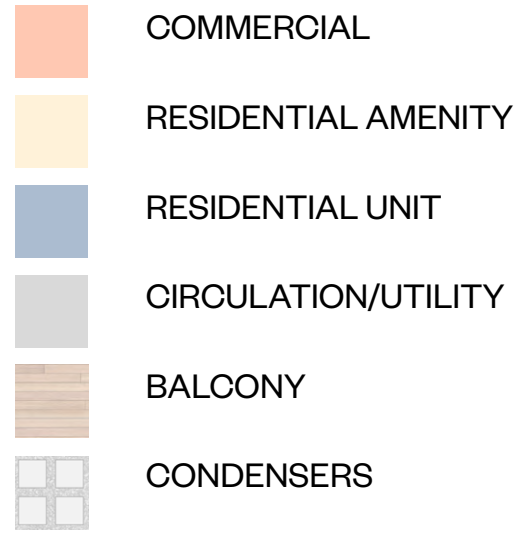
UNIT MATRIX				
FLOOR	STUDIO	1 BED	2 BED	UNITS
FIRST FLOOR	0	4	0	4
SECOND FLOOR	2	7	1	10
THIRD FLOOR	2	7	1	10
FOURTH FLOOR	2	7	1	10
FIFTH FLOOR	2	7	1	10
SIXTH FLOOR	2	7	1	10
SEVENTH FLOOR	2	5	1	8
PROJECT TOTALS	12	44	6	62

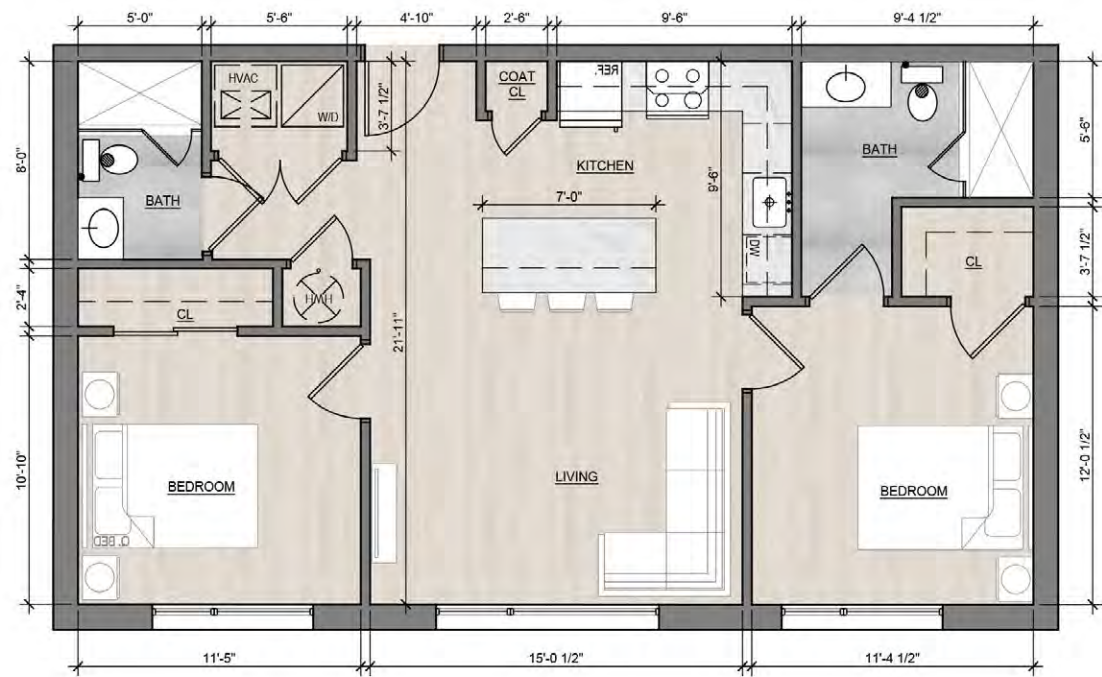


SEVENTH FLOOR PLAN

PROPOSED MIXED-USE BUILDING

- 7- STORIES
- 49,869 TOTAL GSF
- 62 RESIDENTIAL UNITS + 1 COMMERCIAL SPACE
- 13 TOTAL PARKING STALLS
 - 2 ON-SITE PARKING STALLS
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- 52 TOTAL BICYCLE STALLS
 - 21 ON-SITE TYPE 1A BICYCLE PARKING
 - 10 EXTERIOR ON-SITE BICYCLE PARKING
 - 21 OFF-SITE TYPE 1A BICYCLE PARKING





TYPICAL 2 BED UNIT LAYOUT
 SCALE: 1/8" = 1'-0"
 APPROX. 939 SF
 6 TOTAL 2BED UNITS



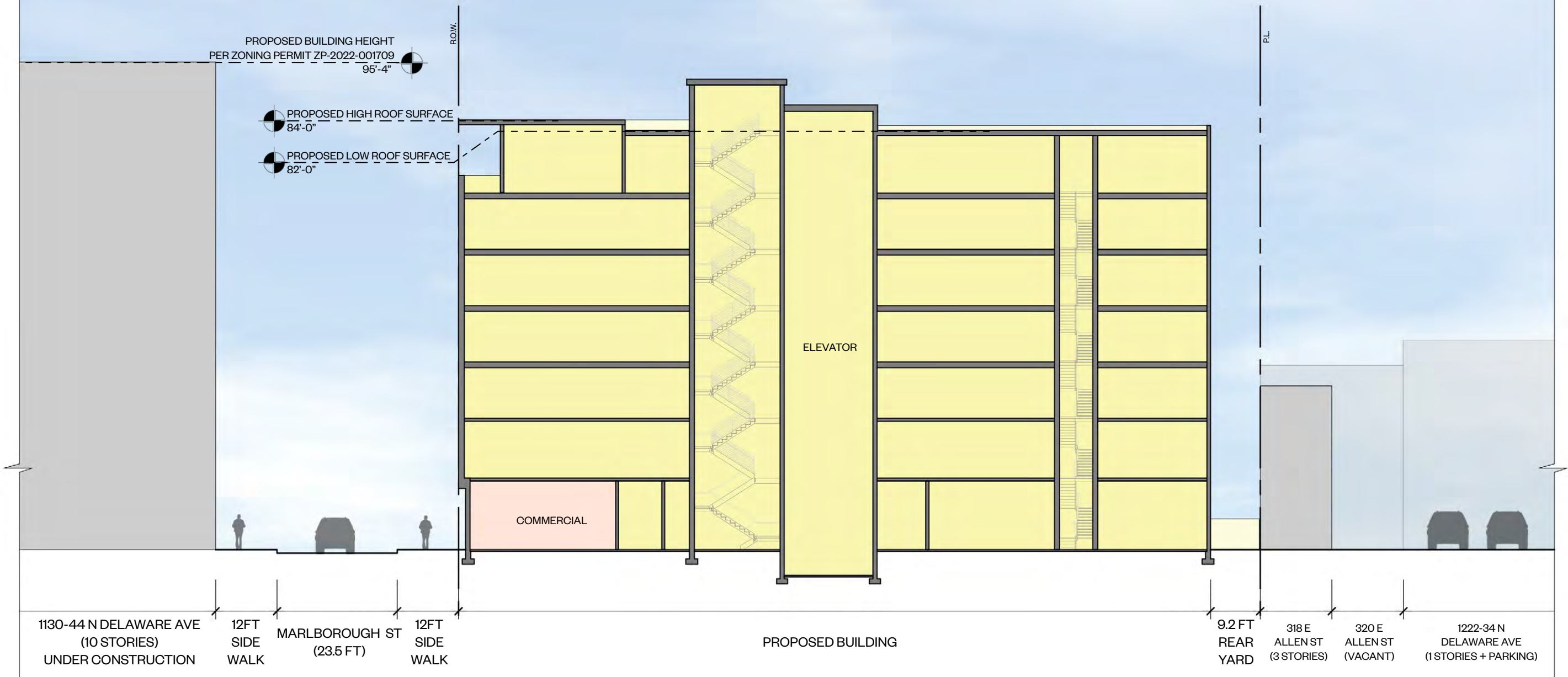
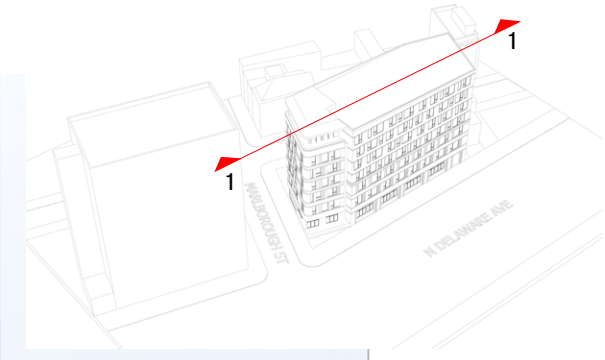
TYPICAL 1 BED UNIT LAYOUT
 SCALE: 1/8" = 1'-0"
 APPROX. 550 SF
 44 TOTAL 1BED UNITS



TYPICAL STUDIO LAYOUT
 SCALE: 1/8" = 1'-0"
 APPROX. 455 SF
 12 TOTAL STUDIO UNITS



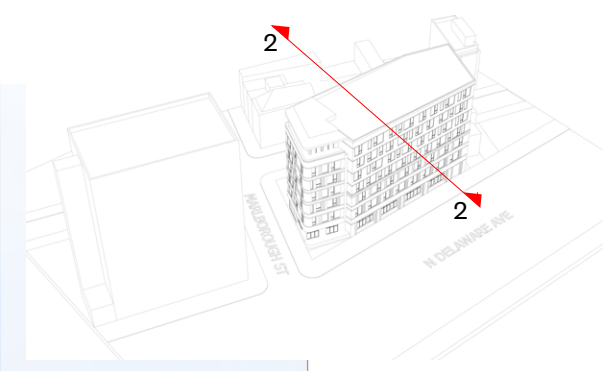
UNITS PLANS



SITE SECTION 1



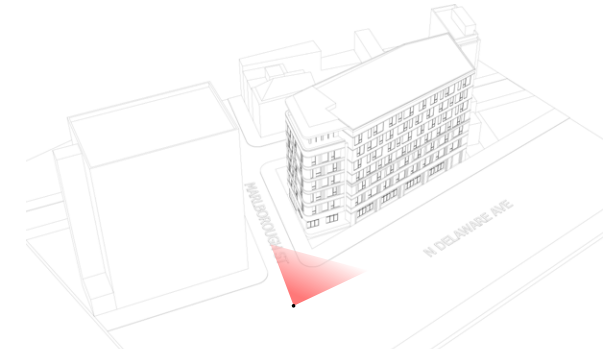
SITE SECTIONS



SITE SECTION 2



SITE SECTIONS



N. DELAWARE AVE RENDERING
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TREES

STREET SCAPE



Ulmus americana 'Princeton'
PRINCETON AMERICAN ELM

SHRUBS



Physocarpus opulifolius
COMMON NINEBARK

PERENNIALS



Pennisetum alopecuroides 'Hameln'
'HAMELN' DWARF FOUNTAIN GRASS



Amelanchier laevis
ALLEGHENY SERVICEBERRY



Rhus aromatica 'Gro-low'
FRAGRANT SUMAC



Carex crinita
FRINGED SEDGE

ENTRY PLAZA



Cercis canadensis
EASTERN REDBUD



Taxus baccata 'Repandens'
DWARF ENGLISH YEWE



Mertensia virginica
VIRGINIA BLUEBELLS



STREET SCAPE



METAL TREE PIT GRATES 3'L x 3'W TO PROTECT THE YOUNG STREET TREES & PLANTING SOIL



STEEL EDGING POWDER-COATED BLACK TO DEFINE PLANTING BEDS ADJACENT TO SIDEWALK



MULTI-HUE ARTIFICIAL TURF AT DOG TURF ADDS GREENERY FROM SIDEWALK

ENTRY PLAZA



RAISED PLANTER WALL USING BRICK TO MATCH ARCHITECTURE WITH TOP ROWLOCK COURSE



RAISED PLANTERS WILL CREATE A GROVE OF SMALL ORNAMENTAL SHADE TREES



BLACK SURFACE-MOUNTED U-SHAPE BIKE RACKS



LARGE 48 x 24 PORCELAIN OR PREST PAVERS IN RUNNING BOND LAYOUT



FLOATING WOOD BENCHES FIXED TO THE SIDE OF RAISED BRICK PLANTERS



BLACK METAL BOLLARDS ALONG STREET WITH SMALL INTEGRATED LED LIGHT AT TOP





1 PRINCETON AMERICAN ELM TREES HAVE AN UPRIGHT & NARROW FORM



3 MULTI-STEM EASTERN REDBUD TREES TO FORM AN ORNAMENTAL GROVE

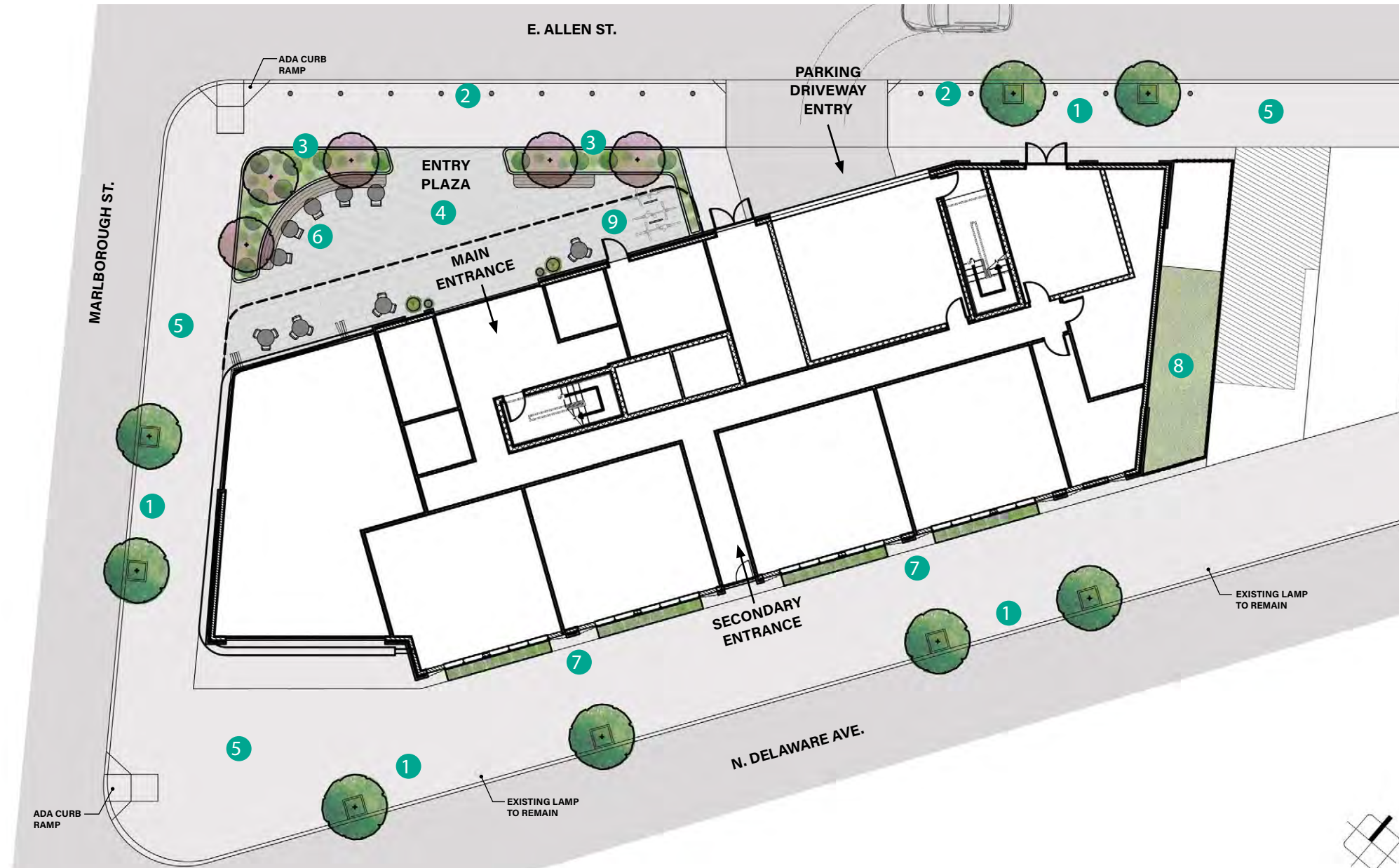


7 2'H PENNISETUM GRASSES EMERGING FROM LINEAR IN-GROUND PLANTING BEDS

- 1 **STREET TREE:** 3'L x 3'W TREE PIT CONTAINING A PRINCETON AMERICAN ELM TREE
- 2 **BOLLARD:** SLIM BOLLARD WITH INTEGRATED TOP LED LIGHT ADJACENT TO E. ALLEN ST.
- 3 **RAISED BRICK PLANTER:** 2'H RED BRICK PLANTER CONTAINING SMALL ORNAMENTAL TREES

- 4 **PAVED PLAZA:** 4'L x 2'W PREST OR PORCELAIN PAVERS RUNNING PARALLEL TO ENTRY FACADE
- 5 **TYP. SIDEWALK:** TYP. POURED CONCRETE SIDEWALK
- 6 **CURVED DINING BENCH:** WOOD CURVED FLOATING BENCH WITH BISTRO TABLES FOR CAFE USE

- 7 **IN-GROUND PLANTING BEDS:** NARROW PLANTING BEDS ALIGNED WITH WINDOWS
- 8 **DOG RUN ARTIFICIAL TURF:** MULTI-HUE TURF WITH LONG GRASS BLADE
- 9 **BIKE RACKS:** TWO SURFACE-MOUNTED U-SHAPE BIKE RACKS



STREET SCAPE - SITE PLAN





3 CURVED BRICK CORNERS OF RAISED PLANTER WITH ROWLOCK COURSE



4 LARGE CONCRETE PAVERS IN RUNNING BOND LAYOUT

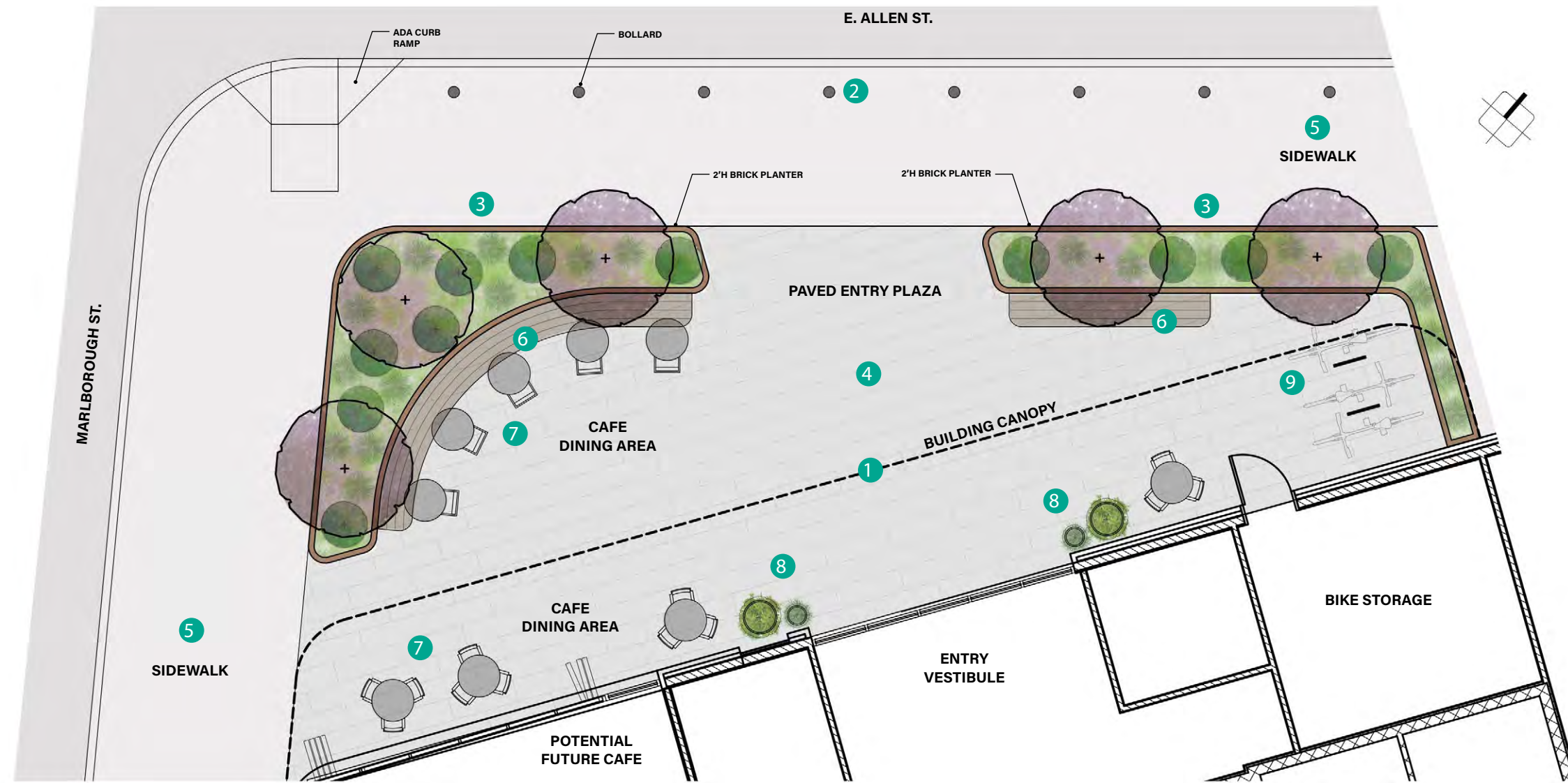


9 TWO U-SHAPED BIKE RACKS CLOSE TO BIKE STORAGE ROOM

- 1 **ENTRY CANOPY:** OVERHEAD METAL CANOPY WITH RECESSED LIGHTING PARALLEL TO FACADE
- 2 **BOLLARD:** SLIM BOLLARD WITH INTEGRATED TOP LED LIGHT ADJACENT TO E. ALLEN ST.
- 3 **RAISED BRICK PLANTER:** 2'H RED BRICK PLANTER CONTAINING SMALL ORNAMENTAL TREES

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- 6 **CURVED DINING BENCH:** WOOD CURVED FLOATING BENCH WITH BISTRO TABLES FOR CAFE USE

- 7 **BISTRO TABLES:** ROUND METAL TABLES & FOLDING CHAIRS FOR CAFE USE
- 8 **ENTRY PLANTERS:** CERAMIC ROUND POTS FLANKING ENTRY
- 9 **BIKE RACKS:** TWO SURFACE-MOUNTED U-SHAPE BIKE RACKS



PUBLIC PLAZA - DETAILED SITE PLAN



- 1 **STREET TREE:** 3'L x 3'W TREE PIT CONTAINING A PRINCETON AMERICAN ELM TREE
- 2 **IN-GROUND PLANTING BEDS:** NARROW PLANTING BEDS ALIGNED WITH WINDOWS
- 3 **DOG RUN ARTIFICIAL TURF:** MULTI-HUE TURF WITH LONG GRASS BLADE

1 PRINCETON AMERICAN ELM TREES HAVE AN UPRIGHT & NARROW FORM



2 LARGE MASS OF HAMELN PENNISETUM GRASSES ALIGNED WITH WINDOWS



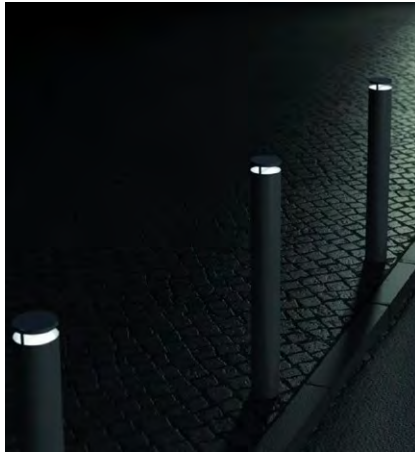
3 MULTI-HUE ARTIFICIAL TURF AT DOG TURF ADDS GREENERY FROM SIDEWALK



ELEVATION PERSPECTIVE SKETCH FROM EAST



ELEVATION PERSPECTIVE SKETCH FROM N. DELAWARE AVE.



1 BLACK METAL BOLLARDS ALONG STREET WITH SMALL INTEGRATED LED LIGHT AT TOP



4 MULTI-STEM EASTERN REDBUDS PLANTED IN RAISED BRICK PLANTERS



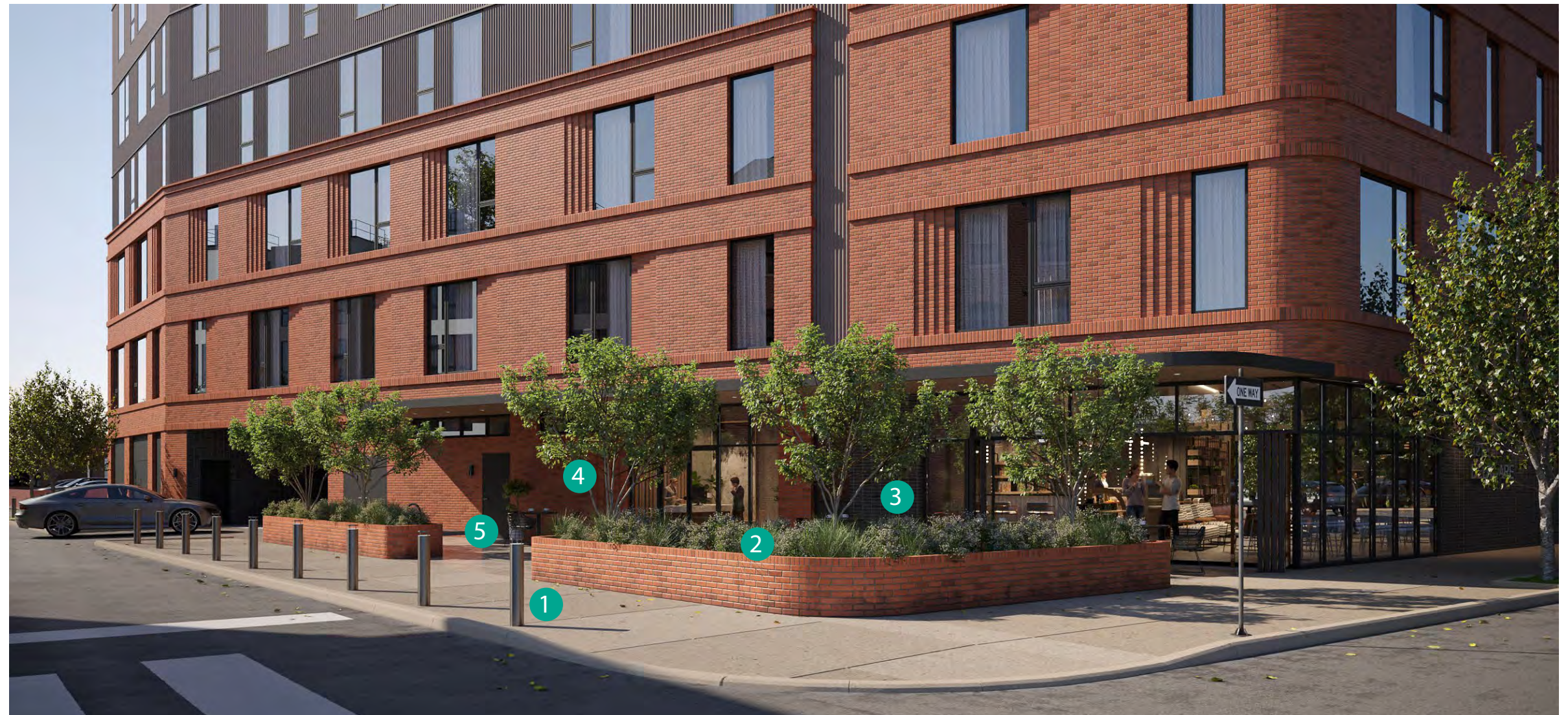
5 LARGE 48 x 24 PORCELAIN OR PREST PAVERS IN RUNNING BOND LAYOUT



2 RAISED BRICK PLANTER: 2'H RED BRICK CURVED PLANTER CONTAINING SMALL ORNAMENTAL TREES



3 MIX OF LOW UNDERSTORY PERENNIALS SUCH AS VIRGINIA BLUEBELLS



PLAZA PERSPECTIVE SKETCH

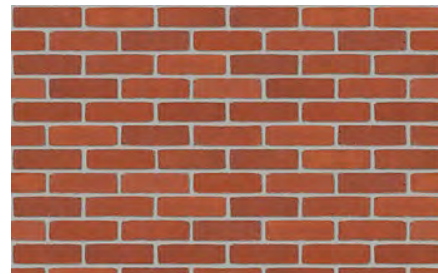




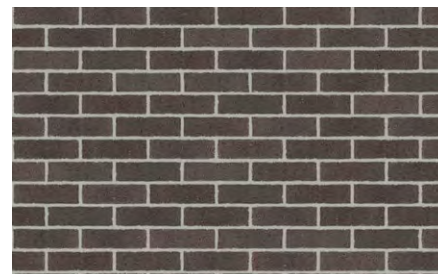
4 BLACK ALUMINUM FRAME WINDOWS/STOREFRONT



3 WOOD PANEL | STOCAST COLOR: LIGHT OAK



2 BRICK VENEER | GLEN GERY COLOR: RED



1 BRICK VENEER | GLEN GERY COLOR: BLACK



8 CORRUGATED METAL PANEL | DREXEL METAL COLOR: STONE WHITE



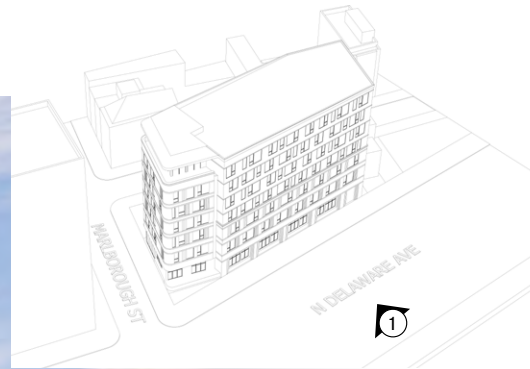
7 CORRUGATED METAL PANEL | DREXEL METAL COLOR: CITY SCAPE



6 CORRUGATED METAL PANEL | DREXEL METAL COLOR: SLATE GREY



5 CORRUGATED METAL PANEL | DREXEL METAL COLOR: MUSKET GREY

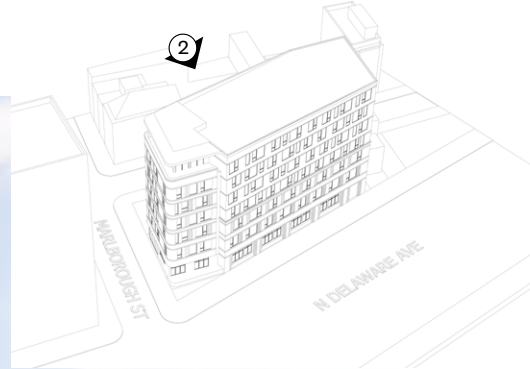


NOTE:
STREET FURNISHINGS OMITTED FOR CLARITY



N. DELAWARE AVE ELEVATION

1212-16 N. DELAWARE AVE | CIVIC DESIGN REVIEW | PAGE 33



NOTE:
STREET FURNISHINGS OMITTED FOR CLARITY

E. ALLEN ST ELEVATION

1212-16 N. DELAWARE AVE | CIVIC DESIGN REVIEW | PAGE 34





NOTE:
STREET FURNISHINGS OMITTED FOR CLARITY

MARLBOROUGH ST ELEVATION

1212-16 N. DELAWARE AVE | CIVIC DESIGN REVIEW | PAGE 35





NOTE:
STREET FURNISHINGS OMITTED FOR CLARITY

REAR YARD ELEVATION

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CDR PROJECT APPLICATION FORM

Note: For a project application to be considered for a Civic Design Review agenda, complete and accurate submittals must be received no later than 4 P.M. on the submission date. A submission does not guarantee placement on the agenda of the next CDR meeting date.

L&I APPLICATION NUMBER: ZP-2023-009750

What is the trigger causing the project to require CDR Review? Explain briefly.

The application includes a proposed structure with a use that is greater than 50 residential units

PROJECT LOCATION

Planning District: River Wards Council District: 5th

Address: 1212-16 N. Delaware Ave

Philadelphia, PA 19125

Is this parcel within an Opportunity Zone? Yes No Uncertain
 If yes, is the project using Opportunity Zone Funding? Yes No Uncertain

CONTACT INFORMATION

Applicant Name: Gabriel Deck, RA Primary Phone: 215.279.7531

Email: permits@gnomearch.com Address: 1901 S. 9th St. Rm: 310

Philadelphia, PA 19148

Property Owner: 1212 N. Delaware Ave LP Developer Philly Capital Group

Architect: Gnome Architects LLC

SITE CONDITIONS

Site Area: 9,968.6 GSF

Existing Zoning: CMX-3 Are Zoning Variances required? Yes No

Proposed Use:

Area of Proposed Uses, Broken Out by Program (Include Square Footage and # of Units):

1,140 GSF of Vacant Commercial Space : 1 commercial spaces

48,700 GSF of Residential Space : 62 Residential Units

Proposed # of Parking Units: 13 parking stalls: 2 On-site + 11 off-site stalls (including 2 ADA van, 2 auto-share, and 21 onsite bike parking stalls

COMMUNITY MEETING

Community meeting held: Yes No

If yes, please provide written documentation as proof.

If no, indicate the date and time the community meeting will be held:

Date: 11/14/2023 Time: 6:30 pm

ZONING BOARD OF ADJUSTMENT HEARING

ZBA hearing scheduled: Yes No NA

If yes, indicate the date hearing will be held:

Date: _____

Civic Design Review Sustainable Design Checklist

Sustainable design represents important city-wide concerns about environmental conservation and energy use. Development teams should try to integrate elements that meet many goals, including:

- Reuse of existing building stock
- Incorporation of existing on-site natural habitats and landscape elements
- Inclusion of high-performing stormwater control
- Site and building massing to maximize daylight and reduce shading on adjacent sites
- Reduction of energy use and the production of greenhouse gases
- Promotion of reasonable access to transportation alternatives

The Sustainable Design Checklist asks for responses to specific benchmarks. These metrics go above and beyond the minimum requirements in the Zoning and Building codes. All benchmarks are based on adaptations from Leadership in Energy and Environmental Design (LEED) v4 unless otherwise noted.

Categories	Benchmark	Does project meet benchmark? If yes, please explain how. If no, please explain why not.
Location and Transportation		
(1) Access to Quality Transit	Locate a functional entry of the project within a ¼-mile (400-meter) walking distance of existing or planned bus, streetcar, or rideshare stops, bus rapid transit stops, light or heavy rail stations.	Yes: -Bus routes 43, 5, 25 -Septa Trolley Lines, and MFL -Project includes car share spaces
(2) Reduced Parking Footprint	All new parking areas will be in the rear yard of the property or under the building, and unenclosed or uncovered parking areas are 40% or less of the site area.	Yes: the proposed parking is either under the building footprint, or fully enclosed within an existing warehouse on an adjacent site.
(3) Green Vehicles	Designate 5% of all parking spaces used by the project as preferred parking for green vehicles or car share vehicles. Clearly identify and enforce for sole use by car share or green vehicles, which include plug-in electric vehicles and alternative fuel vehicles.	Yes: 2 out of 13 car spaces are dedicated for car share spaces (15%)
(4) Railway Setbacks (Excluding frontages facing trolleys/light rail or enclosed subsurface rail lines or subways)	To foster safety and maintain a quality of life protected from excessive noise and vibration, residential development with railway frontages should be setback from rail lines and the building's exterior envelope, including windows, should reduce exterior sound transmission to 60dBA. (If setback used, specify distance) ⁱ	NA
(5) Bike Share Station	Incorporate a bike share station in coordination with and conformance to the standards of Philadelphia Bike Share.	Project includes private bike storage and is adjacent to existing bike share kiosks.

Water Efficiency		
(6) Outdoor Water Use	Maintain on-site vegetation without irrigation. OR, Reduce of watering requirements at least 50% from the calculated baseline for the site's peak watering month.	Selected plans will be drought tolerant to reduce watering needs in peak months.
Sustainable Sites		
(7) Pervious Site Surfaces	Provides vegetated and/or pervious open space that is 30% or greater of the site's Open Area, as defined by the zoning code. Vegetated and/or green roofs can be included in this calculation.	No, although planting areas will be provided it will not be 30%
(8) Rainwater Management	Conform to the stormwater requirements of the Philadelphia Water Department(PWD) and either: A) Develop a green street and donate it to PWD, designed and constructed in accordance with the PWD Green Streets Design Manual, OR B) Manage additional runoff from adjacent streets on the development site, designed and constructed in accordance with specifications of the PWD Stormwater Management Regulations	No, as our site is not over the 15,000 SF limit for storm water management we are not proposing a rainwater management system. Neither option A or B will be provided
(9) Heat Island Reduction (excluding roofs)	Reduce the heat island effect through either of the following strategies for 50% or more of all on-site hardscapes: A) Hardscapes that have a high reflectance, an SRI>29. B) Shading by trees, structures, or solar panels.	Yes, hardscapes will meet SRI>29 and trees will be provided in the sidewalks, and at roof deck location.
Energy and Atmosphere		
(10) Energy Commissioning and Energy Performance - Adherence to the New Building Code	PCPC notes that as of April 1, 2019 new energy conservation standards are required in the Philadelphia Building Code, based on recent updates of the International Energy Conservation Code (IECC) and the option to use ASHRAE 90.01-2016. PCPC staff asks the applicant to state which path they are taking for compliance, including their choice of code and any options being pursued under the 2018 IECC. ⁱⁱ	2018 IECC, prescriptive approach with ComCheck
(11) Energy Commissioning and Energy Performance - Going beyond the code	Will the project pursue energy performance measures beyond what is required in the Philadelphia code by meeting any of these benchmarks? ⁱⁱⁱ •Reduce energy consumption by achieving 10% energy savings or more from an established baseline using	No, the project will not be pursuing additional performance standards.

	ASHRAE standard 90.1-2016 (LEED v4.1 metric). •Achieve certification in Energy Star for Multifamily New Construction (MFNC). •Achieve Passive House Certification	
(12) Indoor Air Quality and Transportation	Any sites within 1000 feet of an interstate highway, state highway, or freeway will provide air filters for all regularly occupied spaces that have a Minimum Efficiency Reporting Value (MERV) of 13. Filters shall be installed prior to occupancy. ^{iv}	Yes: Filters will be installed to meet MERV 13 minimum
(13) On-Site Renewable Energy	Produce renewable energy on-site that will provide at least 3% of the project's anticipated energy usage.	No, we are not proposing any renewable energy production on site
Innovation		
(14) Innovation	Any other sustainable measures that could positively impact the public realm.	Street planting and a public plaza will be incorporated to reduce the heat island effect and provide social spaces and seating for the public.

ⁱ Railway Association of Canada (RAC)'s "Guidelines for New Development in Proximity to Railway Operations. Exterior Sound transmission standard from LEED v4, BD+C, Acoustic Performance Credit.

ⁱⁱ Title 4 The Philadelphia Building Construction and Occupancy Code See also, "The Commercial Energy Code Compliance" information sheet: <https://www.phila.gov/li/Documents/Commercial%20Energy%20Code%20Compliance%20Fact%20Sheet--Final.pdf> and the "What Code Do I Use" information sheet: <https://www.phila.gov/li/Documents/What%20Code%20Do%20I%20Use.pdf>

ⁱⁱⁱ LEED 4.1, Optimize Energy Performance in LEED v4.1 For Energy Star: www.Energystar.gov For Passive House, see www.phius.org

^{iv} Section 99.04.504.6 "Filters" of the City of Los Angeles Municipal Code, from a 2016 Los Angeles Ordinance requiring enhanced air filters in homes near freeways



COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



INSTRUCTIONS

This Checklist is an implementation tool of the *Philadelphia Complete Streets Handbook* (the “Handbook”) and enables City engineers and planners to review projects for their compliance with the Handbook’s policies. The handbook provides design guidance and does not supersede or replace language, standards or policies established in the City Code, City Plan, or Manual on Uniform Traffic Control Devices (MUTCD).

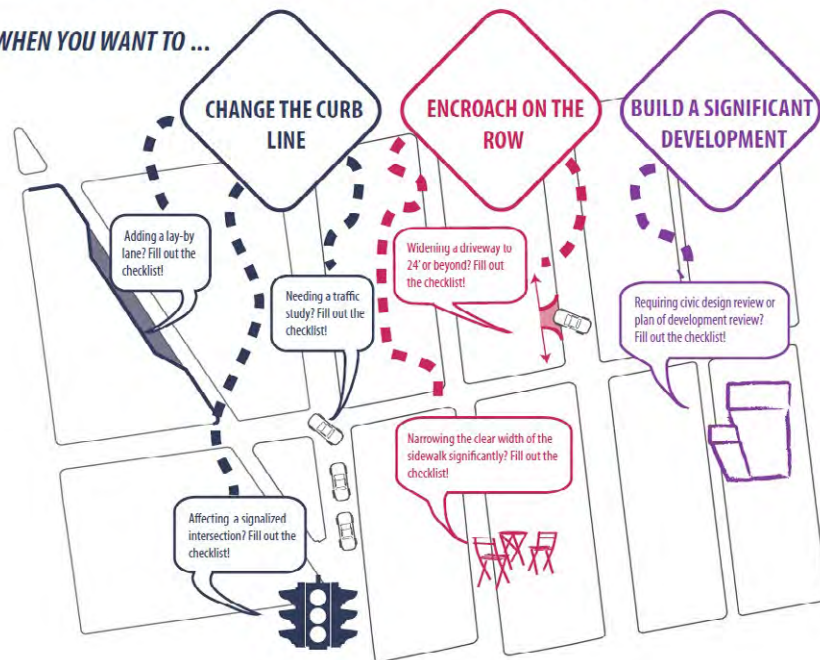
The Philadelphia City Planning Commission receives this Checklist as a function of its Civic Design Review (CDR) process. This checklist is used to document how project applicants considered and accommodated the needs of all users of city streets and sidewalks during the planning and/or design of projects affecting public rights-of-way. Departmental reviewers will use this checklist to confirm that submitted designs incorporate complete streets considerations (see §11-901 of The Philadelphia Code). Applicants for projects that require Civic Design Review shall complete this checklist and attach it to plans submitted to the Philadelphia City Planning Commission for review, along with an electronic version.

The Handbook and the checklist can be accessed at

<http://www.phila.gov/CityPlanning/projectreviews/Pages/CivicDesignReview.aspx>

WHEN DO I NEED TO FILL OUT THE COMPLETE STREETS CHECKLIST?

WHEN YOU WANT TO ...



PRELIMINARY PCPC REVIEW AND COMMENT:

DATE

FINAL STREETS DEPT REVIEW AND COMMENT:

DATE

COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



INSTRUCTIONS (continued)

APPLICANTS SHOULD MAKE SURE TO COMPLY WITH THE FOLLOWING REQUIREMENTS:

- This checklist is designed to be filled out electronically in Microsoft Word format. Please submit the Word version of the checklist. Text fields will expand automatically as you type.
- All plans submitted for review must clearly dimension the widths of the Furnishing, Walking, and Building Zones (as defined in Section 1 of the Handbook). “High Priority” Complete Streets treatments (identified in Table 1 and subsequent sections of the Handbook) should be identified and dimensioned on plans.
- All plans submitted for review must clearly identify and site all street furniture, including but not limited to bus shelters, street signs and hydrants.
- Any project that calls for the development and installation of medians, bio-swales and other such features in the right-of-way may require a maintenance agreement with the Streets Department.
- ADA curb-ramp designs must be submitted to Streets Department for review
- Any project that significantly changes the curb line may require a City Plan Action. The City Plan Action Application is available at <http://www.philadelphiastreet.com/survey-and-design-bureau/city-plans-unit>. An application to the Streets Department for a City Plan Action is required when a project plan proposes the:
 - Placing of a new street;
 - Removal of an existing street;
 - Changes to roadway grades, curb lines, or widths; or
 - Placing or striking a city utility right-of-way.

Complete Streets Review Submission Requirement*:

- EXISTING CONDITIONS SITE PLAN, should be at an identified standard engineering scale
 - FULLY DIMENSIONED
 - CURB CUTS/DRIVEWAYS/LAYBY LANES
 - TREE PITS/LANDSCAPING
 - BICYCLE RACKS/STATIONS/STORAGE AREAS
 - TRANSIT SHELTERS/STAIRWAYS
- PROPOSED CONDITIONS SITE PLAN, should be at an identified standard engineering scale
 - FULLY DIMENSIONED, INCLUDING DELINEATION OF WALKING, FURNISHING, AND BUILDING ZONES AND PINCH POINTS
 - PROPOSED CURB CUTS/DRIVEWAYS/LAYBY LANES
 - PROPOSED TREE PITS/LANDSCAPING
 - BICYCLE RACKS/STATIONS/STORAGE AREAS
 - TRANSIT SHELTERS/STAIRWAYS

*APPLICANTS PLEASE NOTE: ONLY FULL-SIZE, READABLE SITE PLANS WILL BE ACCEPTED. ADDITIONAL PLANS MAY BE REQUIRED AND WILL BE REQUESTED IF NECESSARY

COMPLETE STREETS CHECKLIST

1212-16 N. DELAWARE AVE | CIVIC DESIGN REVIEW | PAGE 39



COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



GENERAL PROJECT INFORMATION

- | | |
|---|---|
| <p>1. PROJECT NAME
<u>1212 N. Delaware Avenue</u></p> <p>3. APPLICANT NAME
<u>1212 North Delaware Avenue QOZ, LLC</u></p> <p>4. APPLICANT CONTACT INFORMATION
<u>1199 Ludlow Street, Unit 1412, Phila., PA 19107</u>
<u>(302) 562-2199 / mihir@phillycapiatgroup.com</u></p> <p>6. OWNER NAME
<u>1212 North Delaware Avenue QOZ, LLC</u></p> <p>7. OWNER CONTACT INFORMATION
<u>1199 Ludlow Street, Unit 1412, Phila., PA 19107</u>
<u>(302) 562-2199 / mihir@phillycapiatgroup.com</u></p> <p>8. ENGINEER / ARCHITECT NAME
<u>Ruggiero Plante Land Design / Ronald Glenn</u></p> <p>9. ENGINEER / ARCHITECT CONTACT INFORMATION
<u>5900 Ridge Avenue, Phila., PA 19128</u>
<u>(215) 508-3900 / ronald@ruggieroplante.com</u></p> <p>10. STREETS: List the streets associated with the project. Complete Streets Types can be found at www.phila.gov/map under the "Complete Street Types" field. Complete Streets Types are also identified in Section 3 of the Handbook.</p> | <p>2. DATE
<u>10/30/23</u></p> <p>5. PROJECT AREA: list precise street limits and scope
<u>Project fronts 106lf of East Allen Street east of Marlborough Street, 80lf of Marlborough Street between E. Allen St & N. Delaware Ave, and 155lf of N. Delaware Ave east of Marlborough Street. The scope includes construction of a 7-story building containing a commercial space and 63 DUs. Other site improvements include curb/sidewalk replacement, landscaping, landscape walls, utility laterals, etc.</u></p> |
|---|---|

Also available here: <http://metadata.phila.gov/#home/datasetdetails/5543867320583086178c4f34/>

STREET	FROM	TO	COMPLETE STREET TYPE
<u>E. Allen St</u>	<u>Marlborough St</u>	<u>N. Delaware Ave</u>	<u>Local</u>
<u>Marlborough St</u>	<u>N. Delaware Ave</u>	<u>E. Allen St</u>	<u>City Neighborhood</u>
<u>N. Delaware Ave</u>	<u>E. Marlborough St</u>	<u>E. Allen St</u>	<u>Urban Arterial</u>

11. Does the **Existing Conditions** site survey clearly identify the following existing conditions with dimensions?
- | | | | |
|---|---|-----------------------------|------------------------------|
| a. Parking and loading regulations in curb lanes adjacent to the site | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | |
| b. Street Furniture such as bus shelters, honor boxes, etc. | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> |
| c. Street Direction | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | |
| d. Curb Cuts | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> |
| e. Utilities, including tree grates, vault covers, manholes, junction boxes, signs, lights, poles, etc. | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> |
| f. Building Extensions into the sidewalk, such as stairs and stoops | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> |

DEPARTMENTAL REVIEW: General Project Information

APPLICANT: General Project Information

Additional Explanation / Comments:



COMPLETE STREETS CHECKLIST

1212-16 N. DELAWARE AVE | CIVIC DESIGN REVIEW | PAGE 40

COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



PEDESTRIAN COMPONENT (Handbook Section 4.3)

12. SIDEWALK: list Sidewalk widths for each street frontage. Required Sidewalk widths are listed in Section 4.3 of the Handbook.

STREET FRONTAGE	TYPICAL SIDEWALK WIDTH (BUILDING LINE TO CURB)	CITY PLAN SIDEWALK WIDTH
	Required / Existing / Proposed	Existing / Proposed
<u>E. Allen St</u>	<u>10' / 10' / 10'</u>	<u>10' / 10'</u>
<u>Marlborough St</u>	<u>12' / 12' / 12'</u>	<u>12' / 12'</u>
<u>N. Delaware Ave</u>	<u>12' / 16' / 16'</u>	<u>16' / 16'</u>
_____	____ / ____ / ____	____ / ____

13. WALKING ZONE: list Walking Zone widths for each street frontage. The Walking Zone is defined in Section 4.3 of the Handbook, including required widths.

STREET FRONTAGE	WALKING ZONE
	Required / Existing / Proposed
<u>E. Allen St</u>	<u>5' / 5.5' Min. / 6' Min.</u>
<u>Marlborough St</u>	<u>6' / 6.7' Min. / 8' Min.</u>
<u>N. Delaware Ave</u>	<u>8' / 8.6' Min. / 12' Min.</u>
_____	____ / ____ / ____

14. VEHICULAR INTRUSIONS: list Vehicular Intrusions into the sidewalk. Examples include but are not limited to; driveways, lay-by lanes, etc. Driveways and lay-by lanes are addressed in sections 4.8.1 and 4.6.3, respectively, of the Handbook.

EXISTING VEHICULAR INTRUSIONS

INTRUSION TYPE	INTRUSION WIDTH	PLACEMENT
<u>Curb Cut</u>	<u>24'</u>	<u>N. Delaware Ave</u>
<u>Curb Cut</u>	<u>11.5'</u>	<u>E. Allen St</u>
_____	_____	_____
_____	_____	_____

PROPOSED VEHICULAR INTRUSIONS

INTRUSION TYPE	INTRUSION WIDTH	PLACEMENT
<u>Curb Cut</u>	<u>24'</u>	<u>E. Allen St</u>
_____	_____	_____
_____	_____	_____
_____	_____	_____

COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



PEDESTRIAN COMPONENT (continued)

15. When considering the overall design, does it create or enhance a pedestrian environment that provides safe and comfortable access for all pedestrians at all times of the day?

YES NO

DEPARTMENTAL
APPROVAL

YES NO

APPLICANT: Pedestrian Component

Additional Explanation / Comments: _____

DEPARTMENTAL REVIEW: Pedestrian Component

Reviewer Comments: _____



COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



BUILDING & FURNISHING COMPONENT (Handbook Section 4.4)

16. BUILDING ZONE: list the MAXIMUM, existing and proposed Building Zone width on each street frontage. The Building Zone is defined as the area of the sidewalk immediately adjacent to the building face, wall, or fence marking the property line, or a lawn in lower density residential neighborhoods. The Building Zone is further defined in section 4.4.1 of the Handbook.

STREET FRONTAGE	MAXIMUM BUILDING ZONE WIDTH	
	Existing	Proposed
<u>E. Allen St</u>	4.5'	0'
<u>Marlborough St</u>	5.4'	0'
<u>N. Delaware Ave</u>	2.8'	0'
_____	_____	_____

17. FURNISHING ZONE: list the MINIMUM, recommended, existing, and proposed Furnishing Zone widths on each street frontage. The Furnishing Zone is further defined in section 4.4.2 of the Handbook.

STREET FRONTAGE	MINIMUM FURNISHING ZONE WIDTH		
	Recommended	Existing	Proposed
<u>E. Allen St</u>	3.5'	3.5'	4'
<u>Marlborough St</u>	4'	4'	4'
<u>N. Delaware Ave</u>	4'	4'	4'
_____	_____	_____	_____

18. Identify proposed "high priority" building and furnishing zone design treatments that are incorporated into the design plan, where width permits (see Handbook Table 1). Are the following treatments identified and dimensioned on the plan?

- Bicycle Parking YES NO N/A
- Lighting YES NO N/A
- Benches YES NO N/A
- Street Trees YES NO N/A
- Street Furniture YES NO N/A

19. Does the design avoid tripping hazards? YES NO N/A

20. Does the design avoid pinch points? Pinch points are locations where the Walking Zone width is less than the required width identified in item 13, or requires an exception YES NO N/A

DEPARTMENTAL APPROVAL

- YES NO
- YES NO
- YES NO
- YES NO
- YES NO
- YES NO

COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



BUILDING & FURNISHING COMPONENT (continued)

21. Do street trees and/or plants comply with street installation requirements (see sections 4.4.7 & 4.4.8) YES NO N/A YES NO

22. Does the design maintain adequate visibility for all roadway users at intersections? YES NO N/A YES NO

APPLICANT: Building & Furnishing Component
Additional Explanation / Comments:

DEPARTMENTAL REVIEW: Building & Furnishing Component
Reviewer Comments:



COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



BICYCLE COMPONENT (Handbook Section 4.5)

23. List elements of the project that incorporate recommendations of the Pedestrian and Bicycle Plan, located online at <http://phila2035.org/wp-content/uploads/2012/06/bikePedfinal2.pdf>

The proposed development includes pedestrian zones larger than what is required and furnishing zones equal to or greater than what is required. Additionally, ADA curb ramps are proposed at each fronting intersection to ensure ADA-accessibility to the project.

24. List the existing and proposed number of bicycle parking spaces, on- and off-street. Bicycle parking requirements are provided in The Philadelphia Code, Section 14-804.

BUILDING / ADDRESS	REQUIRED SPACES	ON-STREET Existing / Proposed	ON SIDEWALK Existing / Proposed	OFF-STREET Existing / Proposed
1212 N. Delaware Ave	21	0 / 0	0 / 0	0 / 21
_____	_____	____/____	____/____	____/____
_____	_____	____/____	____/____	____/____
_____	_____	____/____	____/____	____/____

25. Identify proposed "high priority" bicycle design treatments (see Handbook Table 1) that are incorporated into the design plan, where width permits. Are the following "High Priority" elements identified and dimensioned on the plan?

<ul style="list-style-type: none"> ▪ Conventional Bike Lane ▪ Buffered Bike Lane ▪ Bicycle-Friendly Street ▪ Indego Bicycle Share Station 	YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> N/A <input type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/>	DEPARTMENTAL APPROVAL YES <input type="checkbox"/> NO <input type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/> YES <input type="checkbox"/> NO <input type="checkbox"/>
---	--	---

26. Does the design provide bicycle connections to local bicycle, trail, and transit networks? YES NO N/A DEPARTMENTAL APPROVAL YES NO

27. Does the design provide convenient bicycle connections to residences, work places, and other destinations? YES NO N/A DEPARTMENTAL APPROVAL YES NO

APPLICANT: Bicycle Component

Additional Explanation / Comments: The development fronts N Delaware Avenue, which has semi-protected bike lanes in both directions. Additionally, the Delaware River Trail is across the street and accessible to both pedestrians and bicyclists. The bike lanes within N Delaware Ave lead to various work spaces, recreational destinations, and other residential developments.

DEPARTMENTAL REVIEW: Bicycle Component

Reviewer Comments:

COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



CURBSIDE MANAGEMENT COMPONENT (Handbook Section 4.6)

28. Does the design limit conflict among transportation modes along the curb? YES NO DEPARTMENTAL APPROVAL YES NO

29. Does the design connect transit stops to the surrounding pedestrian network and destinations? YES NO N/A DEPARTMENTAL APPROVAL YES NO

30. Does the design provide a buffer between the roadway and pedestrian traffic? YES NO N/A DEPARTMENTAL APPROVAL YES NO

31. How does the proposed plan affect the accessibility, visibility, connectivity, and/or attractiveness of public transit? DEPARTMENTAL APPROVAL YES NO

APPLICANT: Curbside Management Component

Additional Explanation / Comments: Similar to many city sidewalks in Philadelphia, the design includes an asphalt cartway, 4-8" curb, and varying sidewalk width.

DEPARTMENTAL REVIEW: Curbside Management Component

Reviewer Comments:



COMPLETE STREETS HANDBOOK CHECKLIST

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VEHICLE / CARTWAY COMPONENT (Handbook Section 4.7)

32. If lane changes are proposed, identify existing and proposed lane widths and the design speed for each street frontage;

STREET	FROM	TO	LANE WIDTHS Existing / Proposed	DESIGN SPEED
n/a	_____	_____	_____/____	_____
n/a	_____	_____	_____/____	_____
n/a	_____	_____	_____/____	_____
n/a	_____	_____	_____/____	_____

- | | | |
|---|--|---|
| 33. What is the maximum AASHTO design vehicle being accommodated by the design? | <u>19' Pedestrian Vehicle</u> | DEPARTMENTAL APPROVAL
YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 34. Will the project affect a historically certified street? An inventory of historic streets ⁽¹⁾ is maintained by the Philadelphia Historical Commission. | YES <input type="checkbox"/> NO <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 35. Will the public right-of-way be used for loading and unloading activities? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 36. Does the design maintain emergency vehicle access? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 37. Where new streets are being developed, does the design connect and extend the street grid? | YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 38. Does the design support multiple alternative routes to and from destinations as well as within the site? | YES <input type="checkbox"/> NO <input type="checkbox"/> N/A <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 39. Overall, does the design balance vehicle mobility with the mobility and access of all other roadway users? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |

APPLICANT: Vehicle / Cartway Component
Additional Explanation / Comments: _____

DEPARTMENTAL REVIEW: Vehicle / Cartway Component
Reviewer Comments: _____

(1) http://www.philadelphiastreet.com/images/uploads/documents/Historical_Street_Paving.pdf

COMPLETE STREETS HANDBOOK CHECKLIST

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URBAN DESIGN COMPONENT (Handbook Section 4.8)

- | | | |
|--|--|---|
| 40. Does the design incorporate windows, storefronts, and other active uses facing the street? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/> | DEPARTMENTAL APPROVAL
YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 41. Does the design provide driveway access that safely manages pedestrian / bicycle conflicts with vehicles (see Section 4.8.1)? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 42. Does the design provide direct, safe, and accessible connections between transit stops/stations and building access points and destinations within the site? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |

APPLICANT: Urban Design Component
Additional Explanation / Comments: _____

DEPARTMENTAL REVIEW: Urban Design Component
Reviewer Comments: _____



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INTERSECTIONS & CROSSINGS COMPONENT (Handbook Section 4.9)

43. If signal cycle changes are proposed, please identify Existing and Proposed Signal Cycle lengths; if not, go to question No. 48.

SIGNAL LOCATION	EXISTING CYCLE LENGTH	PROPOSED CYCLE LENGTH
n/a	_____	_____
n/a	_____	_____
n/a	_____	_____
n/a	_____	_____

- | | YES | NO | N/A | DEPARTMENTAL APPROVAL |
|---|---|-------------------------------------|---|--|
| 44. Does the design minimize the signal cycle length to reduce pedestrian wait time? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 45. Does the design provide adequate clearance time for pedestrians to cross streets? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 46. Does the design minimize pedestrian crossing distances by narrowing streets or travel lanes, extending curbs, reducing curb radii, or using medians or refuge islands to break up long crossings?
<i>If yes, City Plan Action may be required.</i> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 47. Identify "High Priority" intersection and crossing design treatments (see Handbook Table 1) that will be incorporated into the design, where width permits. Are the following "High Priority" design treatments identified and dimensioned on the plan? | | | | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| ▪ Marked Crosswalks | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| ▪ Pedestrian Refuge Islands | YES <input type="checkbox"/> | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| ▪ Signal Timing and Operation | YES <input type="checkbox"/> | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| ▪ Bike Boxes | YES <input type="checkbox"/> | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 48. Does the design reduce vehicle speeds and increase visibility for all modes at intersections? | YES <input type="checkbox"/> | NO <input type="checkbox"/> | N/A <input checked="" type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 49. Overall, do intersection designs limit conflicts between all modes and promote pedestrian and bicycle safety? | YES <input checked="" type="checkbox"/> | NO <input type="checkbox"/> | N/A <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |

APPLICANT: Intersections & Crossings Component
Additional Explanation / Comments: _____

DEPARTMENTAL REVIEW: Intersections & Crossings Component
Reviewer Comments: _____

COMPLETE STREETS HANDBOOK CHECKLIST

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ADDITIONAL COMMENTS

APPLICANT
Additional Explanation / Comments: _____

DEPARTMENTAL REVIEW
Additional Reviewer Comments: _____

