



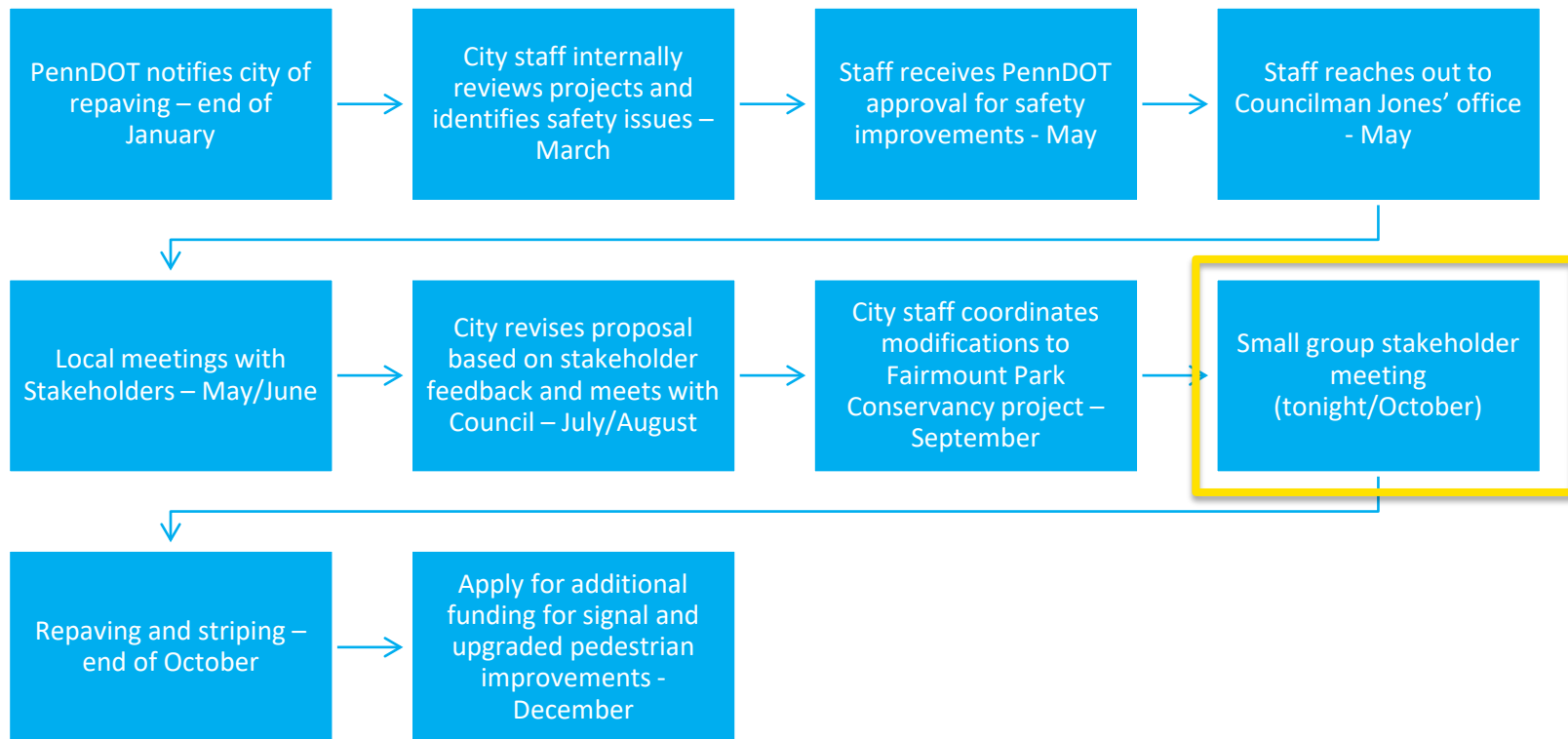
VISION ZERO

CITY OF PHILADELPHIA

Parkside Avenue Safety Project



Project Process



Background



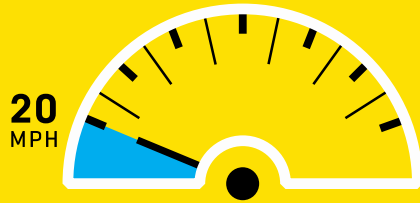
What is Vision Zero?

A policy that states clearly that traffic deaths are preventable and unacceptable.

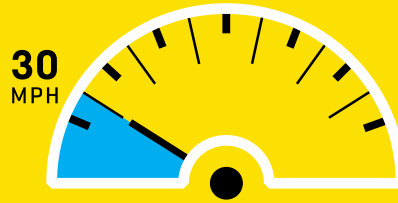
CORE PRINCIPLES:

- Human life takes priority over mobility;
- Human error is inevitable and unpredictable;
- People are inherently vulnerable and speed is a fundamental predictor of crash survival;
- Safe human behaviors, education, and enforcement are essential contributors to a safe system.

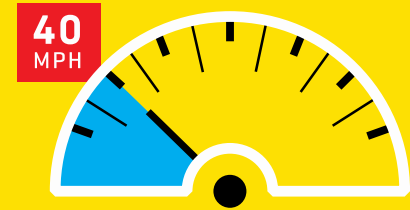
WHEN A PERSON IS HIT BY A DRIVER AT...



1 OUT OF **10** DIE



5 OUT OF **10** DIE



9 OUT OF **10** DIE

Slowing down saves lives.

Why Vision Zero in Philadelphia?

Every year, there are over 10,000 reported crashes in Philadelphia.

5-YEAR TREND:

- 2011: 87 killed / 277 severely injured
- 2012: 107 killed / 291 severely injured
- 2013: 89 killed / 257 severely injured
- 2014: 97 killed / 257 severely injured
- 2015: 94 killed / 275 severely injured

100 PEOPLE EVERY YEAR



100 people are killed in traffic related crashes.

Why Vision Zero in Philadelphia?

Every year, there are over 10,000 reported crashes in Philadelphia.

4 CHILDREN EVERY DAY

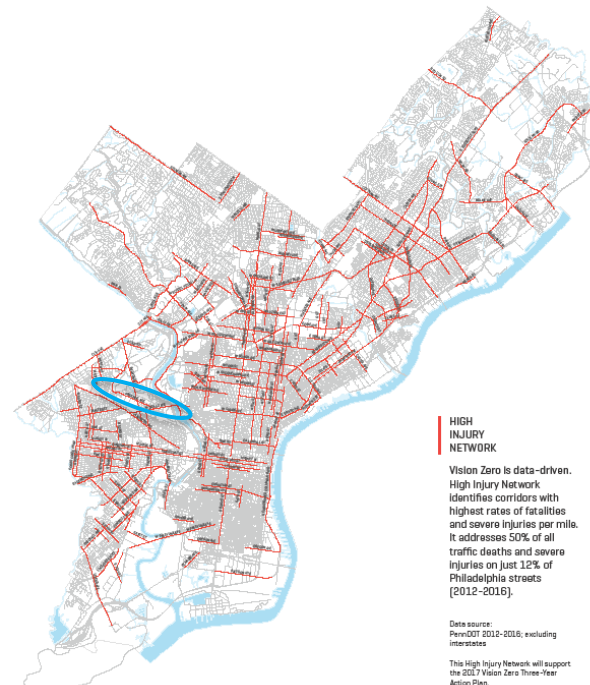


4 children every day are reported to
be involved in traffic crashes.

Vision Zero: High Injury Network



*50% of Killed/Serious
Injury Crashes on 12%
of Streets*



Why is this important?

Crash history



Over 5 years, 2012-2016:

- 147 reportable crashes, 8 people seriously injured
- 1 bicyclist & 2 vehicle passengers killed

Summer 2017:

- **Pedestrian fatality** on Parkside between Bryn Mawr – 53rd
- **Pedestrian (12 year old) serious injury** at 50th & Parkside



**VISION
ZERØ**

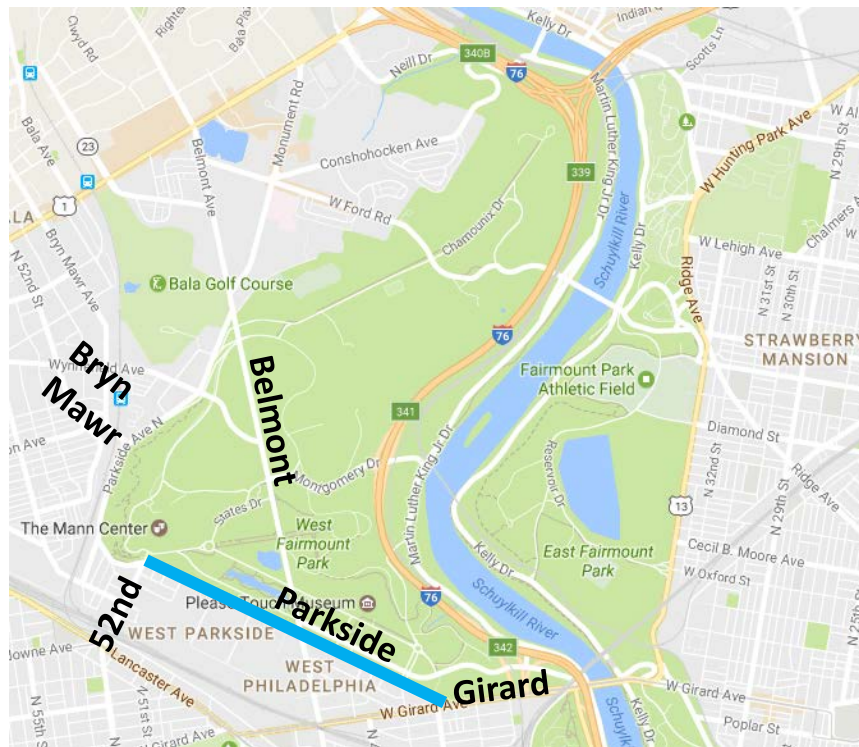
CITY OF PHILADELPHIA

**OUR CITY AND OUR
FAMILIES DESERVE
SAFER STREETS.**

Zero traffic deaths by 2030.

Where is the project area?

- In the East and West Parkside neighborhoods
- Fairmount Park adjacent
- Residential, industrial, and business uses
- 3 schools, many park destinations

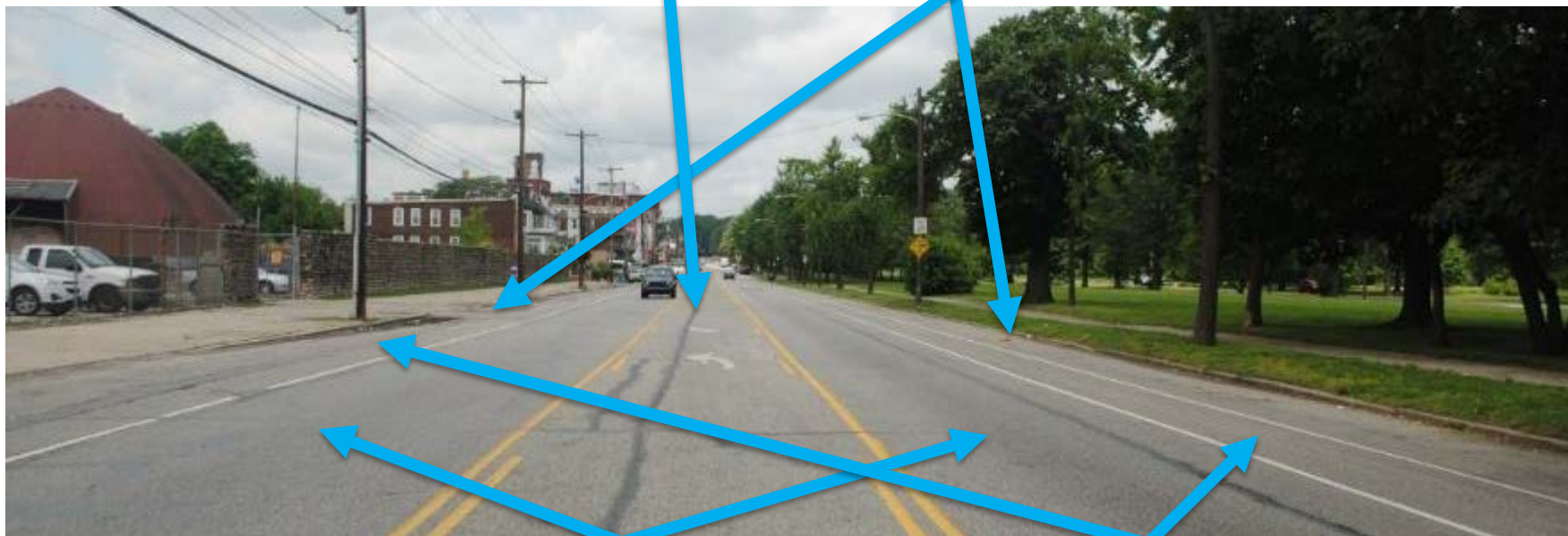


Parkside, 52 - Girard

At 49th, looking west

1 center turn lane

2 parking/loading lanes



2 motor vehicle lanes

2 painted bicycle lanes

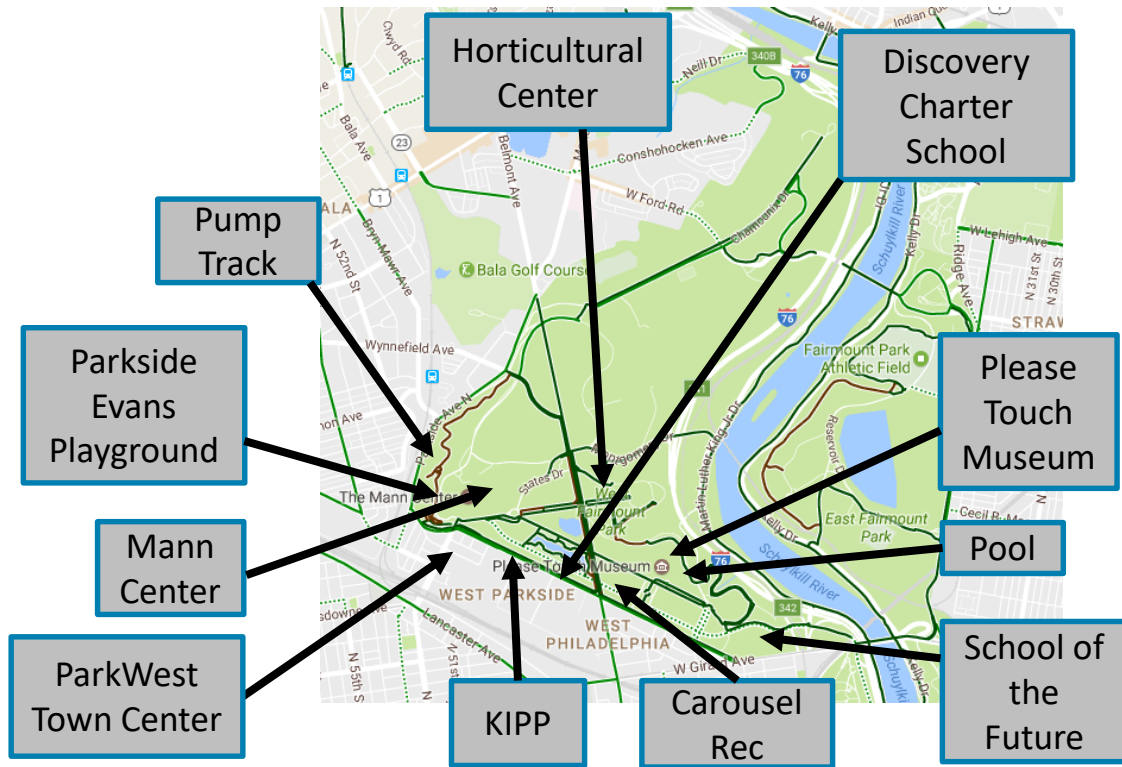
Why is this important?

High pedestrian crossing demand and volumes



Why is this important?

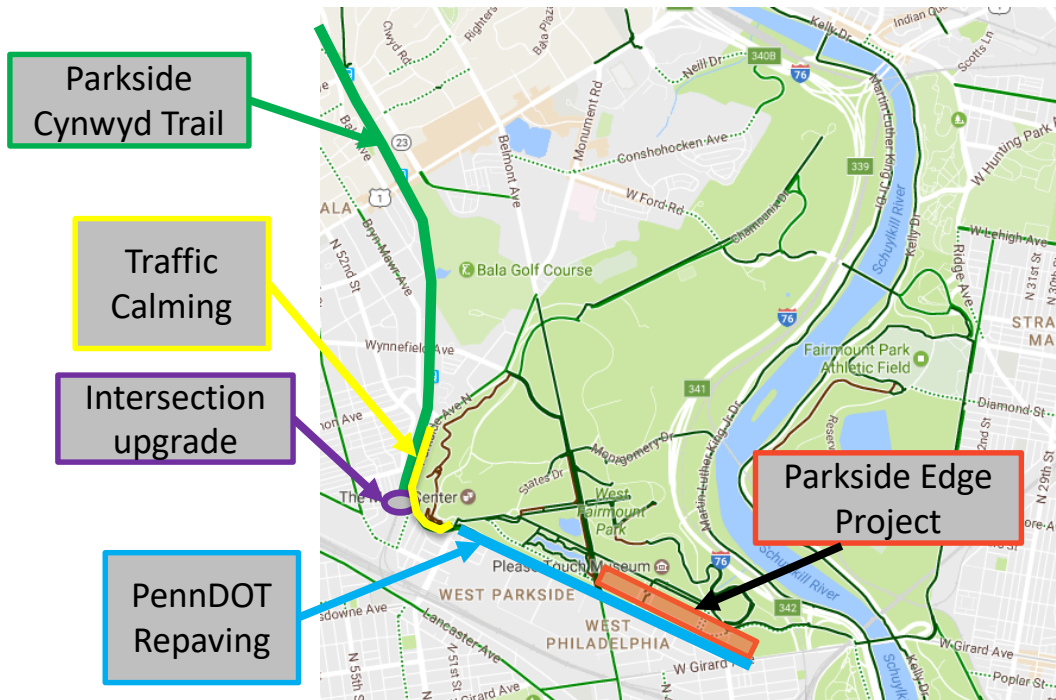
Many community assets



Why now?

Funded new community assets coming soon

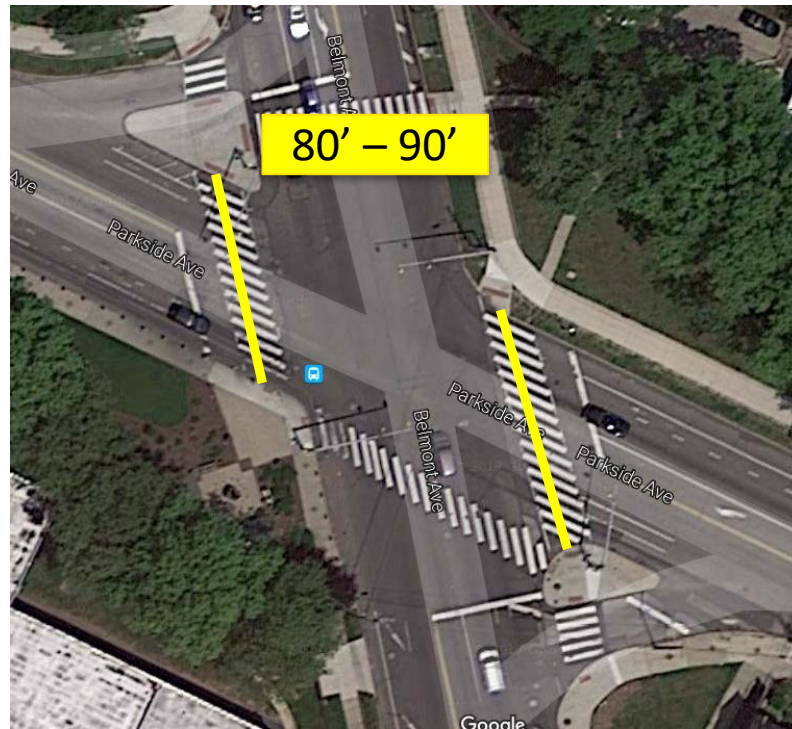
- Parkside Edge Pedestrian Improvements - 2017
- **PennDOT Repaving on Parkside, Girard-52nd**
- Parkside Ave Traffic Calming – 52 – Bryn Mawr
- 53 & Parkside Intersection Upgrade – 2018-2019
- Parkside Cynwyd Trail - 2019+



What are the safety issues?

Long pedestrian crossings

Ideal maximum pedestrian crossing distance = 33 feet or 3 lanes without median



Goals of Parkside Ave Safety Project



Increase pedestrian safety by:

- Shortening pedestrian crossing distances
- Providing painted pedestrian refuge areas

Maintain travel and turn lanes throughout the project area

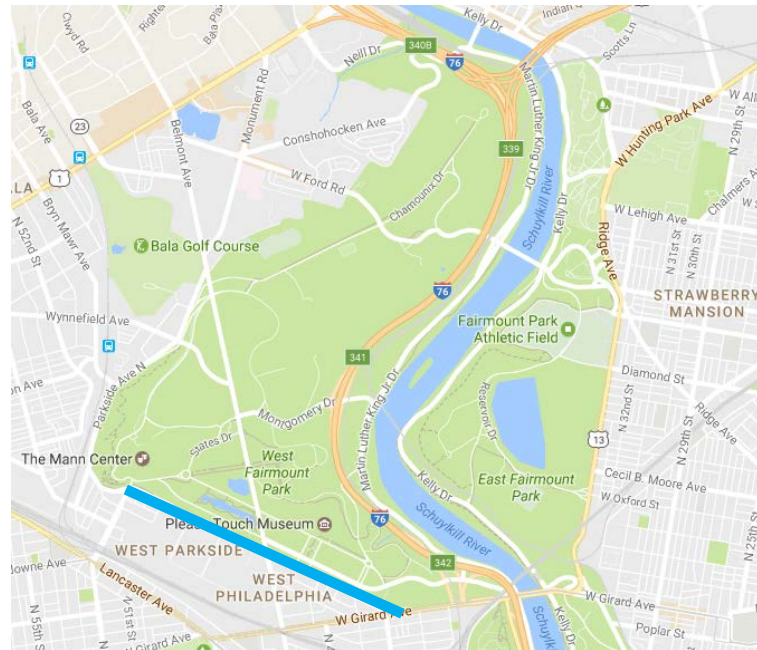
Provide 1 + miles of **new protected bicycle lanes**



What is proposed?

Area of Discussion:

- PennDOT Repaving Project, Girard – 52nd



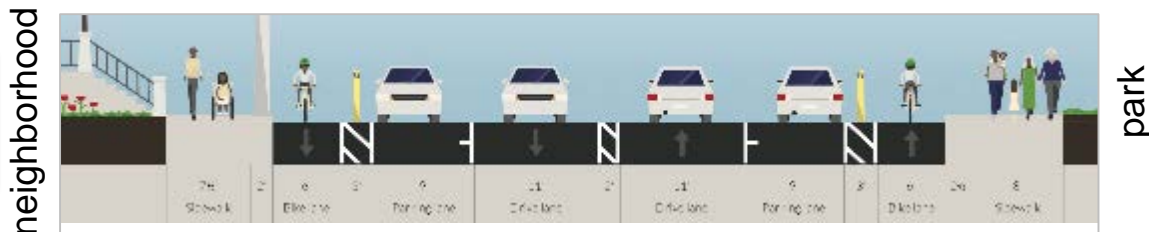
What is proposed?

PennDOT Repaving Project,
Girard – 52nd

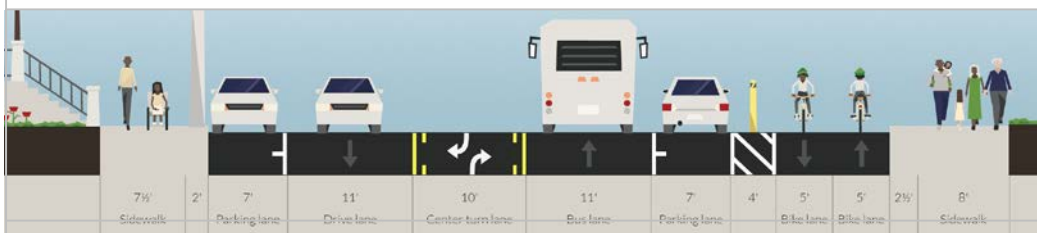
Existing Parkside
cross section



Proposed cross
section
May 2017



REVISED
Proposed cross
section
October 2017



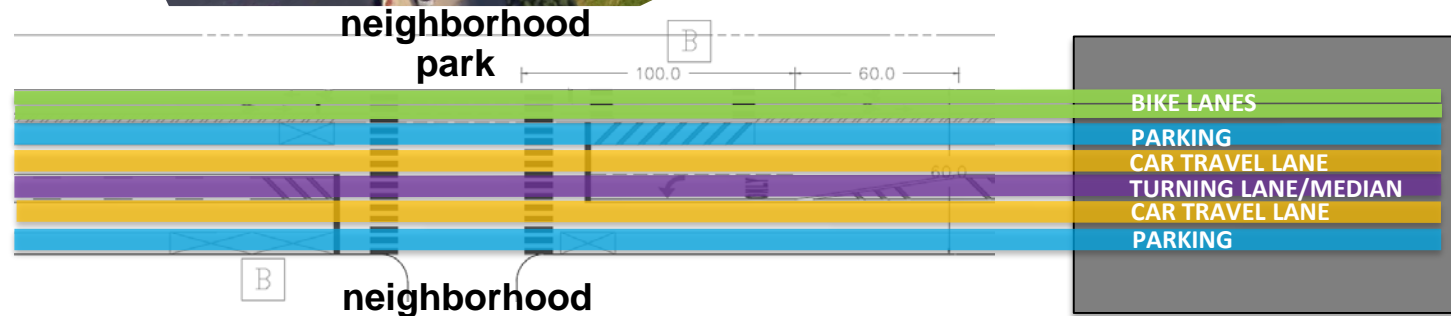
What is proposed?

PennDOT Repaving Project,
Girard – 52nd

NOW



PROPOSED



What is proposed?

PennDOT Repaving Project, Girard – 52nd

Philadelphia
Ryan Avenue



Examples in other cities



Mid-Block, Typical

REVISED
Proposed cross
section
October 2017

neighborhood



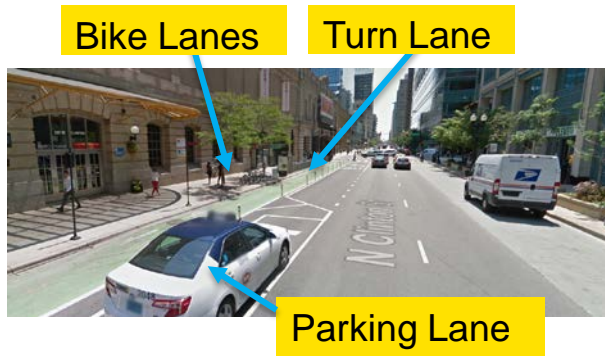
park

What is proposed?

At Bus Stops and Intersections



*Philadelphia
Chestnut Street*



Bike Lanes

Turn Lane

Parking Lane



Painted Pedestrian
Refuge

REVISED
Proposed cross
section
August 2017

neighborhood



park

What is proposed?

PennDOT Repaving Project,
Girard – 52nd



Bikeway Shifts
at Bumpout

At Parkside Edge Bumpouts/Median

REVISED
Proposed cross
section
August 2017

neighborhood



park

Summary

Benefits of new proposal:

- Adjusted design based on community feedback
- Curb side parking on neighborhood side
- Increased pedestrian safety by:
 - Shortening pedestrian crossing distances
 - Providing painted pedestrian refuge areas
- 2 travel lanes throughout the project area
- Provide 1.7 miles of new protected bicycle lanes

Next Steps

PennDOT Paving Project: End of October 2017

Potential Improvements for Future Funding Applications:

- Concrete Islands at Pedestrian Refuges
- Bus Stop Islands
- Traffic Signal at 51st Street



Thank you



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