



# Logan Square Neighborhood - Parkway Plan

PREPARED FOR  
LOGAN SQUARE NEIGHBORHOOD ASSOCIATION  
IN PARTNERSHIP WITH  
PHILADELPHIA CITY PLANNING COMMISSION

MARCH 10, 2009

PREPARED BY  
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## Greetings friends and stakeholders of the Logan Square Neighborhood–Parkway!

The concept of a comprehensive plan for the Logan Square neighborhood was first made more than five years ago. In 2006 a Long Range Planning Committee was established and a group of dedicated neighbors met and devoted considerable time learning about other neighborhood planning efforts and weighing options for pursuing a vibrant and effective process. Ultimately, the Logan Square Neighborhood - Parkway Plan Steering Committee was formed and over the course of two years, local residents, businesses, institutions, the City and a myriad of stakeholders met and worked diligently to develop the Logan Square Neighborhood–Parkway Plan (LSN-PP). There were numerous hurdles along the way, from deciding whether to even undertake the process to searching for funding, to forging a partnership with the City Planning Commission to choosing planning, traffic and economic consultants through an extensive RFP process, to organizing meetings reaching across all of the stakeholders. The goal was, and is, to create a vision that will help guide the development and the operation of the neighborhood, over the next decade and beyond, that both recognizes the positives we

want to retain while providing a roadmap for future enhancements and developments in the area.

Throughout, we worked hard to create an open, inclusive, and collaborative process where all voices, big and small, would be heard. We believe that this major effort can serve as a great model for how other neighborhoods of Philadelphia can undertake planning and that a number of the recommendations coming out of the LSN-PP can be applied on a broader citywide basis, even informing the ongoing Zoning Code reform efforts.

Like most neighborhoods, the Logan Square neighborhood (defined as the area bounded by Market Street to the South, Spring Garden Street to the North, Broad Street to the East and the Schuylkill River to the West) continues to experience change, although recently the pace has had many concerned about the preservation of the great livable and sustainable aspects and character of the area. Too often, new developments and plans have not taken into account the existing strengths (e.g., a diverse walkable family-friendly community with great amenities) and wants of the neighborhood. And too often, it has been easy for residents to say what they don't like about a

## PROLOGUE



development without a clear vision to help promote what they want. It was a cascade of large scale projects such as the Barnes Tower (22<sup>nd</sup> & Pennsylvania Avenue) and “River City” (along JFK Boulevard and the River) that served to make this clear and spur the creation of a Plan. The LSN-PP is designed to better express the existing strengths of the area and provide a vision and direction for its future development and improvement.

As many recognize, the Logan Square neighborhood holds a special place in Philadelphia as its “front yard” where an eclectic mix of residential (both traditional row homes and contemporary high rises), commercial, office, educational institutions, parks, and the region’s core cultural and arts institutions all coexist. This eclectic mix is a blessing that makes the area so vital, but also makes planning a major challenge. After consulting with the Planning Commission and nearby neighborhoods, it became clear that the neighborhood needed to engage professional planners. Another challenge was to develop a comprehensive plan that would connect to other ongoing efforts along the Parkway, Riverfront, adjacent communities and major developments already in the pipeline. The Logan Square Neighborhood-Parkway Plan Steering

Committee was formed in late 2006 in order to carry out this work.

Another key step along the way was the partnership developed with the City’s Planning Commission. And although we may not always be in agreement on all issues, our level of collaboration has been unprecedented in this community. This partnership was critical because it came with many professional planning resources (in particular, Community Planner Laura Spina and Transportation Planner Deborah Schaaf), the ability to bring other stakeholders to the table, the contribution of funding, and use the power of the City to move the Plan towards implementation. This was exciting because planning across the City has traditionally been carried out by neighborhood organizations or the City, but not necessarily in such a close partnership. As important was the choice of Kise Straw & Kolodner (KSK) as the lead planning consultant. KSK, led by John Gibbons, brought a great team of professionals who were able to utilize much of their institutional memory in their previous planning work done around Center City and the Parkway.

As an outgrowth of the Planning process, a number of important concepts and planning principles have

been put forth that can potentially be applied to other neighborhoods or to upcoming zoning code revisions. For example, the uses of illustrations and visual examples from existing developments were critical to the understanding of the Zoning Code and the use of Floor Area Ratio (FAR) bonus changes that might replace existing height restrictions. A key outcome of this conversation was the recommendation that in the context (i.e., contextual zoning) of transitional parcels that are between denser areas and less dense areas, existing zoning district controls must be modified to assure that new development does not overwhelm existing development. Other important components of the Plan are the examination of the use of a Conservation District within the core residential areas of Logan Square, the development of Callowhill Street as a neighborhood oriented retail corridor, and the promotion of sustainable development approaches within Logan Square that might be applied elsewhere.

While the development of this Plan was an accomplishment itself, the critical important work in its implementation is still to be done. Some of the early action and priority items will happen soon while other items are longer-term goals. With the help of

many who helped to create the Plan and others who support its recommendations, we hope that the Plan will make a significant, positive difference in the future of Logan Square and the City.

### Thanks

There are many people and organizations that participated in this effort and many to thank for assistance. First, the Philadelphia City Planning Commission was essential to the Plan. Thanks begin with previous Executive Director, Janice Woodcock and current Executive Director, Alan Greenberger, as well as Gary Jastzrab, Rick Redding, Brian Flanagan, Deborah Schaaf, many other Commission staff and a special thanks to Laura Spina. The Planning Commission and Commerce Department (Duane Bumb) also provided funding that allowed the Plan to move forward. And thanks to Councilman Darrell Clarke for his support and attendance at our Community Forum.

Three other critical supporters at the beginning and throughout were the Delaware Valley Regional Planning Commission (special thanks to Executive



Director, Barry Seymour and Ted Dahlburg), State Representative Babette Josephs and the Society Created to Reduce Urban Blight (Executive Director, Mary Tracey). Important advice and information that aided the LSN-PP Steering Committee to decide upon a direction and to develop Requests For Proposals (RFPs) came from the Center City Residents Association (Lou Coffey) and Fairmount CDC (Lori Salganicoff). Meeting space for the Community Forums and Advisory Committee meetings was provided by the Moore College of Art (President, Happy Fernandez and Emily Johnson), Community College (Randolph Merced and Monique Evans-Wescott), and the Science Leadership Academy (Principal, Chris Lehman). We would also like to thank Whole Foods for providing refreshments for the meetings.

And thanks again to KSK (John Gibbons, Doug Robbins, Elizabeth Burling, Liz Lankenau, Jody Holton, and Shawn Rairigh) and partners (including Frank Jaskiewicz, Jim Hartling and Chris Lankenau) for going above the call of duty to make the planning process and outcome a success. John Gibbons, who led the team, should receive special recognition for working through many difficult issues and helping

to keep the Plan moving forward with a steady and experienced hand.

Finally, the LSN-PP Steering Committee spent countless hours and effort in stewarding the process and providing a high level of input and feedback at every step of the way. A special thanks to all on the Steering Committee (listed below).

We appreciate all the individuals and organizations who have contributed their ideas, time, and reflections in this process. Now we encourage everyone to remain committed to the next steps in implementing many of the bold recommendations contained in the plan. Clearly, we see the publication of this plan as a milestone, not as an end but rather as a beginning. Going forward, neighbors, planners, developers, and representatives will have a consistent framework from which to work, plan, live and dream. Logan Square is already one of the most special neighborhoods in world, but we are confident that this plan will make it even better. Thanks again for your support of our Plan and our community!

Andrew Toy, Chair, LSN-PP Steering Committee  
Rob Stuart, President, LSNA Board

The Logan Square Neighborhood - Parkway Plan represents a remarkable effort by a local civic association to articulate a vision for development in their neighborhood. Many hundreds of hours have been contributed by members of the community, working in parallel with the Philadelphia City Planning Commission, to analyze trends, understand the possible implications of existing public policy, and make suggestions that could help guide future development.

Unlike many neighborhoods, Logan Square - Parkway not only encompasses vibrant residential areas, but also includes part of the downtown core of the city as well as our great civic boulevard - the Benjamin Franklin Parkway - and its many cultural institutions. These special circumstances have placed great demands on the plan's authors to recognize needs beyond those of the residents. They have shown great sensitivity to these needs in a struggle to balance many competing interests that would impact any vibrant downtown.

Achieving that best balance is very hard work. Participants in the planning process have never shied away from this effort and are to be commended for it. Inevitably, there are some disagreements between the Plan's authors and the Planning Commission on specific recommendations for specific sites. Rather than gloss over these differences, they are noted in the plan.

However, the small number of differences does nothing to take away from the otherwise extraordinary level of cooperation and collaboration that is evidenced in this document. As the agency responsible for planning in the city, we value these efforts and are committed to working with the Logan Square Neighborhood Association to help implement the many recommendations that we all support.

Alan Greenberger  
Executive Director  
Philadelphia City Planning Commission

## MESSAGE FROM PCPC







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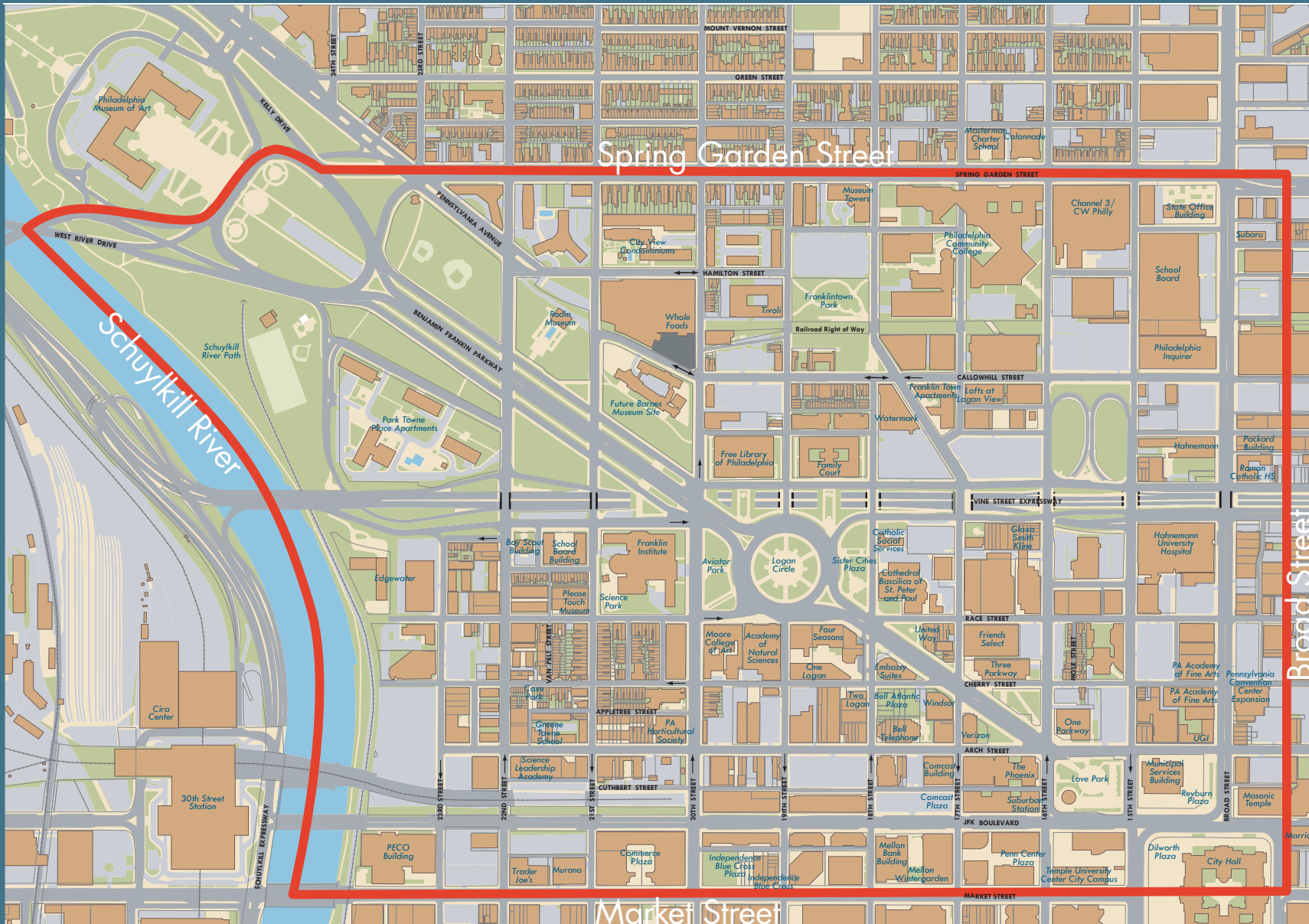
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1

# INTRODUCTION





## 1.1 Introduction

The Logan Square Neighborhood - Parkway occupies the northwest quadrant of Center City, and extends from the Schuylkill River in the west to Broad Street in the east, and from Spring Garden Street in the north, to Market Street in the south (*See Map 1*). It includes several distinct areas:

### AREAS WITHIN LOGAN SQUARE

1. The Center City commercial office core in the southeast and the south
2. The historic Logan Square residential rowhouses neighborhood in the southwest
3. The Benjamin Franklin Parkway and Logan Square, with their world-class arts and science related institutions
4. The Parkway North residential area, with a mix of high-rise residential and townhouse developments in the northwest
5. The Community College institutional area in the northeast
6. The Schuylkill River Bank and trail

◀ **Map 1. Boundaries of the Logan Square-Parkway District**

1.



2.



3.



4.



5.



6.



Several parts of the Logan Square Neighborhood are likely to undergo significant transformation over the next decade. Perhaps more than any other neighborhood in Center City, Logan Square-Parkway has a number of large-scale development opportunity sites that could significantly change the character of the community and the skyline of the city as a whole. Many of these sites are zoned for high-rise, high-density development. While these changes can have a positive effect on the neighborhood, it is important that the existing residential areas be protected and enhanced. In addition, there are several developments of national and international stature proposed for the Benjamin Franklin Parkway, including the Barnes Museum and Free Library addition, that could reinvigorate the Parkway as an international visitor destination.

The Logan Square Neighborhood-Parkway Plan articulates a shared vision for the Logan Square neighborhood, including the Benjamin Franklin Parkway. The Plan is unique in the way it has brought together the numerous and varied constituencies that comprise the Logan Square-Parkway community.

The partnership entered into by the Philadelphia City Planning Commission and the Logan Square Neighborhood Association to create the Plan is unprecedented in nature and was designed to facilitate implementation by anticipating realities and working through differences early in the process. Plan participants recognize that implementation will require ongoing dialogue, a dynamic balancing of the interests and needs of divergent constituencies, and the willingness of Logan Square's many stakeholders to compromise with one another. Above all, the Plan represents a concerted effort to be as inclusive as possible, to improve the communication among all constituencies, and to achieve the Plan's incorporation into the larger planning effort now underway for Philadelphia while conserving the identity and quality of the Logan Square Neighborhood - Parkway.

The Plan reflects the commitment of the diverse but unified members of the Logan Square-Parkway community to achieve the following broad objectives:

- Maintain and enhance the Logan Square

Neighborhood - Parkway's status as a premier place to live, work, and visit, providing an optimal location for enjoying Philadelphia's rich cultural heritage and urban amenities, living and raising a family, and growing a successful business;

- Protect the dignity and accessibility of Logan Square's public spaces, in particular those along the Benjamin Franklin Parkway;
- Preserve, expand, and further beautify the green and open parkland of the Logan Square Neighborhood - Parkway;
- Achieve a sustainable neighborhood that avoids depletion of energy and natural resources, and creates an environment that is livable, comfortable, and safe.
- Support and promote the commercial and cultural institutions located in the Logan Square Neighborhood - Parkway, recognizing their collective role as a major economic engine of the City and region;

- Preserve and enhance the existing predominantly residential/mixed-use "urban villages" of the Logan Square Neighborhood - Parkway, while also recognizing the need for density and commercial development in this downtown area; and
- Improve the mobility and transportation amenities of the Logan Square Neighborhood - Parkway by creating more and better alternatives to automobile use, rationalizing parking demand and availability, and exploring how transportation facilities can be reshaped to meet connectivity goals.





▲ Image 1. Community Forum: January 9th, 2008

## 2

## PLANNING PROCESS AND COMMUNITY PARTICIPATION

## 2.1 Planning Process

The plan was developed in close cooperation with neighborhood residents and community stakeholders. Neighborhood issues, Planning Principles, and Recommendations were identified and refined through a number of meetings with a project Steering Committee, a broader-based Advisory Committee, public forums, and stakeholder meetings. The Logan Square Neighborhood Association also conducted an online resident survey.

The Project Steering Committee included board members of the Logan Square Neighborhood Association, representatives of key residential developments, such as Kennedy House and City View Condominiums, and the Philadelphia City Planning Commission staff.

The Project Advisory committee included a broad range of public, private, and non-profit entities with an interest in the Logan Square-Parkway area, including civic groups, institutions, cultural organizations, businesses, developers, transportation

interests, and government agencies. A full list of invited Advisory Committee invitees is included in the acknowledgements. Advisory Committee meetings were held at each stage of the planning process. Each Advisory Committee meeting was followed by a public forum to obtain broader feedback.

The first Advisory Committee meeting, to review Existing Conditions and Issues, was held on January 7, 2008, and was followed by a Community Forum on January 9, 2008 (*Image 1*). The second Advisory Committee meeting, to review Planning Principles and Goals was held on April 24, 2008 and was followed by a second public forum on May 6, 2008. A third Advisory Committee meeting, to review detailed Recommendations and discuss Priorities was held on September 15, 2008. The final public meeting was held on October 27, 2008. Input from each of the meetings was incorporated in the final document.

## 2.2 Stakeholder Meetings

In addition to broad-based Advisory Committee meetings and public forums, interviews were conducted with representatives of key interests in the community. Staff from the Philadelphia City Planning Commission, and Kise Straw & Kolodner, with members of the steering committee, interviewed over twenty stakeholders in the Logan Square neighborhood. The stakeholders came from a broad cross-section of neighborhood institutions and property-owners, including academic facilities, cultural institutions, residents, government agencies, non-profit organizations, philanthropic foundations, and religious institutions. Through the discussions several common themes and issues emerged:

### Neighborhood Issues

Stakeholders value the diversity and cultural richness of the neighborhood. The collection of museums and institutions in a “museum mile” makes this one of the most unique areas in the country. This concentration

of culture is a major draw for residents, as well as tourists and visitors. The schools in the area take full advantage of their proximity to the museums.

Although the area has a large number of residents and many of the museums and businesses have evening hours and events, this quadrant of Center City becomes relatively dormant after 5:00 p.m. The lack of restaurants that cater to residents, area workers and tourists was a primary complaint. Also, the neighborhood has few retail services for area residents and workers.

The Benjamin Franklin Parkway is the neighborhood’s greatest asset, serving as the link for many of the cultural institutions as well as the primary green space for Logan Square. However, the Parkway also splits the neighborhood physically and psychologically. Efforts by the Center City District, the Fairmount Park Commission and the City Planning Commission to change the roadway to make it more pedestrian-friendly will help knit the north and south sections of the neighborhood.

*Image 2. Rush hour traffic on 21st Street*







## Transit/Parking Issues

The Logan Square neighborhood is well served by public transit, with only the Philadelphia Museum of Art (PMA) being underserved by the bus system. The Parkway Council Foundation is working to make the PHLASH a permanent tourist-oriented transit alternative running along the Parkway.

Parking emerged as an issue for most of the stakeholders due to the number of residents, visitors, and daily workers parking in the neighborhood. The area has a number of garages and surface parking lots that absorb the cars each day (*Image 2*). As more development comes into the neighborhood, supplanting the parking lots and bringing more visitors and residents, the parking situation will become tighter. Stakeholders made suggestions, including:

- Install better signage directing tourists to parking garages;
- Promote remote parking lots, perhaps in

conjunction with the PHLASH or other public transit;

- Build garages under the library extension, the Barnes Foundation museum, and possibly Eakins Oval;
- Cooperate with Philadelphia Community College to use their parking lots and garages on weekends; and
- Promote alternatives such as bicycling, car share and Transitchek.

## Safety

All stakeholders said that the area is relatively safe. All, however, said that the neighborhood needed better lighting at night. Many commended the Center City District for the installation of more pedestrian-oriented lighting along the Parkway and wished that more of this lighting could be incorporated along the other streets in the neighborhood.

*Image 3. Homeless encampment along the Parkway*



## Homeless

Stakeholders expressed great concern about the disproportionately large chronic street homeless population in the area. The institutions along Broad Street also noted the mental health and parole board facilities at Broad and Arch Streets as potential problems. These circumstances, coupled with the chronic street homeless encampments that have sprung up in various sections of the neighborhood, give the impression to visitors that this may not be a safe neighborhood. The residents noted that the neighborhood has welcomed a number of support services for the chronic street homeless, including a shelter, but that these programs are undermined by religious and other volunteer groups — often from



the suburbs — that come into the neighborhood (and especially to the Parkway) to feed the chronic street homeless. Stakeholders point out that these food distributions are considered undignified and unproductive by social services professionals, impede the use of Logan Square's public spaces by other Philadelphians and visitors, and often leave large amounts of trash in their wake, among other concerns. Stakeholders express frustration that these distributions continue despite the City's efforts over many years to persuade these groups to take their operations indoors and help them do so.

While stakeholders expressed compassion, particularly in these difficult economic times, a large majority was critical of the City's management of the chronic street homeless population, which consists largely of the mentally ill and drug-addicted. Most stakeholders felt that there are excessive restrictions on law enforcement targeting public camping, bathing, sex, and urination/defecation, among other conduct that should not be tolerated. Stakeholders believe that such public behaviors are particularly destructive in this downtown center for culture, tourism, and business.



## Events

The stakeholders felt that a few large parades (such as the Thanksgiving Day Parade) and Parkway-oriented events (like the Parkway Fun Day) are positive, family-oriented events that highlight the beauty of the Parkway and provide opportunities for the institutions and neighborhood to relate to the larger community. Many residents enjoy the festivities leading up to the Thanksgiving Day Parade.

However, for the majority of the events, most of the stakeholders had negative reactions. Snarled traffic, lack of parking, detoured transit, and overall congestion deter residents, tourists, and visitors from using the Parkway and going to the museums when non-museum events occupy the Parkway. The museums suffer because ticket-buying patrons cannot or will not venture into the area. The Philadelphia Museum of Art sees its revenues drop by as much as 75% on weekends where the events close the Parkway. There is little overlap of function between the events and the institutions on the Parkway, as someone running in the 5K to raise money for

cancer typically does not go to the Franklin Institute afterwards. Only two stakeholders, the Cathedral and Library, saw the events as a chance to raise their visibility among the community, although the church acknowledged that on Sundays, their attendance also suffered. Even the stakeholders that are not directly affected by events because they are closed on the weekends suffer when trash and graffiti are left after parades or events, which is often the case.

The stakeholders agreed that most of the events should be moved off of the Parkway and that the Parkway should not be used as a fairground. As one stakeholder put it - too many events get prestige, visibility, and cachet by being on the Parkway without giving anything back to the neighborhood or the institutions that suffer because of the events.

From a conservation point of view, both the Fairmount Park Commission and Pennsylvania Horticultural Society expressed concern for the landscape. The thousands of people who use the Parkway each year cause visible stress to the trees, grass, and other landscaping along the Parkway.

*Image 4. 5K Run on the Parkway*









## Development

Some institutions, like the Community College of Philadelphia and Hahnemann University Hospital, have small expansion plans in their future. More significantly, developers have proposed mixed-use tower developments at 16th and Vine Streets and in the southwest corner of the neighborhood. The residents expressed concern that any new development must be sympathetic to the historic character and diverse nature of the existing neighborhood.

Along the Parkway, the Barnes is planning to break ground soon for its new facility, and the Free Library will be developing its new addition across the street. Many of the stakeholders discussed possible development further west on the Parkway. Most believed that some improvements would be advantageous to help enliven the pedestrian experience and to link Logan Square with the PMA. The Center City District envisions permanent, mid-rise development of residential and perhaps commercial buildings. The vast majority of the

stakeholders and community members, however, believe that the development should be more small-scale, such as pavilions or kiosks to house cafes or site-appropriate retail vending, such as bookstalls. The Fairmount Park Commission and the PMA feel strongly, as do most residents, that the historic viewshed of the PMA and the ceremonial nature of the Parkway must be preserved.



*Image 5. Small scale cafe development along the Parkway* ▲

## Open Space - Recreation

Although the Logan Square neighborhood is the gateway to Fairmount Park, many of the stakeholders believe that little of the open space is well programmed or used. The renovations to Swann Fountain and Aviator Park, and improved pedestrian access around Logan Square have made those spaces more user-friendly, especially to workers and visitors. Many are looking forward to similar upgrades to Sister Cities Park. The Pennsylvania Academy of Fine Arts (PAFA) would like to see a link from the Parkway to Broad Street, specifically along Cherry Street, to extend this open space eastward.

The Schuylkill River Trail receives a lot of use from neighborhood residents and the improved Race Street access will be welcomed. Residents also worked for improvements to Coxe Park, a small pocket park that has play equipment. In the northern section, residents had hoped to change some aspects of Franklin Town Park but, as an art installation requirement for the redevelopment of the area, it cannot be altered.

Despite all of the open space in the neighborhood, residents and some workers would like to see some more programmed recreational space, such as tot lots, dog runs and tennis courts. Also, the neighborhood schools share many amenities with local institutions but lack a soccer field. The Horticultural Society noted that designed landscaping is also missing in much of the neighborhood. It could be used as both a welcoming tool and a method to link spaces, especially along the north-south corridors within the area.

Overall, the many stakeholders have shared concerns and similar desires for the Logan Square neighborhood. All of the stakeholders envisioned a more-lively, better-connected, pedestrian-friendly neighborhood that bustled with activity at night as much as it does during the day.







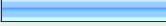
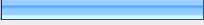



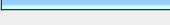
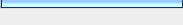
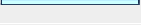
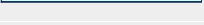
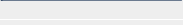

▲ *Image 6. Opportunity for recreational development adjacent the Vine Street Expressway.*

## 2.3 Resident Survey

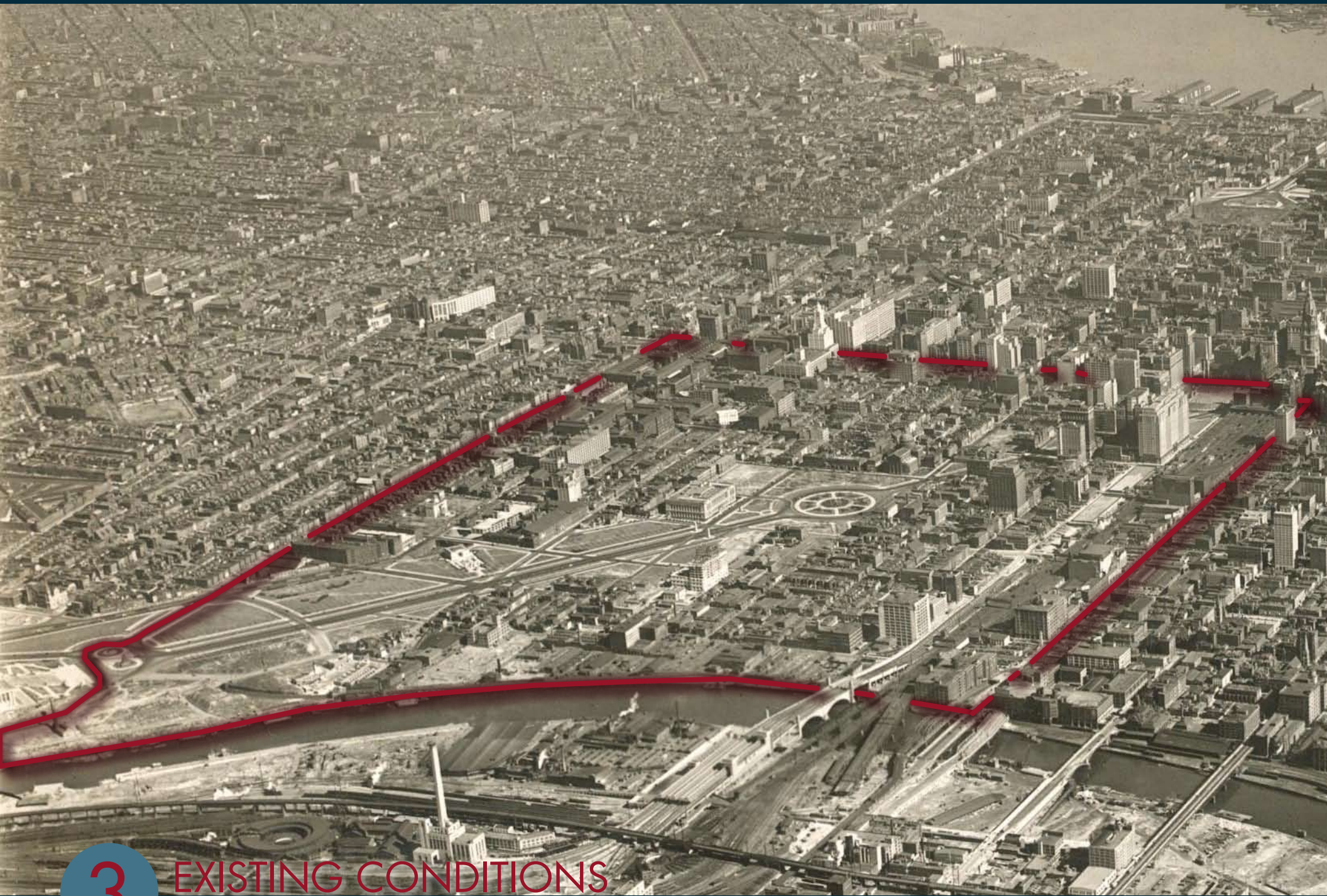
The Logan Square Neighborhood Association received 468 responses to the online survey. Residents identified the walkability of the neighborhood, access to the Schuylkill River Trail, and access to transit as the three most important attributes of the Logan Square Neighborhood - Parkway (*Figure 1*).

Residents identified the chronic street homeless population, inadequate neighborhood-serving retail, and development pressures as the three most important challenges facing the neighborhood. Notably, the chronic street homeless population in the neighborhood was cited by most respondents as their primary concern above all other issues.

*Figure 1. Sample Survey Response* ►

| 36. What are the best features of your immediate neighborhood? (Choose the 3 best features.) |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Walkability  |    | 73.6%            | 306            |
| Historic character   |    | 12.7%            | 53             |
| Open space along the Parkway   |    | 25.2%            | 105            |
| Access to Schuylkill River Trail   |    | 31.0%            | 129            |
| Close-knit community   |    | 10.6%            | 44             |
| Schools  |     | 1.9%             | 8              |
| Playgrounds  |     | 1.9%             | 8              |
| Proximity to work  |    | 26.2%            | 109            |
| Proximity to museums   |    | 27.9%            | 116            |
| Proximity to cultural events   |   | 20.9%            | 87             |
| Convenience of public transportation   |  | 30.8%            | 128            |
| Convenience of Center City retail  |  | 27.6%            | 115            |
| Other (please specify)   |   | 6.0%             | 25             |





3

EXISTING CONDITIONS





### 3.1 STUDY AREA OVERVIEW

With its healthy mix of uses and attractions, the Logan Square Neighborhood - Parkway is a vital piece of Philadelphia's Center City. One of the keys to Center City's success is its considerable downtown population, to which the Logan Square Neighborhood - Parkway contributes almost 7,000 residents. Downtown residents add to the lively environment in Logan Square-Parkway, which mixes walkable neighborhoods, national attractions, and the City's important office commercial district.

The Benjamin Franklin Parkway is a grand boulevard, developed in the 1920s to the designs of Jacques Gréber that runs diagonally through the neighborhood and concentrates open greens, institutions, and civic spaces. Local, national, and international visitors come to the Parkway to experience its cultural institutions. In 2006, the Franklin Institute, Please Touch Museum, and the Academy of Natural Sciences drew close to 1.5 million visitors, and if the Philadelphia Art Museum visitors are counted, the total was over 2 million visitors. The addition of the Barnes Museum to the Parkway will significantly increase the number of visitors. The Parkway also draws large numbers to events. It is considered the City's celebratory space for large local and regional events. Finally, the Parkway

◀ *Image 7. Logan Square c. 1932*



carries a good portion of regional and local traffic with its connections to I-676, I-76, the River Drives, and Spring Garden, Market, and Broad Streets.

To the south of the Parkway, the historic rowhouse neighborhoods of Appletree and Cherry Streets are nestled between the commercial business district and the institutions of the Parkway. Between JFK Boulevard, Arch Street, 20th Street, and the Schuylkill River, the commuter rail tunnel resurfaces and a series of underpasses connect Logan Square with the rest of Center City to the south. City Hall anchors the predominantly office-oriented southeast corner of the study area, framed by Dilworth Plaza, the Municipal Services Building, Love Park, and Penn Center. Penn Center was developed in the 1950s as a modern office district on the site of the demolished Broad Street Station and “Chinese Wall” elevated rail tracks. The office district, served by the suburban regional rail station, is now an economic engine for the City and extends from Penn Center along JFK and Market Streets between Broad Street and the Schuylkill River, and north to Logan Square, with occasional residential and retail pockets. At the

western boundary of the study area, the Schuylkill River is accessible to the community at ground level at Race Street.

New residential development along 23rd Street such as the Edgewater and 2200 Arch can take advantage of this access and the river views. Opportunity sites for redevelopment include the underdeveloped parcels fronting the river along 23rd Street, currently occupied by surface parking, air-rights parcels over the SEPTA tracks, and undeveloped parcels along Arch Street. In the northwestern corner of the study area south of the Parkway, Park Towne Place Apartments is isolated from the river and related open space by an entrance ramp to I-676 at the west and I-676 itself to the south.

In the area northeast of the Parkway, North Broad Street is home to Hahnemann Hospital, Roman Catholic High School, the expanded Pennsylvania Academy of Fine Arts, and the Convention Center expansion. Hahnemann Hospital is a regional full-service acute-care hospital with emergency room services. Its facility and its partnership with

*Image 8. “Chinese Wall” looking west from City Hall c. 1940’s* ►



▲ Image 9. Along Market Street c.1957

▼ Image 10. Penn Center c.1957





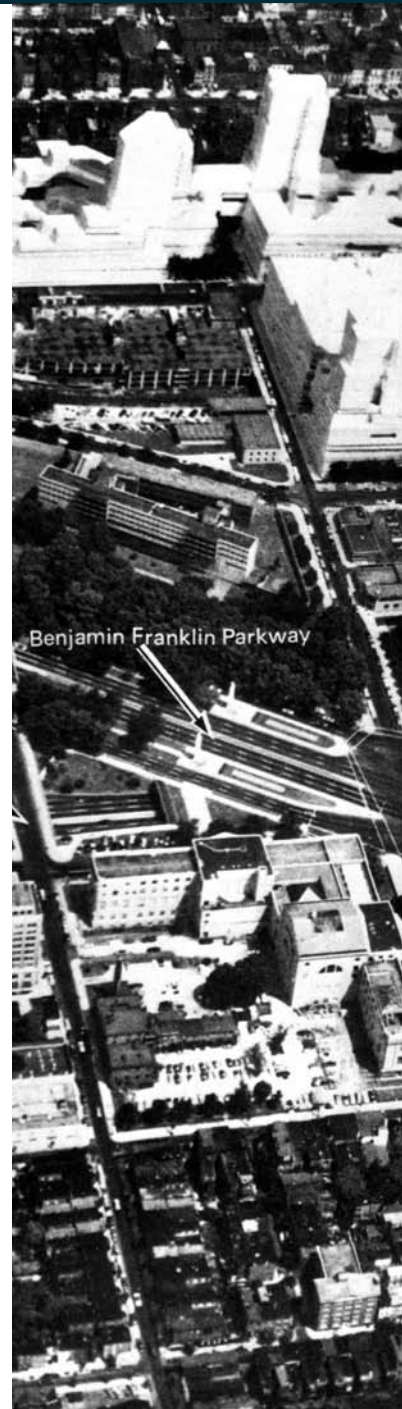
Drexel University are among the reasons why Philadelphia is renowned in the medical and research community. With the Convention Center expansion, this area will evolve to meet the new demands of the conventioners. North of Vine Street to Spring Garden Street, the historic Philadelphia Inquirer Building and the former State Office Building offer opportunities for adaptive reuse.

Much of the area west of 16th Street through to 21st Street north of Race was the subject of a 1970s redevelopment plan known as Franklin Town, designed by Philip Johnson. Former industrial areas were cleared and planned for new high-density development. Philadelphia Community College now encompasses much of the land between 16th and 18th and Callowhill and Spring Garden Streets. Completed elements of the Franklin Town Plan include the diagonal Franklin Town Boulevard from Vine Street to Callowhill Street, the Watermark, City View Condominiums, and Franklin Town Park. Franklin Town Park is flanked by parking lots to the north (a future development site), the railroad cut and vacant land to the south, and the new Tivoli

residential development to the west. The original vision of the Franklin Town redevelopment plan was for development on all sides of Franklin Town Park (*Image 11: Franklin Town Building*). There are large parking lots north of Vine Street between 16th and 18th Streets, which are major development opportunity sites. The current, court-related functions of the Family Court Building on Logan Square will be relocating, making the building available for new uses.

The former Reading Company City Branch rail right-of-way, now owned by SEPTA, runs east-west through the Study Area in a cut runs from Broad Street (at the Inquirer Building). The railroad cut parallels Callowhill Street to 20th Street, then extends under the Whole Foods block and Pennsylvania Avenue. (*Image 12: Reading Company City Branch Rail Right-of-Way*). In some areas parking has been located in the cut. The right-of-way provides an opportunity for a potential transit connection to Center City, serving the Barnes Museum, the library and other attractions.

*Image 11. Franklin Town Buildout Rendering c.1972* ►





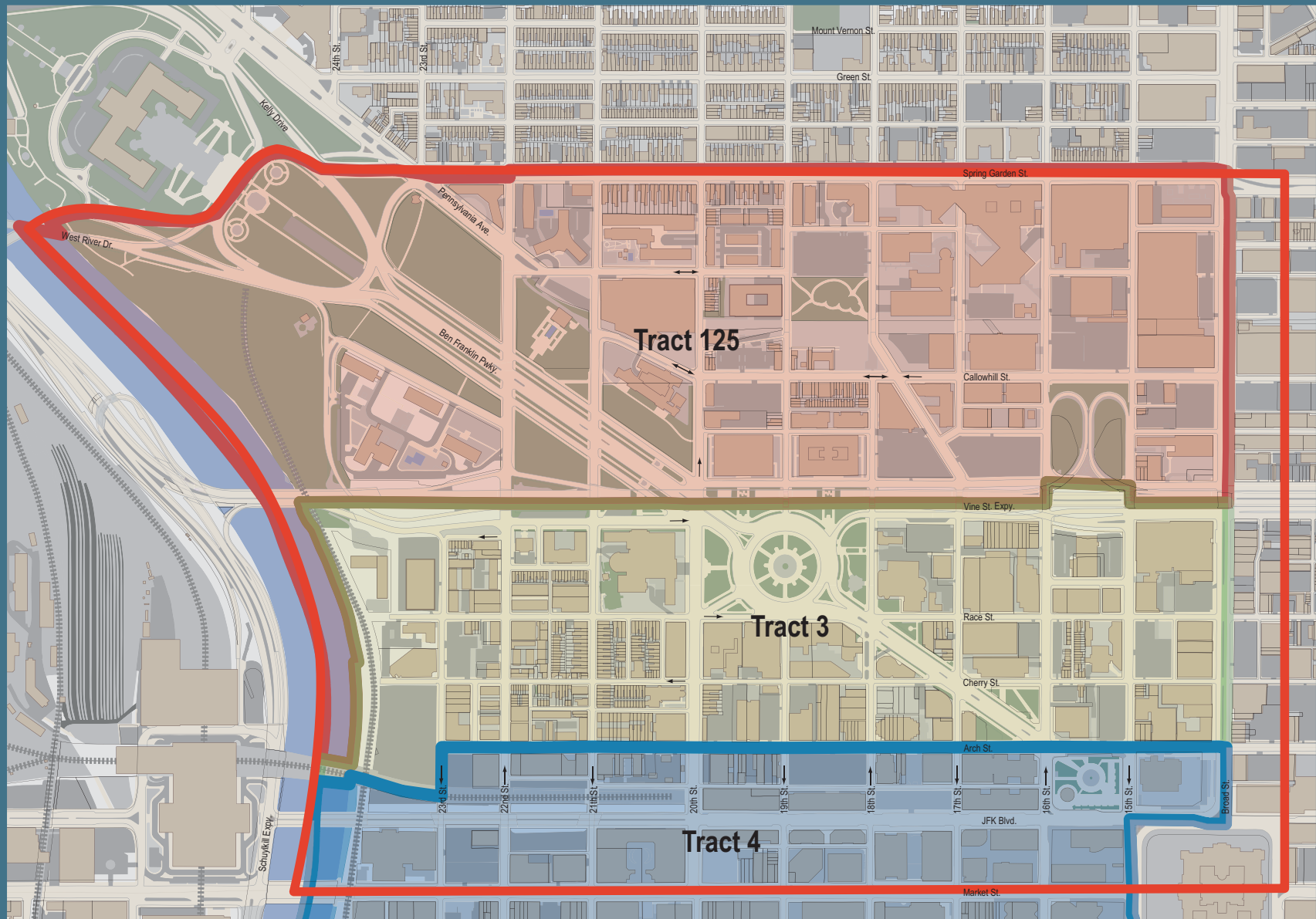
Eakins Oval and the northwestern corner of the study area provide connections to the Schuylkill River Trail, Spring Garden Street out to West Philadelphia, Martin Luther King Drive (formerly known as West River Drive), Fairmount Park, the Art Museum, and Kelly Drive and Boat House Row. Recent planning efforts have sought to improve the pedestrian connections and vehicular traffic patterns in this area, including a the West Bank Greenway, which will provide pedestrian connections from Spring Garden Street Bridge to the Schuylkill River Trail.

▼ (Image 12. Reading Company City Branch Rail Right-of-Way, c. 1900





# CENSUS TRACTS



## 3.2 NEIGHBORHOOD DEMOGRAPHIC PROFILE

The Logan Square Neighborhood - Parkway includes Census Tracts 3, 125, and portions of 4 (*Map 2*). A snapshot of the Study Area based on 2000 data shows a primarily white population with the largest share being between the ages of 20 to 44, with above-average median household incomes, living primarily in rented housing units, and residing in mainly single-person living alone households.

The total population in the Study Area increased from 6,083 in 1990 to 6,922 in 2000. This reflected a 14% increase in population, compared with the overall population in Philadelphia, which declined 4% between 1990 and 2000.

In Logan Square, whites comprised 83% (4,992) of the total population in 1990 and 72% (4,955) of the total population in 2000. While the white population decreased 0.7% between 1990 and 2000, the black population increased 36%, from 753 to 1,025. The Asian population has more than tripled in the study

area, from 203 in 1990 to 629 in 2000, and the Latino population increased 56% from 196 in 1990 to 307 in 2000. Citywide, between 1990 and 2000, the white population had seen a 4% decrease, while the black, Asian, and Latino populations experienced slight increases.

In the Logan Square-Parkway Study Area in both 1990 and 2000, the majority of the population, 54%, and 51% respectively, were between the ages of 20 to 44. Citywide, the proportion of the population between the ages of 20 to 44 in 1990 was 38% and in 2000, 37%. In 2000, 19% of Study Area residents were over 65, compared to 14% for the city as a whole.

However, since the 2000 census there has been substantial residential growth in the neighborhood, and the 2000 census data does not entirely reflect current conditions.

◀ *Map 2. Logan Square Neighborhood - Parkway Census Tracts*

## Housing Data

Within the Study Area, in 2000, the highest proportion of householders lived alone (67%). In 2000, citywide, 34% of householders lived alone, up from 32% in 1990. In 2000, only 16% of the households in the Study Area were married couple households, compared with 32% of households citywide. Only 6% of households in 2000 included school-age children, compared with 33% city wide, likely reflecting the large number of smaller rental apartments and the perceived lack of quality schools.

In the Study Area in 2000, 85% of the housing units were rented, and 15% were owner-occupied. These statistics are quite different from the statistics for Philadelphia citywide where in 2000, 41% of the housing units were rented, and 59% were owner-occupied. There was a significant (25%) increase in the number of owner-occupied units between 1990 and 2000.

Vacancy rates between 1990 and 2000 decreased in the Study Area from 18% to 11%. Citywide, vacancy rates remained stable at 11%.

## School Enrollment/Educational Attainment

School enrollment figures for the Study Area for 1990 and 2000 show an increase in the number of people who were enrolled in college or graduate school. In the Study Area, the percentage of the population aged 25 and older with a bachelor's degree was 24% in 2000. Citywide, in 2000, only 10% of the population had a bachelor's degree.

## Socioeconomic Data

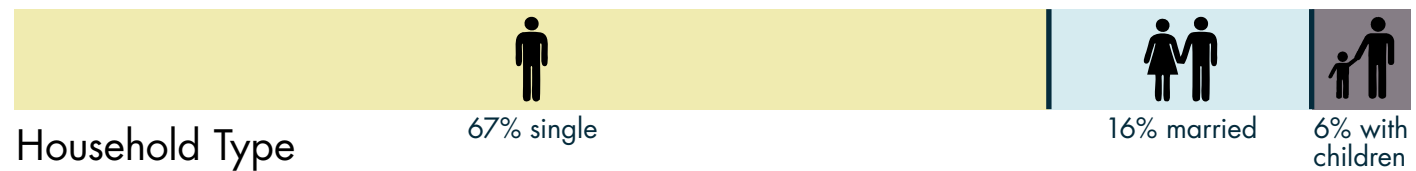
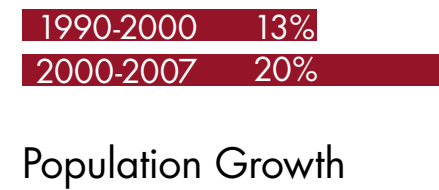
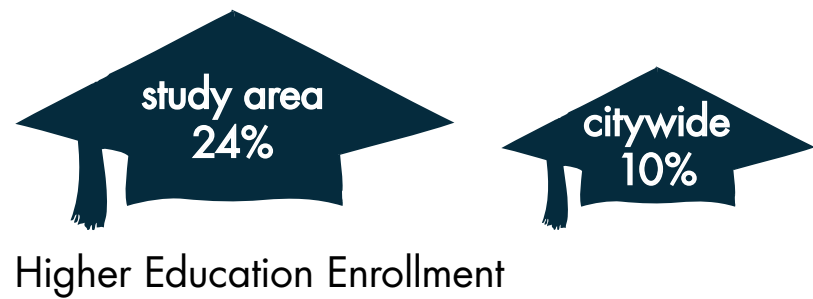
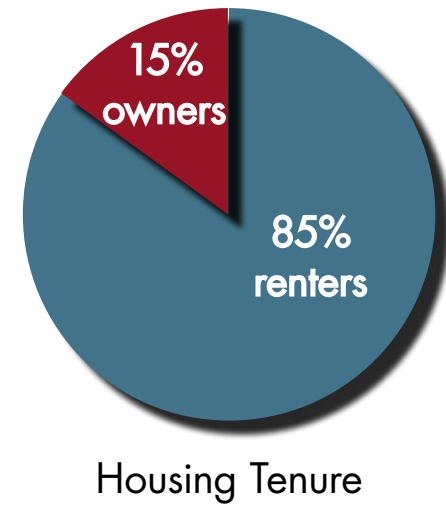
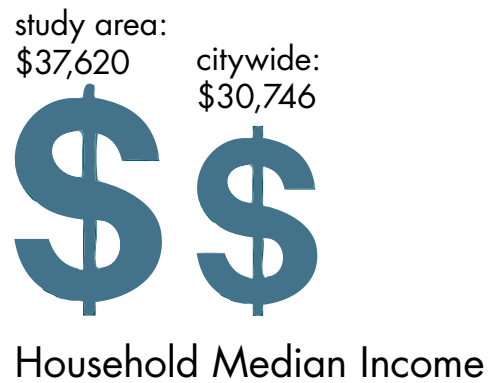
According to Census data, the median household income for the City of Philadelphia was \$30,746 in 2000. Within the Study Area, the median household income was higher than the citywide median at \$37,620 in 2000.

Both the poverty rate and the unemployment rate in 2000 were lower in the Study Area than in the city. In 2000, the Study Area poverty rate for individuals was 20% and the unemployment rate was 3%. In 2000, the citywide poverty rate for individuals was 23% and the unemployment rate was 6%.

*Figure 2. Neighborhood Demographics*



FIGURE 2. NEIGHBORHOOD DEMOGRAPHICS





# DEVELOPMENTS AND SOFT SITES



### 3.3 DEVELOPMENT

The Logan Square Neighborhood - Parkway presents a variety of opportunities for development, both new construction and adaptive use. While most of the large-scale projects with approved plans are located north of Vine Street and west of 19th Street, potential development sites are distributed throughout the study area. Adaptive use opportunities are located primarily north of Vine Street and east of 20th Street.

**Map 3: Soft Sites** shows various sites that have plans with approvals in place, sites with proposed plans, and recent development, and the distribution of soft sites throughout the LSNA boundaries.

Major projects were identified based on their large footprints, total development area, or significant impact on the neighborhood. Eight sites have approved plans, including the Parkway 22 development, which would occupy the largest parcel (127,171 square feet) of all approved plans. The American Commerce Center, a proposed 1500' tall tower at 18th and Arch Streets, received zoning approval in late 2008, and is awaiting final plan approval.

Many of the projects planned in 2007 are now on hold, due to the 2008 economic downturn. Six sites had proposed plans but had not obtained approvals as of the date of this plan.

Soft sites are underutilized parcels that are susceptible to development, such as surface parking lots, former industrial buildings converted to parking garages, or one-story retail establishments that offer the potential for higher-density redevelopment. Eighteen sites, such as the Whole Foods site at 20th and Callowhill Streets and the former Please Touch Museum at 21st and Race Streets, present development opportunities with no plans or approvals in place.

Five sites are either in the process of being adapted, such as the Board of Education Building being converted to rental apartments at 21st and Winter Streets, or present an opportunity for adaptive use, such as the State Office Building at Broad and Spring Garden Streets.

#### Map 3. Soft Sites

## Recent Developments

Notable developments completed in the last five years within the Logan Square-Parkway Study Area include:

1. 2200 Arch Condominiums: Conversion of a 12-story, former industrial loft building as 160 condominium units with first floor commercial space, as well as a new 4-story above-ground parking garage at the corner of Arch and 23rd Streets, which lacks active first floor uses or any screening of the parking decks.
2. 1500 Spring Garden Street: Renovation of the 12-story, 1 million square feet former Smith Kline French headquarters as Class A office space targeted to media, marketing, and professional services companies. The site benefits from excellent access to the Vine Street Expressway and the Broad Street subway.
3. Edgewater Apartments: At 23rd and Vine, the Edgewater Apartments consist of 290 rental units in a 12-story high-rise building

and townhouses that wrap an above-ground parking garage. One- to 3-bedroom units are offered in the apartment building, as well as 3-bedroom townhouses. The development successfully wraps what would have been an unattractive above-ground parking garage with wood-frame built, brick-fronted townhouses. A second phase of the project would include a high-rise residential building on the northern portion of the site.

4. Tivoli Condominiums: A condominium development at 20th and Hamilton Streets that includes a 10-story condominium building accommodating 2- and 3-bedroom units, and 2- and 3-bedroom, 3- and 4-story townhouses. The 10-story building fronts Franklin Town Park. The form of the development was refined in conjunction with the Logan Square Neighborhood Association, and the community regards it as a possible model for future residential development in the neighborhood in terms of form and density, and its mix of townhouses and apartments.

*Table 1: Development Density for Recent Residential Developments* ►

5. King's Court, 22nd and Race Streets: A development of 6 large contemporary design, 4-story, 4,500 square foot townhouses with 4 bedrooms, 4 baths, elevators and rear garage parking. The scale of the townhouses dwarfs adjacent townhouse development.
6. Board of Education Building: Located at 21st and Winter Streets on the south side of the Parkway, this building has been renovated and converted into 127 residential units. Approximately 80 parking spaces are provided in the Ortlieb warehouse building at 22nd and Race Streets.
7. Comcast Center: The Comcast Center at 17th and JFK Boulevard consist of two towers in two phases enclosing a half-acre landscaped

plaza fronting JFK Boulevard. The first tower at 1701 JFK Boulevard was completed in 2008. It is a 1.4 million square foot, 975-foot-tall, 57-story office building. The second phase will include an 18-story 280,000 sq. ft. building at the southwest corner of the site.

8. Murano Condominiums: The Murano is a new-construction, 302-unit, 42-story condominium tower at 21st and Market Streets that stretches through the block to JFK Boulevard. The ground floor includes 9,000 square feet of retail. A separate stand-alone, above-ground parking garage faces JFK Boulevard. The parking garage is an unattractive presence on JFK Boulevard and 21st Street. There are no active first floor uses or screening of exposed parking decks.

**Table 1. Development Density for Recent and Proposed Residential Developments**

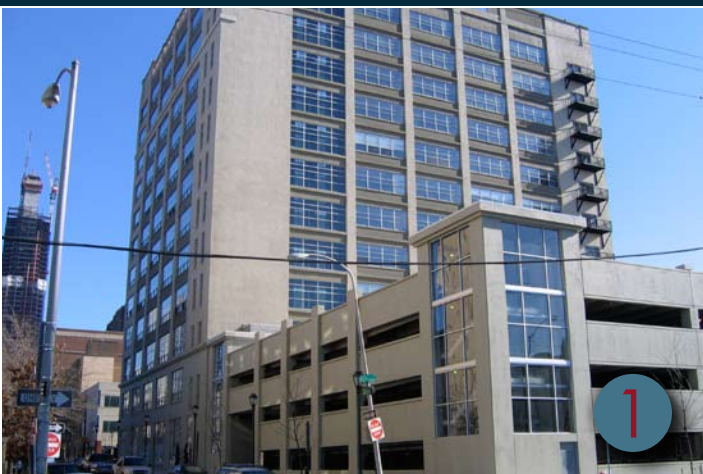
| Name                | Zoning | Site Area      | No. of Units | Units/Acre | FAR |
|---------------------|--------|----------------|--------------|------------|-----|
| Murano              | C5     | 63,900 sq.ft.  | 321          | 220        | 10+ |
| Tivoli              | RC4    | 56,817 sq.ft.  | 128          | 98         | 4+  |
| Parkway 22          | R15    | 127,171 sq.ft. | 371          | 127        | 4+  |
| Edgewater (Phase 1) | RC4    | 165,528 sq.ft. | 290          | 76+        | 4+  |
| Cherry Street West  | R10B   | 35,800 sq.ft.  | 43           | 50         | 2   |



9. Lofts at 1835 Arch Street: In 2001, this former Bell Telephone Company building was converted into 191 apartments. The building is 18 stories and listed on the National Register of Historic Places.
10. 440 N. Broad Street: Renovation of the former Philadelphia Inquirer printing facility as the Philadelphia School District administration building. The relocation of the School District operations to this site freed up the historic Board of Education building at 21st and Winter Street.
11. The Lofts at Logan View: Conversion of an existing apartment building at 17th and Cal-lowhill Street into condominiums.
12. The Phoenix at 16th and Arch Streets: In 2006, the Insurance Company of North America building was converted from office into 267 condominiums. This building is listed as a National Historic Landmark.

*Image 13. Recent Developments* 







## Proposed Developments

Proposed developments that either have planning approval, or for which a significant amount of preliminary design work has been completed, include:

- **1601 Vine Street:** The proposed mixed-use development at 16th and Vine Streets includes approximately 1.5m sq. ft of construction in two buildings. The first phase includes a 46-story tower with a 250-room hotel and residences above. The first floor accommodates a Whole Foods Grocery Store and other retail. The development includes an 8-level above-ground parking garage podium that could have a significant adverse visual impact on the surrounding streets. Phase 2 is proposed as a 12-story residential building.
- **22 Parkway (*Image 16*):** The 22 Parkway development, formerly known as the Barnes Tower, is located at 22nd Street and

Pennsylvania Avenue on the site of the former Best Western Motel. Phase 1 of the development would consist of a 407-foot-high, 35-story building containing 238 units facing the Parkway, 5-story townhouses facing Spring Garden Street containing 12 units, and 7-story loft building facing 21st Street containing 30 units. An 18-story Phase 2 tower containing 91 units is also proposed along 22nd Street. The development fronts a landscaped court above underground parking. The approved development was the result of protracted negotiations between the developer and community organizations after the original proposal for a higher building that blocked views from an existing residential development and lacked any street fronting buildings on 21st Street generated significant community opposition. The project is currently on hold.

- **American Commerce Center (*Image 15*):** The proposed 63-story American Commerce Center at 18th and Arch Street, currently a parking lot would, at 1,500 feet, be the tallest tower in the



city. It would include a 26-story hotel, high-level roof gardens, 3-6 stories of retail along Arch Street. Multi-level public gardens would face the Arch Street Presbyterian Church. Car and bike parking would be underground.

- **River City (*Image 14*):** The proposed River City Project is not proceeding, but would have had a significant impact on the Logan Square community. The proposed site extended from the Schuylkill River to 20th Street along JFK Boulevard, and between the Schuylkill River and 23rd Street north to Race Street. The proposed development would have used air rights above the SEPTA tracks. Four major separate development sites were included in the proposal. Over 8 million sq. ft. of mixed-use development was proposed in a series of towers up to 56 stories in height with almost 4,000 parking spaces. The development would have bridged Arch Street west of 23rd Street and required significant zoning variances for density for the parcels west of 23rd Street. The development would have had significant

adverse impacts on the adjacent residential community north of Arch Street and east of 23rd Street, such as increased traffic and building shadow impacts. The proposed first phase development would have extended from 20th to 21st Street along JFK Boulevard, and included two separate buildings with a 51-story residential tower and a 34-story office slab structure above a podium plaza spanning the SEPTA tracks.

- **Cherry Street West at 21st and Cherry:** A development of 43, 4-story townhouses on a site currently used as a parking lot at 23rd and Cherry Street. This project is currently on hold.
- **1919 Market Street (Opus East):** This proposed development by Opus East at 20th and Market Streets consists of a 600,000 sq. ft. 14-story building containing 257 apartments, 16,530 sq. ft. of retail on the first floor, and a 168-car parking garage enclosed by residential units on

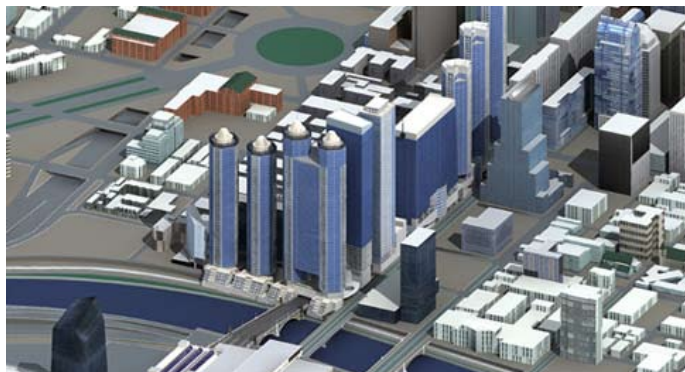
the public streets. The project is currently on hold.

- **Convention Center Expansion:** The expansion of the Pennsylvania Convention Center will extend the convention center to North Broad Street between Race and Arch. The expansion will increase the area of the center from 440,000 square feet to 700,000 square feet, and include the largest contiguous exhibit space in the northeast (541,000 square feet). Completion is scheduled for early 2010. The expansion will generate a significant market for new hotel development in the vicinity with several potential sites in the Logan Square area.
- **Barnes Museum:** The highest profile project proposed within the Study Area is the new Barnes Museum, at 20th and the Parkway. The Barnes Foundation selected Todd Williams and Billie Tsien as architects for building, and the building is in design. The City is moving ahead with relocation of the Youth Study Center that currently occupies the site. Completion

of the museum would likely have a significant positive impact on increasing visitor interest in other Parkway Museum attractions.

- **The Free Library of Philadelphia:** Moshe Safdie was retained to design the expansion of the Library's main branch on Logan Square. The expansion design includes 180,000 square feet of new and improved facilities such as a Teen Center, hundreds of public access computers, and a glass-enclosed atrium featuring shops, a cafe, and ample space for community gatherings. Renovation of the existing building is already underway including a new roof and new café. The total cost of the expansion is estimated at \$150 million. It is unclear at this time when construction will begin.
- **Rodin Place:** Located at 22nd and Hamilton over the Reading Company City Branch right-of-way, this proposed development no longer has building permits. The development would have included 44 residential units with 74 parking spaces. The development is not proceeding as of the date of this plan.

- The Granary: Located between 19th and 20th Streets and Callowhill, the proposed development included high-rise residential with parking underground. The parking may have been proposed in the Reading Company City Branch right-of-way.



▲ *Image 14. River City Development Proposal*



▲ *Image 15. American Commerce Center*



▲ *Image 16. 22 Parkway*



## Soft Sites

Soft sites are underutilized parcels, such as surface parking lots, former industrial buildings, or one-story retail establishments that offer the potential for higher density development. Twenty-four soft sites, including parcels that might have plans but no plan approval, were inventoried and analyzed in **Table 2**.

These sites total 1,650,790 square feet or approximately 37 acres. Under the current zoning, without consideration to height controls, these sites could yield 10 million square feet of development as-of-right or almost 20 million square feet of development if all bonus provisions were utilized.

Areas where significant change could occur include:

- the Whole Foods, Granary, Callowhill area;
- along the River and over the SEPTA tracks;
- Vine Street between 16th and 18th;
- Arch Street between 18th and 21st; and

- the former Please Touch Museum site.

A high-profile soft site is the Archdiocese property surrounding the Cathedral of SS. Peter and Paul. This site, with frontage on Logan Square and currently consisting of surface parking and a low-rise residence building, could provide the opportunity for a world-class mixed-use development comparable to the One Logan Square development that could significantly enhance Logan Square.

## Residential Building Permits Issued

Between 2001 and 2007, residential building permits were issued for a total of 1,116 new construction and conversion units in the Study Area. Permits were issued for townhouses, apartments, and condominium units in 11 different developments. Between 2001 and 2003, only one permit was issued for 10 condominium units in the Study Area, but in 2004, residential permits were issued for a total of 564 units in three different developments. This is the peak number of units for which permits were

*Table 2. Soft Site Analysis*



TABLE 2: SOFT SITE ANALYSIS

| Name                                     | Address                                | Existing Zoning | Parcel Square Feet | Allowable FAR (sf) | Allowable FAR with Bonus (sf) |
|--|--|-----------------|--------------------|--------------------|-------------------------------|
| American Commerce Center                 | 18th and Arch                          | C4*             | 64,000             | 320,000            | 832,000                       |
| Boy Scout Building                       | 2100 block of Vine                     | R14*            |                    |                    |                               |
| Broad St. and Callowhill                 | Broad St. and Callowhill               | C4              | 59,200             | 296,000            | 769,600                       |
| Calder Museum                            | 21st and Parkway                       | REC*            |                    |                    |                               |
| Existing Whole Foods Site                | 21st and Hamilton                      | C4*             | 174,370            | 871,850            | 2,266,810                     |
| Family Court Building (existing)         | 1800 block of Vine                     | R15*            |                    |                    |                               |
| Former Please Touch Museum               | 21st and Race                          | R10A*           | 27,040             | 27,040             | 27,040                        |
| Granary                                  | 19th-20th Sts and Callowhill           | C2*             | 50,181             | 112,907            | 112,907                       |
| Marriott                                 | Broad and Race                         | C4              | 72,000             | 360,000            | 936,000                       |
| Philadelphia River City                  | 20th-24th, between Cherry and JFK Blvd | RC4/C5*         | 353,756            | 2,805,540          | 7,074,612                     |
| State Office Building site               | Broad and Spring Garden                | G2              |                    |                    |                               |
| 15th and Callowhill                      | 15th and Callowhill                    | G2              | 22,500             | 45,000             | 45,000                        |
| 1601 Vine Street                         | 1601 Vine Street                       | C5              | 84,787             | 1,017,444          | 1,695,740                     |
| 16th and Callowhill                      | 16th and Callowhill                    | G2              | 187,595            | 375,190            | 375,190                       |
| 16th and Spring                          | 16th and Spring                        | C5              | 19,000             | 228,000            | 380,000                       |
| 16th and Wood                            | 16th and Wood                          | C4              | 39,000             | 195,000            | 507,000                       |
| 17th and Spring                          | 17th and Spring                        | C5*             | 20,500             | 246,000            | 410,000                       |
| 17th and Winter                          | 17th and Winter                        | R15*            | 50,000             | 175,000            | 175,000                       |
| 18th and Vine St (Logan Place)           | 18th and Vine                          | C5*             | 69,982             | 839,784            | 1,399,640                     |
| 19th and Arch                            | 19th and Arch                          | C4*             | 58,650             | 293,250            | 762,450                       |
| 19th and Buttonwood                      | 19th and Buttonwood                    | RC4*            | 68,576             | 342,879            | 342,879                       |
| 19th and Cherry                          | 19th and Cherry                        | C4*             | 80,000             | 400,000            | 1,040,000                     |
| 20th and Arch                            | 20th and Arch                          | C4*             | 22,839             | 114,195            | 296,907                       |
| 20th and Franklin Town Blvd.             | 20th and Franklin Town Blvd.           | RC4*            | 49,762             | 548,809            | 548,809                       |
| 22nd and Market                          | 22nd and Market                        | C5*             | 77,052             | 924,624            | 1,541,040                     |
|  |  | <b>TOTAL</b>    | <b>800,547</b>     | <b>4,793,337</b>   | <b>9,752,159</b>              |
| * Falls within 125' Height Limit Overlay |  |                 |                    |                    |                               |

issued between 2001 and 2007. In 2005, residential permits were issued for six townhouse units; in 2006, residential permits were issued for 439 units in three different developments; and as of August 31, 2007, residential permits were issued for 97 units in three different developments.

## Residential Real Estate Market

During the period of 2003 to 2006, there were a total of 78 single-family home sale transactions. The median price of all single-family home sales was \$433,250. The Study Area experienced a 44% increase in the median sales price of single-family homes (from \$349,500 in 2003 to \$505,000 in 2006). The volume of single-family home sales decreased 55% (from 27 transactions in 2003 to 12 in 2006).

Consistently for all four years analyzed, the highest-priced home sales occurred in Census Tract 3. Between 2003 and 2006, Census Tract 3 experienced an increase of 52% in single-family home median

sales prices. Between 2003 and 2006, Census Tract 125 experienced an increase of 34% in single-family home median sales prices.

During the period of 2003 to 2006, there were 748 residential condominium sales in the Study Area, and the median price of all condominium sales was \$292,500. The Study Area experienced a steady 52% increase in the median sales price of condominiums (from \$176,000 in 2003 to \$367,784 in 2006). The volume of condominium sales decreased 46% (from 287 transactions in 2003 to 153 transactions in 2006).

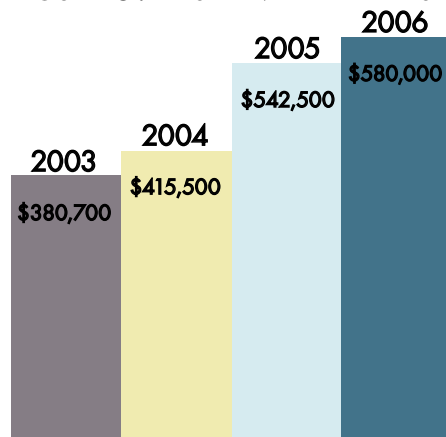
Most of the highest-priced condominium sales occurred in Census Tract 3. Between 2003 and 2006, Census Tract 3 experienced an increase of 38% in condominium sales prices. Between 2003 and 2006, Census Tract 125 experienced an increase of 112% in condominium sale prices.

*Figure 3. Residential Real Estate Market*





FIGURE 3. RESIDENTIAL REAL ESTATE MARKET



Census Tract 4 Median Home Price

2.9% 1990-2000

20.9% 2001-2007

Growth in Housing Units





## 3.4 ZONING

### Summary of Zoning Districts

**Map 4** shows the existing zoning classifications. **Table 3** provides a summary of FAR per zoning district.

The C-4 and C-5 Commercial classifications encompass a significant portion of the Study Area, stretching along the southern boundary and north along Broad Street. These zoning designations are intended for areas with the highest density and the most transportation infrastructure, whether by bus, rail, or car. C-4 and C-5 allow a base FAR of 5 and 12 respectively. An additional bonus FAR of 8 is available for larger parcels in both C-4 and C-5 zoned areas with two street frontages of at least 50 feet. A further incentive FAR bonus of 4 is available for C-4 and C-5 parcels fronting JFK Boulevard, Market, and Broad Streets in the study area. The confluence of transportation hubs, including City Hall and Suburban Station, permits the area to be densely populated, while not requiring a burdensome amount of parking. The C-4 and C-5 Commercial

classifications have no parking requirements for non-residential uses, and in Center City, multi-family residential development has a reduced parking requirement. Generally, C-5 zoned parcels front JFK Boulevard, Broad Street south of Race, and the south side of the Parkway east of 18th Street. C-4 parcels on Arch Street form a somewhat lower-density transition to the residential blocks of Logan Square, but still permit high-rise development. The C4 and C5 classifications do not have any height restriction, or building bulk restrictions, other than percentage lot coverage restrictions. Permitted lot coverage decreases with height. The exception is the south side of JFK Boulevard, where building width is limited to 250 feet, with a minimum 75' between buildings, to assure sunlight penetration to the street. The lack of restrictions on building footprint dimensions has resulted in some excessively bulky buildings, such as Kennedy House, and the Sterling, 30-story buildings that extend a full city block (400 feet), and block views and daylight access to adjacent parcels.

The area bounded by Spring Garden Street, 18th Street, Callowhill Street, and Broad Street consists

◀ **Map 4. Existing Zoning**



**Table 3: Density Analysis of Zoning Districts in Logan Square Neighborhood**

| Zoning Classification | Base FAR | Base Max. Density (units per acre) | Bonus FAR <sup>+</sup> | Incentive Bonus FAR <sup>#</sup> | Total FAR | Theoretical Max. Density (units per acre) | Height Limit – Ft. (Stories) |
|-----------------------|----------|------------------------------------|------------------------|----------------------------------|-----------|---|------------------------------|
| R10A                  | 2*       | 30                                 | —                      |                                  | 2*        | 30  | 35 (3)                       |
| R10                   | 2*       | 60                                 | —                      |                                  | 2*        | 60  | 35 (3)                       |
| R10B                  | 4*       | 116                                | —                      |                                  | 4*        | 116                                       | 50 (5)                       |
| R14                   | 1.5      | 44                                 | 1*                     |                                  | 2.5*      | 73  | No Limit                     |
| R15                   | 3.5      | 100                                | 2*                     |                                  | 5.5*      | 160                                       | No Limit                     |
| RC4                   | 5        | 145                                | 8                      | 4                                | 17        | 493                                       | No Limit                     |
| C2                    | 2.25*    | 65                                 | —                      |                                  | 2.25*     | 65  | 35 (60 for commercial)       |
| C3                    | 4.5      | 130                                | —                      |                                  | 4.5       | 130                                       | No Limit                     |
| C4                    | 5        | 145                                | 8                      | 4                                | 17        | 493                                       | No Limit                     |
| C5                    | 12       | 232                                | 8                      | 4                                | 24        | 580                                       | No Limit                     |

\*(FAR is interpreted)

+ site must meet certain requirements in order to be eligible for bonus FAR

# only sites on Market, JFK, N. Broad, and 1800 block of Arch St are eligible for this additional FAR

**Table 3. Current Zoning Classifications** ▲

entirely of Institutional Development District (IDD) and General Industrial (G-2) zoning classifications. The IDD classification is reserved for the Community College of Philadelphia, and is tailored to meet the needs of that campus. The Industrial zoning in this area does not reflect current and future land uses. The whole area, including the Franklin Town urban renewal area, and former Baldwin Locomotive Works site, was industrial at one time, but these uses left long ago and so the G2 zoning should be changed.

The area along the Schuylkill River from I-676 south to John F. Kennedy Boulevard is zoned RC-4 Residential-Commercial. This classification permits the construction of residential towers with first floor retail. The RC-4 zone permits an as-of-right FAR of 5, with bonuses providing potential total FAR of 13 for large parcels with frontage on two streets.

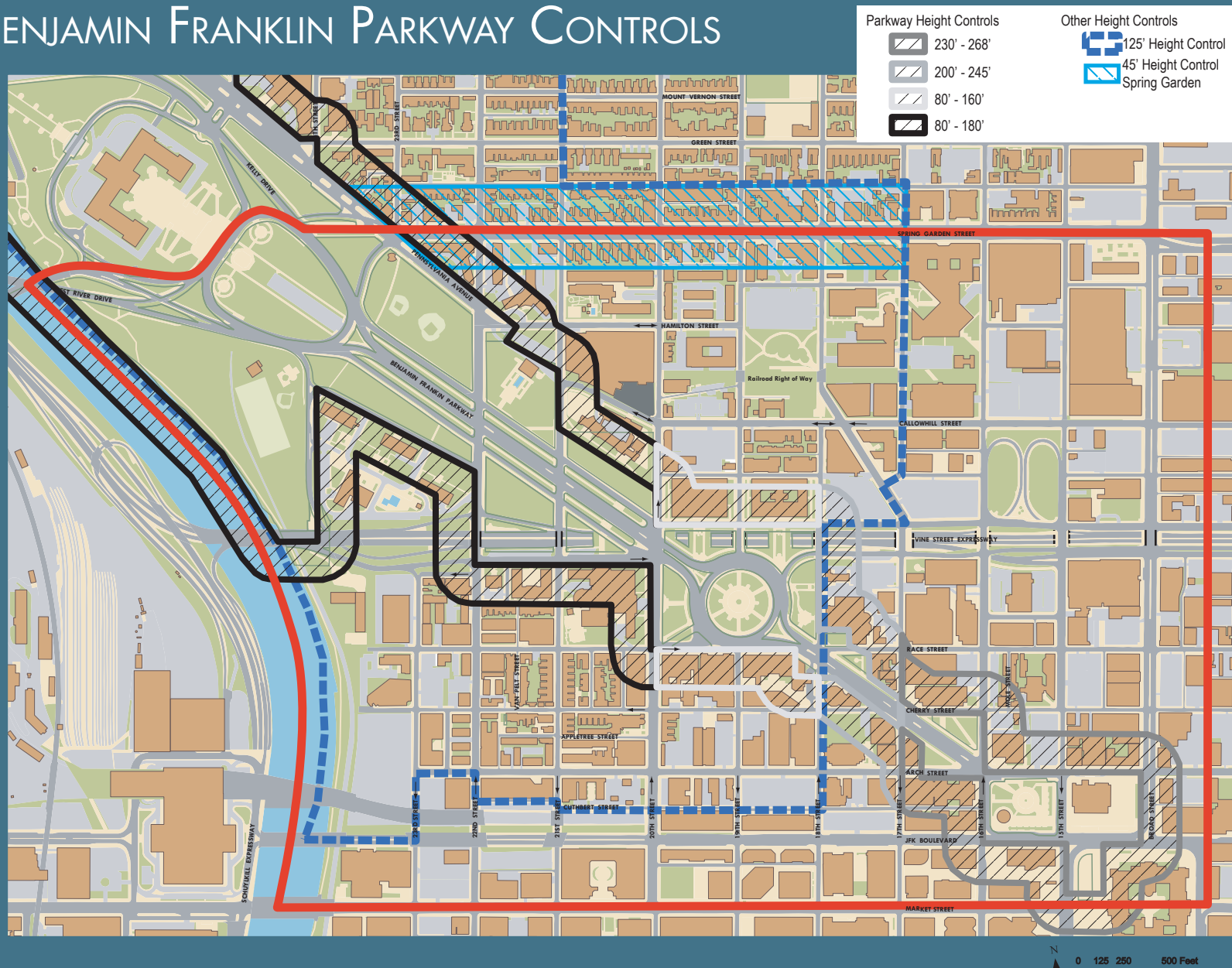
The Benjamin Franklin Parkway bisects the Logan Square neighborhood. This Recreationally zoned area houses the museums and galleries for which the area is noted.

In the historic residential core of Logan Square around 21st and Race Streets there are many single-family and multi-family converted rowhouses, and the zoning reflects this context, with districts such as R-10A single-family and R-10/R10B multi-family designations that are consistent with single-family and multi-family row house dwellings. There is a 35' height restriction in the R-10A and R-10 zones and a 50-foot height restriction in the R10B zone.

Along Vine Street and Spring Garden Street there are a number of properties that are zoned R-14, R-15, and R-16 high density residential. These zones permit an FAR of 1.5, 3.5 and 5.0 respectively. The development of these unlimited height (but floor area ratio controlled) residential multi-family categories has led to the expansion of the Benjamin Franklin Parkway Height Controls, one of the many overlays in this area.

**Table 3** indicates the approximate density in terms of actual FAR, and residential units per acre, of recently completed, and proposed developments.

# BENJAMIN FRANKLIN PARKWAY CONTROLS





## Overlays

The Logan Square neighborhood has more zoning overlays within its area than any other neighborhood in the city. These many overlays and special exceptions, while established to address important issues, can create confusion for residents and developers.

## Parking Controls

In most of the city, the ratio of parking spaces to new residential units is 1 to 1. In the downtown core, which in the Study Area includes the area south of Vine Street and east of 20th Street, multi-family developments with 25 units or more have a requirement of five spaces for every 10 units. In the area between Vine and Spring Garden Streets 7 spaces per 10 units are required for developments of more than 25 units. The basis for these parking requirements is the area's proximity to ample public transportation, the fact that Center City auto ownership is the lowest in the city, and an

effort to discourage unnecessary automobile usage and resulting traffic congestion. The parking requirement for single-family residential or for small multi-family projects is 1 space per unit.

## Parkway Controls

Benjamin Franklin Parkway Controls are indicated on **Map 5**. There are different sets of controls, which sometimes conflict.

In the area between Broad Street and 18th Street, the cornice line of buildings located within 200' of the Parkway cannot exceed a height of three times the width of its facade or 200' between Broad and 17th Streets or 230' between 17th and 18th Streets. Portions of buildings may step back from the cornice line and increase in height an amount equal to the set back. However, no portion of these buildings may exceed 268' between Broad and 18th Streets or 245' between 17th and 18th Streets. No buildings can be altered or erected until the Art Commission has approved the design.

◀ **Map 5. Benjamin Franklin Parkway Controls**

Within the area between Fairmount Park and 18th Streets, including Logan Square, the cornice line of buildings in this area also cannot exceed a height of three times the width of its facade, but in this case cannot be greater than 80'. Building height may increase with setbacks, but no portion of the building can exceed 100' in height when located between Fairmount Park and the Crescent (the Crescent is roughly located at Eakins Oval), or 160' between the Crescent and 18th Street.

Between 20th Street and Fairmount Park, higher buildings are permitted. In this area, the main cornice line of the building cannot exceed in height the street width of its facade or 160'. No other portion of the building can exceed 180'.

A new set of controls was enacted on March 28, 2007, in response to the opposition generated by the proposed height of the 22 Parkway development. These controls affect the area bounded by the Schuylkill River, John F. Kennedy Boulevard, 23rd Street, Arch Street, 22nd Street, Walden Street, 17th Street, Brandywine Street, North 21st Street,

Fairmount Avenue, North 22nd Street, and Aspen Street extended to the Schuylkill River. Within this area, no building can exceed 125' in height. Additionally, no building with frontage on Spring Garden Street between Pennsylvania Avenue and 18th Street or on the north side of Spring Garden Street between 18th Street and Broad Street can exceed 45' in height. The 125' height limit was enacted with the understanding that the building height issue would be addressed in a more comprehensive manner in the neighborhood planning effort.

## The Center City Height Controls

These controls were enacted to preserve the views of City Hall Tower from "gateways" to Center City, including Interstate 95, the Schuylkill Expressway, Broad Street, the Belmont Plateau, and the Benjamin Franklin Parkway. This section of the Code is accompanied by three maps (A, B, and C) which contain the height controls. Map C in the code includes Zone 4 the area generally bounded by Arch Street, 13th Street, Chestnut Street and 16th Street, part of which is located in the study area; the specific

height restrictions are given per block.

### Special Sign Controls for the Vine Expressway (I-676)

This overlay was designed to protect the Vine Expressway and vicinity from outdoor signs and it covers the area between Spring Garden Street and Race Street, river to river. It prohibits outdoor advertising signs within this gateway area, and requires the removal of existing signs. The overlay also provides size restrictions on accessory signs.

### Special Height Controls for Arch Street

Within the area bounded by Arch Street, 23rd Street, Cuthbert Street, 22nd Street, Walden Street, 21st Street, Cuthbert Street, and 20th Street, the maximum height of any building cannot exceed 185' from sidewalk level. This overlay predates the present 125' height limit that covers much of this area.

### Avenue of the Arts North Special District Controls

This overlay was designed to prohibit the proliferation of auto-oriented developments with buildings setback from the sidewalk behind parking lots. This overlay applies to all commercially zoned properties with frontage along Broad Street between John F. Kennedy Boulevard and Spring Garden Street. Prohibited uses include auto repair shops, auto sales, vehicle part sales, auto service stations, drive-in restaurants, beer distributors and retail sales of beer for take-out as an accessory use. Additionally, any new building must be at least 25' high. Any new building must be built to the front property line of Broad Street. Parking is not be permitted between the building and Broad Street.



## Recent Variances

LSNA responds to zoning variance requests through the LSNA Zoning Committee. The Committee reviews proposals and recommends action to the Zoning Board of Adjustment regarding neighborhood zoning matters that come to its attention, including changes in use, the construction of new buildings, signage proposals, and modifications to existing buildings and open space.

Applicants may present plans to the Board. The Committee and Board take into account the interests of adjacent owners when considering development proposals. The Association may, at the recommendation of the Committee, appoint an ad hoc Neighborhood Development Committee as a subcommittee of the Zoning Committee to address issues related to a new development. This ad hoc committee may work with the developer to draft a Community Benefits Agreement between the developer and the Association.

The Association does not normally issue letters of support to the City's Zoning Board of Adjustment for proposed projects but may issue a letter of non-opposition or of opposition.

## Neighborhood Agreements

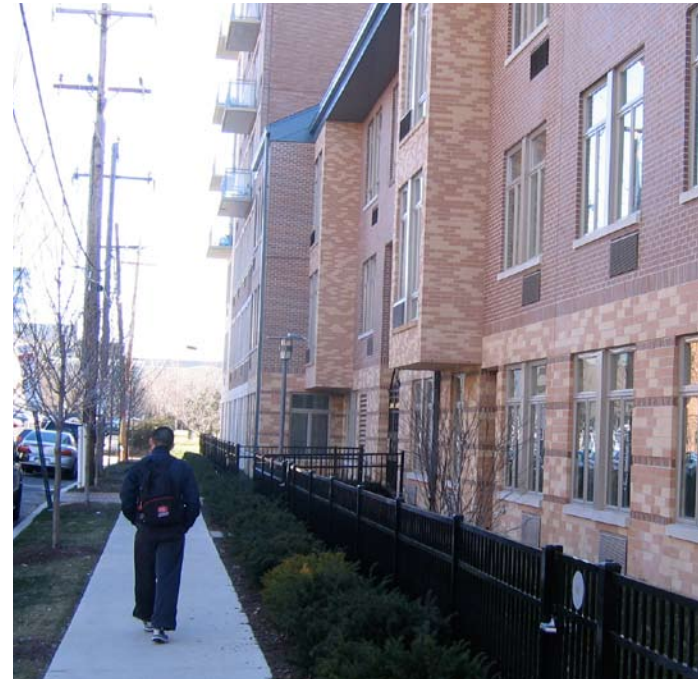
From time to time, LSNA enters into a Neighborhood Agreement with an owner and developer. The agreement is typically recorded and tied to the land, and in the event that the land changes ownership the same agreement applies. Examples of recent agreements entered into by the neighborhood include the following:

- The Ortlieb Building and Board of Education Building;
- Center City District's café located at 110 N. 16th Street;
- Tivoli Development; and
- Edgewater Development.

A neighborhood agreement may cover a variety of topics. In most cases the agreement includes the design plans for the proposed development. These design plans are legally binding and are the product of a negotiation process between the developer and the neighborhood.

### ITEMS TYPICALLY COVERED BY A COMMUNITY BENEFITS AGREEMENT:

- trees, sidewalks, and landscaping location and materials;
- lighting for pedestrian and safety purposes;
- future maintenance of open spaces;
- construction hours on weekdays 7am–4pm;
- prevention of lighting glare ;
- noise levels of garage doors, mechanicals, and tenants;
- number of parking spaces and dedicated car sharing spaces;
- building materials; and
- restoration requirements for existing buildings.



▲ *Image 17. Tivoli development was the subject of a neighborhood-developer agreement.*

## Design Issues Relating to Existing Zoning

Evaluation of the existing zoning controls highlighted several design issues that need to be addressed:

- Existing zoning does not provide adequate protection for established residential neighborhoods. In some cases, the existing high-rise, high-density zoning may permit excessive density, when additional permitted FAR is taken into account, and does not include adequate building form and bulk controls;
- Views from existing high-rise buildings are very important to residents. While views will change over time with development, an evaluation of impacts of new development on views from existing development should be considered, with impacts minimized through building bulk controls. The residents of City View Condominiums are particularly concerned about the character of development on the Whole Foods site, as it would potentially impact their view of the Center City skyline to

the south. Residents of Kennedy House are particularly concerned about development of the 1900 block of Arch Street, as it would potentially impact their view to the north;

- The zoning controls do not include any provisions for addressing or evaluating shadow and daylight impacts of new development; and
- Zoning controls do not include any requirements for inclusion of open space, public or private, in new residential development.



### 3.5 HISTORIC RESOURCES

The Logan Square neighborhood has a wealth of historic properties on both the National Register and the Philadelphia Register of Historic Places. The National Register of Historic Places, administered by the National Park Service and the Department of the Interior, was created in 1966 by federal legislation. Three National Register Historic Districts overlap areas of Logan Square: some properties along North Broad Street are a part of the Broad Street NR Historic District, the 1900 and 2000 blocks of Spring Garden Street are part of the Spring Garden NR Historic District, and the Parkway south to and including Logan Square is part of the Fairmount Park NR Historic District. Individual properties on the National Register, include the Arch Street Presbyterian Church, the Cathedral of SS. Peter and Paul, Family Court building, Franklin Institute, and the Board of Education Building.

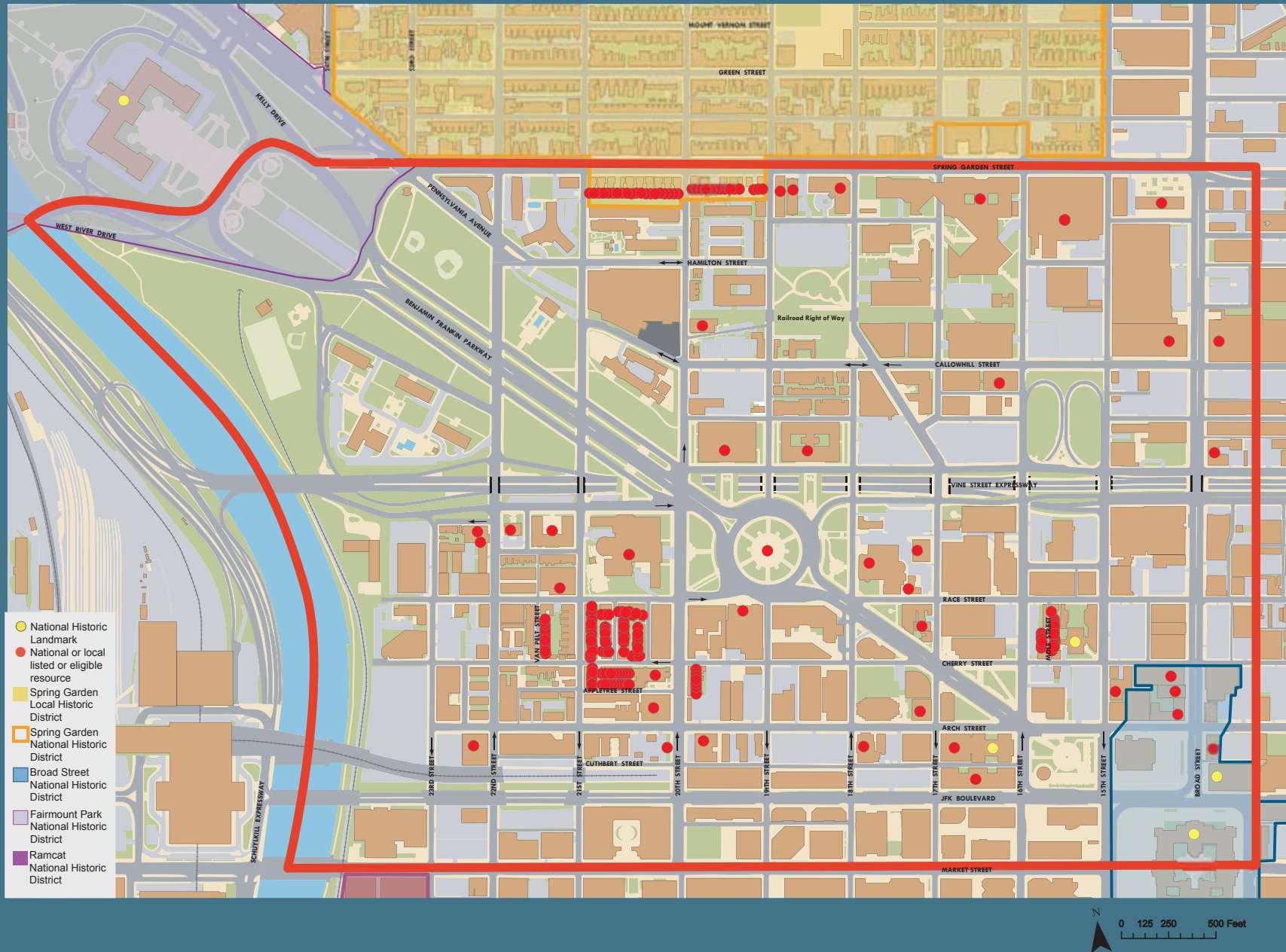
Two buildings in the Logan Square neighborhood recognized for their national significance are National Historic Landmarks, the Pennsylvania

Academy of Fine Arts and the Insurance Company of North America, now the Phoenix.

Philadelphia City Council created the Philadelphia Register of Historic Places in 1955. Listed buildings include landmark buildings, such as the Free Library and Family Court building as well as the Franklin Institute, Pennsylvania Academy of Fine Arts, and the Cathedral. Residential buildings on the register include the row of houses along Mole Street and the mid-19th century houses clustered around 21st and Cherry Streets. The core residential area of Logan Square includes several blocks where groups of attached rowhouses are locally listed. These include houses on Lambert Street, Woodstock Street, 21st Street, Van Pelt Street, Cherry Street and Appletree Street. This core residential area would likely be eligible for designation both as a national and local register historic district. The 1900 and 2000 blocks of Spring Garden Street are a part of the Spring Garden Historic District. The Swann Memorial Fountain is also on the Philadelphia Register.

The National Register designation is mostly honorary.

# HISTORIC RESOURCES



Review of work to a resource on the National Register only takes place when a federal agency is involved or federal monies are used for the work. However, if the property is income-producing, a rehabilitation project may be eligible for the Rehabilitation Tax Credit, up to 20% of the rehabilitation costs.

Protection of historic sites occurs at the local level. If a resource is listed on the Philadelphia Register, any work, including demolition, must receive the approval of the Philadelphia Historical Commission before a building permit can be issued.

**Map 6** shows the locations of properties with historic designation.

*Image 18. 1924-26 Arch Street.* ►

◀ *Map 6. Historic Resources*



## 3.6 PARKS AND OPEN SPACE

### Parks and Open Space Maintained by Fairmount Park Commission

#### *Logan Square*

Logan Square, the original northwest square of William Penn's plan originally, provided burial plots, pasturage, and a place for public executions. In 1825, the square was renamed for James Logan, who served as secretary to William Penn and chief justice to the Pennsylvania Supreme Court. As the neighborhood developed, the City improved the square, planting trees and installing walks and fences. Its size and appearance changed dramatically with the adoption of Jacques Gréber's Parkway plan of 1919. Basing his design on Paris's Place de la Concorde, Gréber created a large traffic circle in the square with space for a monument and formal gardens. The Square's western boundary, originally closed to 19th Street, was extended to 20th. In linking the Square to Center City and the upper Parkway, the immediate surrounding area changed from a residential

neighborhood to a locale for major cultural institutions.

Several distinct areas form Logan Square, including Aviator Park along 20th Street, the Circle, the area along Vine Street across from the Free Library of Philadelphia and the Family Court Building, and the Sister Cities Park along 18th Street.

Aviator Park was recently rehabilitated.

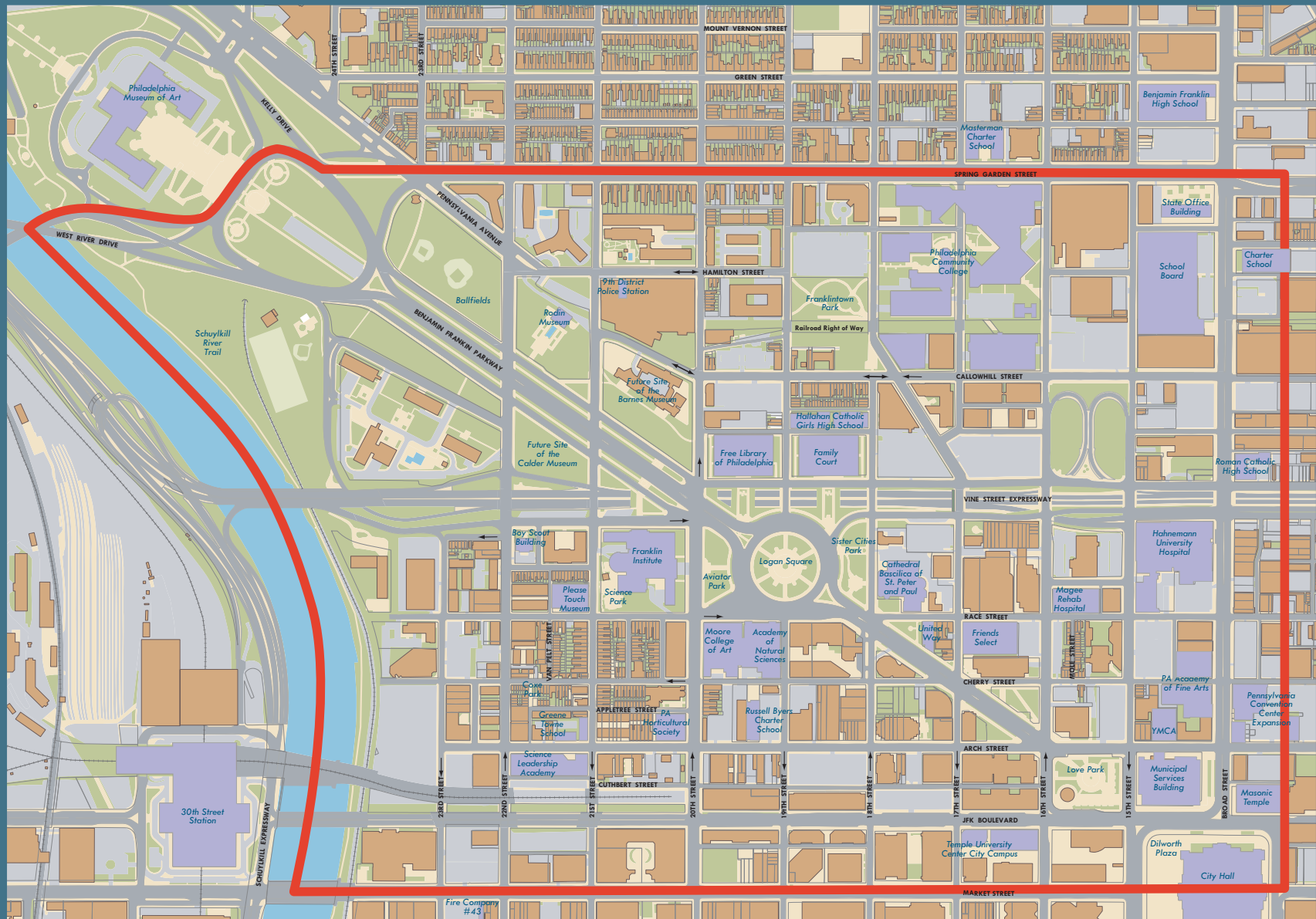
Improvements include new landscaping, signage, seating, trash receptacles, and improved paths to accommodate a diversity of uses by surrounding cultural and educational institutions. Moore College of Art showcases student work along the edge of Race Street.

In the center of Logan Circle is the Swann Memorial Fountain (1924), also known as The Fountain of the Three Rivers. It was created by Wilson Eyre, Jr. and sculptor Alexander Stirling Calder. Recent landscape enhancements to Logan Circle include the installation of new raised planting beds and lawns, gravel paths, and benches.

*Map 7. Institutions and Open Space* ►



# INSTITUTIONS AND OPEN SPACE



The area across Vine Street from the Free Library and Family Court has two major challenges. First, it is punctuated by structured openings overlooking I-676. Second, the area is used most by the chronic street homeless population. According to an October 2007 article in *The Philadelphia Inquirer*, the number of homeless people on Center City streets was the highest in 10 years, with the most recent City census revealing that there are more than 600 people sleeping on the streets of Center City. This amount is double the rate of three years ago.

Sister Cities Park is located in front of the Cathedral Basilica of Saints Peter and Paul. The Park has a 1976 monument commemorating the relationship between Florence, Tel Aviv, and Philadelphia. It shares the chronic street chronic homelessness problem seen in the park area across from the Free Library and Family Court. Plans have been developed for improvements to the park.

### *Benjamin Franklin Parkway*

The Center City District recently installed new streetlighting for autos and pedestrians, illuminated twelve major monuments and sculptures, and lit the facades of civic buildings from City Hall to the Philadelphia Museum of Art. Additional crosswalks have been installed to improve access to Swann Fountain in Logan Square. New interpretive signs and directional signs have also been installed. Landscape improvements have been completed at 16th Street and the Parkway and include an information and café pavilion with outdoor seating.

▼ *Images 19-22. Parks & Open Space, left to right: Schuylkill River Trail, Love Park, Logan Square, Franklin Town Park*





### *Franklin Town Park*

Franklin Town Park was designed by internationally known artist Athena Tacha as a sculpture garden to be appreciated from above by those living and working in future high-rise towers. The project was completed in 1991, but much of the anticipated development fronting the park has not yet occurred, with the exception of the Tivoli. A surface parking lot for Museum Towers fronts the park to the north, while to the south the park abuts a vacant lot and the railroad cut. To the east the park is fronted by the Community College parking lot. None of these uses encourage active use of the park. Tenants of the Tivoli have formed a Friends of Franklin Town Park to improve the park, including the planting of more trees and installing better lighting. The Friends have proposed renaming the park “Matthias Baldwin Park,” to commemorate the founder of the Baldwin Locomotive Works, which once occupied

several blocks near 15th and Spring Garden Streets. New development on abutting parcels will be key to increasing active use of the park.

### *JFK Plaza (LOVE Park)/Tourist Hospitality Center*

JFK Plaza is more commonly known as Love Park because of Robert Indiana's famous sculpture of the word "LOVE" at the southeast corner. A public park has been at this location since 1917 and has been redesigned several times. The general form we see today—large blocks of masonry, a fountain, and many sitting areas—resulted from Edmund Bacon's 1950s plan for the city. The round Tourist Hospitality Center was added to the park circa 1960. Once the LOVE statue was added in 1976, the park, much as we know it now, was complete. After renovations in 2002, the park is greener and more colorful with a variety of plantings. It is popular with the lunchtime crowd. The park has an underground parking garage



that is leased to a private operator and is in need of upgrades, including elevators. The park also has a very large homeless population. During the day, visitors must compete with significant numbers of people sleeping on the walls, benches, and grates, and at night the population explodes to dominate the space. The chronic street homeless population congregates at JFK Plaza in large part because it is a frequent site for food distributions by religious and other volunteer groups.

Homeless encampments have flourished beneath the overpass in this area, creating a major source of concern to the residential and commercial tenants at Park Towne Place, which has also seen significant homeless activity on its grounds abutting the Parkway.

#### *Schuylkill Banks/Schuylkill River Trail and Fairmount Park Athletic Field*

The Logan Square Neighborhood - Parkway abuts the Schuylkill Banks and Schuylkill River Trail, currently a 1.2-mile landscaped trail stretching from the Fairmount Water Works to Locust Street, with a planned extension to the South Street Bridge. Over

1,700 people jog, bike, rollerblade or walk along the trail each day, yet the connections to the Study Area are few. Next to the trail, in the vicinity of Park Towne Place and Race Street west of 23rd Street is a 3.5 acre athletic field that is a part of Fairmount Park and leased to Roman Catholic High School, a 3.5-acre facility in the vicinity of Park Towne Place, Race Street west of 23rd Street, and from the Market Street Bridge. Access from the Athletic Field going south to the Schuylkill River Park requires either crossing multiple lanes of traffic at Eakin's Oval or crossing the I-676 eastbound ramp, which does not have a crosswalk. There are old bridge abutments that have been considered for a pedestrian bridge over the tracks which would provide access from the Athletic Fields and from Park Towne Place to the trail.

*Image 23. Swann Fountain, Logan Square*









## Parks and Open Space Maintained by the Department of Recreation

Coxe Park, located at the corner of Cherry and Beechwood Streets, is a small, shaded tot lot with benches and playground equipment.

The 2.5 acre field located southeast of the Philadelphia Museum of Art, and northwest of the Schuylkill River Park Athletic Field, is the future site of Paine's Park. The park has received approval from the Art Commission and the Fairmount Park Commission. Construction of the \$6 million skate park is projected to begin in 2009.

Von Colln Memorial Field, an athletic field located along the Benjamin Franklin Parkway at 22nd Street, is valued highly for the recreational space it provides, especially for youth baseball leagues. Play equipment,

restrooms, and benches line the western side of the field. While the baseball fields play a central role in the neighborhood, they sit empty much of the time and the land could be better planned to accommodate additional uses.

Dilworth Plaza/City Hall Plaza is connected to the concourse underneath 15th Street and has an arcaded lower level with grand curving stairs linking the concourse with the plaza level above. The current conditions in the park discourage use except by the sizeable homeless population. Several plans for the plaza have been proposed to encourage greater use by neighborhood residents, office workers, and visitors.

▼ *Images 24-27. Parks & Open Space, left to right: Coxe Park, Von Colln Memorial Field, Dilworth Plaza, Science Park*



### *Plazas*

Most of the plazas in the area are located along Arch Street, JFK Boulevard and Market Street and are used by office workers during the week. Some were built to qualify for additional FAR in the zoning ordinance. Thomas Paine Plaza AKA Reyburn Plaza AKA MSB Plaza at 15th and Arch Streets features public art large-scale game pieces. Commerce Square at 21st and Market Street has a water feature and restaurants fronting onto it. Comcast's plaza stretches almost one quarter of a city block on JFK Boulevard between 17th and 18th Streets with a fountain and cafe. The Blue Cross building, Bell Atlantic building, and Penn Center all have public plazas. While some provide respite from the towering office buildings, and offer a place for retail and restaurants, others, such as the plazas associated with the Bell Atlantic Building, appear to be leftover landscaped space,

have few amenities, and are little used. The Mellon Building's Wintergarden is a public indoor plaza with landscaping at Market and 18th Streets.

### *Private Parks*

Science Park is managed by The Franklin Institute. Located at the northeast corner of 21st and Race Streets, Science Park offers interactive exhibits where children can learn about science by testing out devices, such as sundials, sand pendulums, hide-and-seek tunnels, and mini-periscopes. Only visitors with valid admission to The Franklin Institute may enter Science Park from inside the Museum.





## 3.7 SCHOOLS AND INSTITUTIONS

### Schools

Public schools within the neighborhood's boundaries include the Russell Byers Charter School, a public elementary school at 1911 Arch Street, and Science Leadership Academy High School, at 22nd and Arch Streets. Private and parochial schools include Friends Select (*Image 28*) at 17th and the Parkway, Hallahan Catholic Girls High School at 311 N. 19th Street, and Greene Towne Montessori School at 2121 Arch Street. Universities and colleges located in Logan Square include the Pennsylvania Academy of Fine Arts, Moore College of Art, Drexel University's College of Medicine at Hahnemann University Hospital, Philadelphia Community College, Temple University's Center City campus, and Strayer University.



Nonprofit arts and cultural organizations and their audiences have a significant economic and quality-of-life impact, and many of the largest cultural institutions in the region are located within the Study Area. The Benjamin Franklin Parkway serves as the cultural hub of the city with important destinations including the Philadelphia Museum of Art, the Academy of Natural Sciences, the Rodin Museum, the Franklin Institute, and the future Barnes Museum. The Broad Street area has a concentration of medical institutions such as Hahnemann University Hospital, Drexel University College of Medicine, and the Magee Rehabilitation Hospital. Other important institutions in the Study Area include the Friends Center and Quaker Meeting House at 15th and Race Streets, the United Way, the Free Library of Philadelphia, the Archdiocese of Philadelphia and Cathedral of SS. Peters and Paul, *Image 29*, and the Pennsylvania Horticultural Society. Other important churches include St. Clements Episcopal Church at 20th and Appletree Streets, and the Arch Street Presbyterian Church at 18th and Arch Streets. Institutions within the Study Area are indicated on *Map 7*.

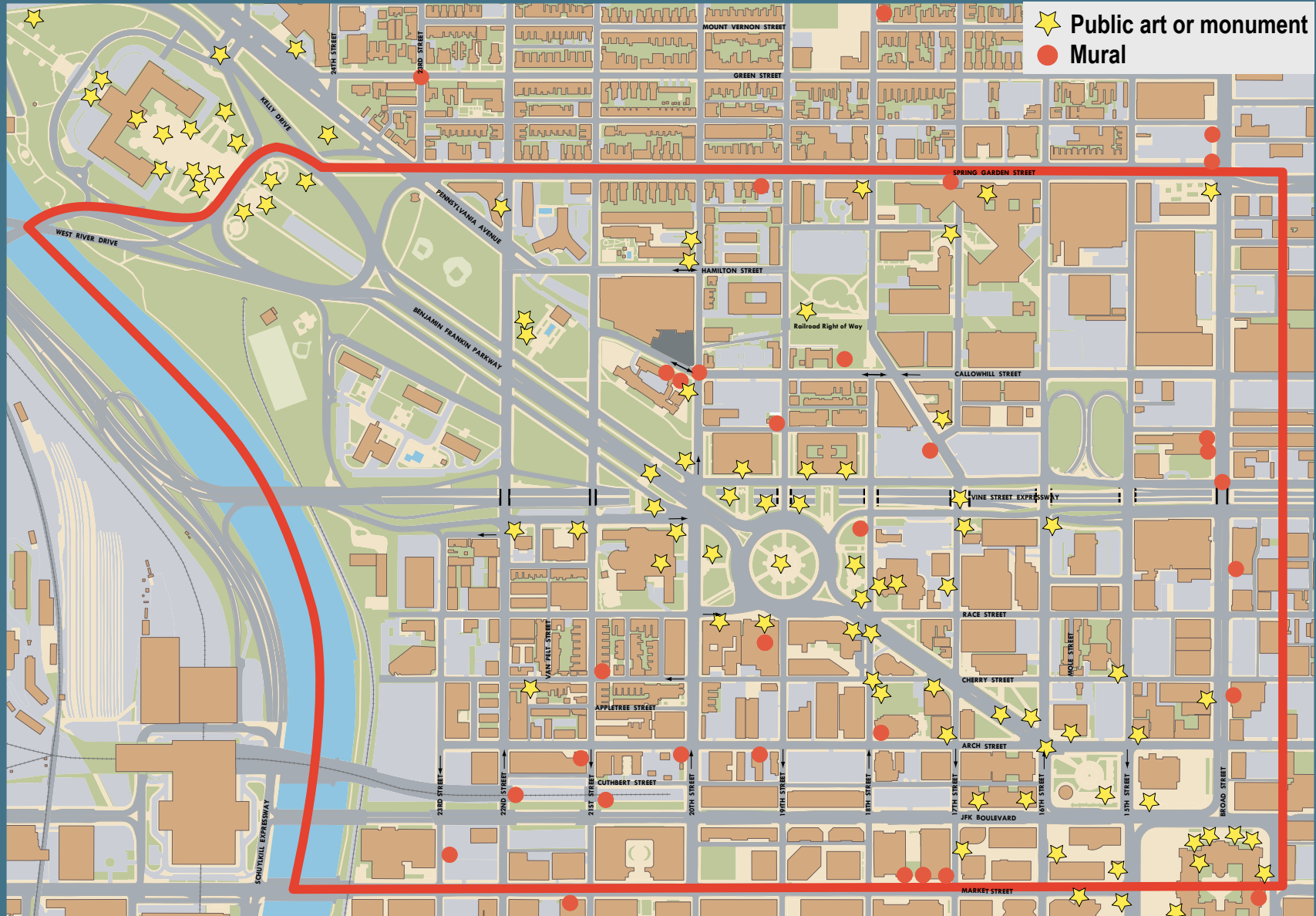
◀ *Image 28. Friends Select School.*

*Image 29. Cathedral Basilica of SS. Peter and Paul* ▶





# PUBLIC ART LOCATIONS





### 3.8 PUBLIC ART

Public art is well represented in the Logan Square neighborhood through sculpture, fountains, and murals. Several organizations are responsible for the creation of new art and maintaining and cataloguing these important cultural resources. The locations of public art in the Logan Square neighborhood appear on *Map 8*.

#### City of Philadelphia Public Art Program

The City's Percent for Art Ordinance applies to city-funded construction projects. Pieces are considered site-specific artworks and may occur as an integral part of the architecture of the project or as a separate element of the site. Examples in the Logan Square neighborhood are *Government of the People* by Jacques Lipchitz on the southeast corner of the Municipal Services Building at 16th and John F. Kennedy Boulevard; *Philadelphia Firsts* at One Parkway Building at 16th and Arch Streets, and *Your Move*, which is also located on the Municipal Services Building plaza.

◀ *Map 8. Public Art Locations*

#### Philadelphia Redevelopment Authority's Percent for Art Program

The Redevelopment Authority administers a Percent for Art Program where private developers developing Authority property allocate one percent of their construction costs for fine art.

Franklin Town Park is part of the Redevelopment Authority's Percent for Art Program. The Franklin Town Corporation worked with the Redevelopment Authority in 1991 to create the park, which in its entirety is considered public art. Maintenance of the park is funded through a small endowment (*Image 30*). The Authority's guidelines indicate that the original conceptual intent of the artist must be respected and maintained.



▲ *Image 30: Franklin Town Park Public Art*



## Fairmount Park Art Association

Founded in 1872, the Art Association commissions public art in Philadelphia. The Association has supported projects such as the design of the Benjamin Franklin Parkway, which was conceived as a grand boulevard punctuated by outdoor sculpture. Examples include *Three-Way Piece Number 1: Points* (1964) by Henry Moore on the Benjamin Franklin Parkway between 16th and 17th Streets; *The Gates of Hell* (1880-1917) by Auguste Rodin located at the Benjamin Franklin Parkway between 21st and 22nd Streets; and *Synergy* (1987) by Albert Paley at the Museum Towers on 18th and Spring Garden Streets.

## Philadelphia Mural Arts Program

The Philadelphia Mural Arts Program started in 1984 as a component of the Anti-Graffiti Network, a city-wide initiative to eradicate graffiti and address neighborhood blight. The Mural Arts Program has produced over 2,700 murals throughout the Philadelphia, more than any other city in the world. The Logan Square Neighborhood - Parkway has

approximately 40 murals within its boundaries (*Image 31*). Examples of murals in the Study Area include *Sea Change* (artist unknown) at 131 N. 21st Street; *Reach High and You Will Go Far* by Josh Sarantitis at 1926 Arch Street; and *Lifelong Learning* at 2 Franklinton Boulevard by Donald Gensler, Larissa Preston Danowitz, and Joshua Mays.



▲ *Image 31: Mural Art Installation at 1926 Arch Street*

*Image 32: Temporary Public Art along the Parkway.* ►







## 3. 9 TRANSPORTATION

### The Interstate Highway System

The interstate highway system represents both one of the greatest assets and biggest challenges of the Logan Square neighborhood (*Map 9*). Cutting east-west through the center of the district is the depressed Vine Expressway (I-676), which connects to I-76 just west of the Logan Square neighborhood and to I-95 (and the Benjamin Franklin Bridge) further east. It is the major distribution link for trips to and from Center City.

The on/off ramp facilities for the Vine Street Expressway represents a variety of ramp types and access locations:

- 22nd Street: Three-quarters of a diamond interchange plus an additional direct on-ramp from Eakins Oval (with additional access around Park Town Place). The eastbound off-ramp at this location also allows direct access to 23rd Street.
- 15th/16th Streets: The north side of the Vine Street Expressway contains a half-cloverleaf for access to/from the westbound expressway. On

the south side, the eastbound exit is a straight slip ramp distributing traffic to 15th Street.

- Broad Street: Just east of Broad Street is the straight on-ramp to I-676 eastbound, which is the only eastbound on-access in the Logan Square area with the exception of the connection from Eakins Oval and Park Towne Place.

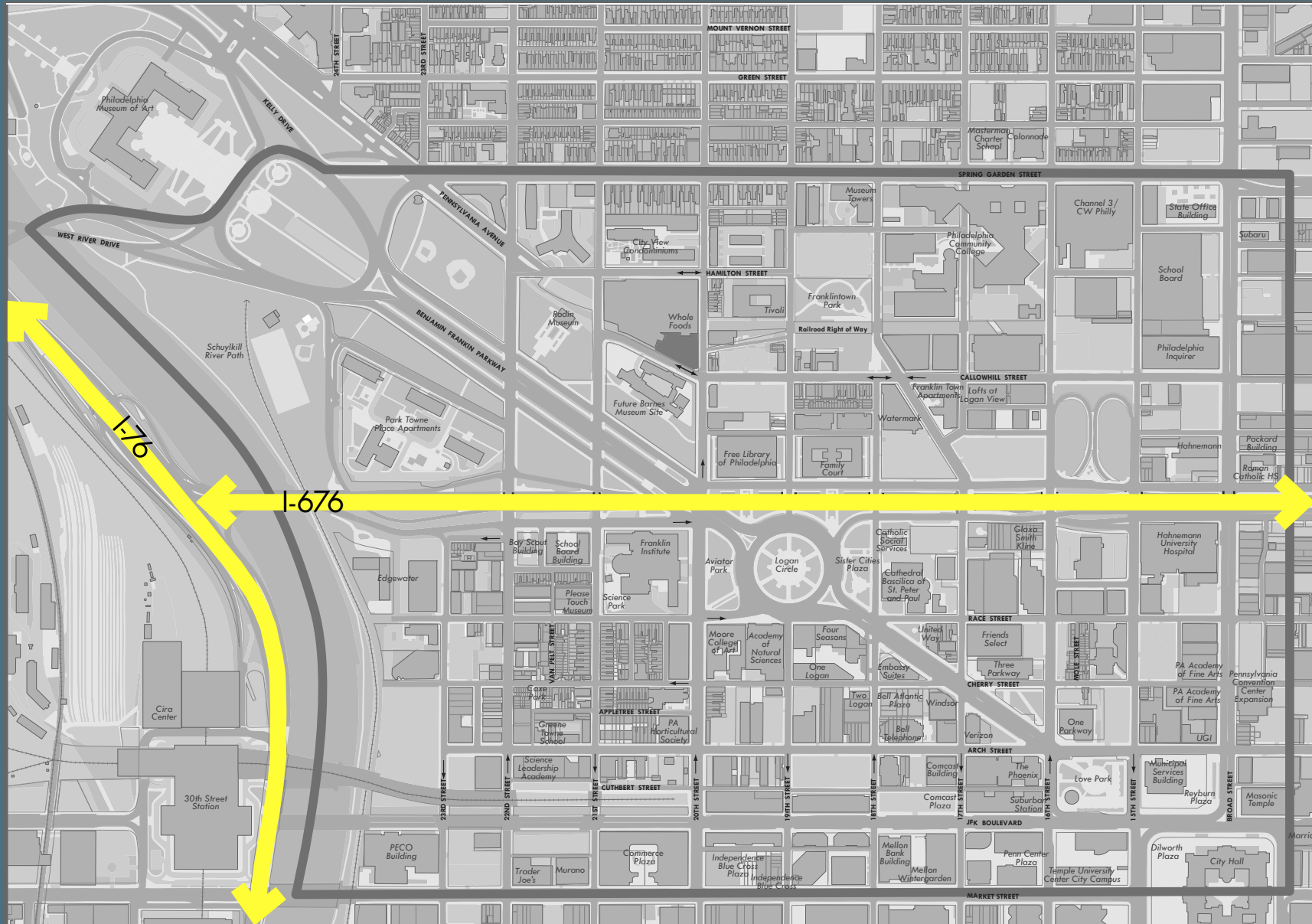
While direct access to the Vine Street Expressway is an amenity to Logan Square residents, it also represents a challenge in the additional traffic it brings to the area (most destined for the Center City office district) and the geometric design qualities it imposes on the urban landscape.

The Schuylkill Expressway (I-76) borders the Logan Square district to the west (across the Schuylkill River), with access from Schuylkill Avenue via JFK Boulevard, and from the Spring Garden Street Bridge via Eakins Oval.

*Map 9. Interstate Highway System.* 



# INTERSTATE HIGHWAY SYSTEM



## Surface Roadways

In addition to the interstate highway system, the Logan Square neighborhood includes several roadways that are major in terms of both their long-distance connectivity and their shorter-distance distribution function (*Image 33*). These include:

1. Broad Street: Bordering the district on the east side, Broad Street (State Highway 611) is a major connector to South Philadelphia, North Philadelphia and beyond.
2. Market Street and JFK Boulevard: The Market-JFK pair functions as State Highway 3 which connects through West Philadelphia and Upper Darby. Both streets are one-way from City Hall to 20th Street and two-way further west. They also function as the primary feeders into the Market Street West office district.
3. The Benjamin Franklin Parkway: The Parkway is a multi-functional boulevard connecting Center City with Fairmount Park. It also serves as a major expressway feeder and a primary commuting route to Northwest Philadelphia.
4. Spring Garden and Callowhill Streets: These streets represent the main crosstown connectors north of the Ben Franklin Parkway.
5. Vine Street: In addition to the Expressway, the Vine Street corridor contains a pair of one-way surface roads serving a distribution/circulation function to/from the Expressway.
6. Race and Arch Streets: Race and Arch Streets represent a one-way pair offering direct access to and from the Logan Square neighborhood south of the Parkway.
7. 15th/16th Streets: 15th and 16th Streets represent the main north-south connectors through the east side of the district, due to their access to the westbound on/off Vine Expressway ramps.
8. 20th Street: South of the Parkway, 20th Street is the sole two-way north-south connector.
9. 21st/22nd Street: These streets serve as a one-way pair offering access to the west side of the Market Street West office district. Southbound traffic on 21st Street is partly shared by 23rd Street due to its eastbound Vine Expressway off-ramp access.

*Image 33. Surface Roadway Conditions* 





1



2



3



4



5



6



7



8



9



## Pedestrian Circulation

Pedestrian facilities are generally strong throughout the district, although there are several significant barriers, including:

- the Benjamin Franklin Parkway, in particular Eakins Oval;
- the Vine Expressway Corridor, in particular where there are on and off-ramps intersecting with the urban street grid (*Images 34 & 35*);
- a number of additional complex intersections, including Pennsylvania Avenue/Spring Garden Street, Pennsylvania Avenue/22nd Street, Parkway/20th Street, and Vine Expressway/15th Street; and
- safety considerations at the underpasses of JFK Boulevard and the SEPTA rail corridor.

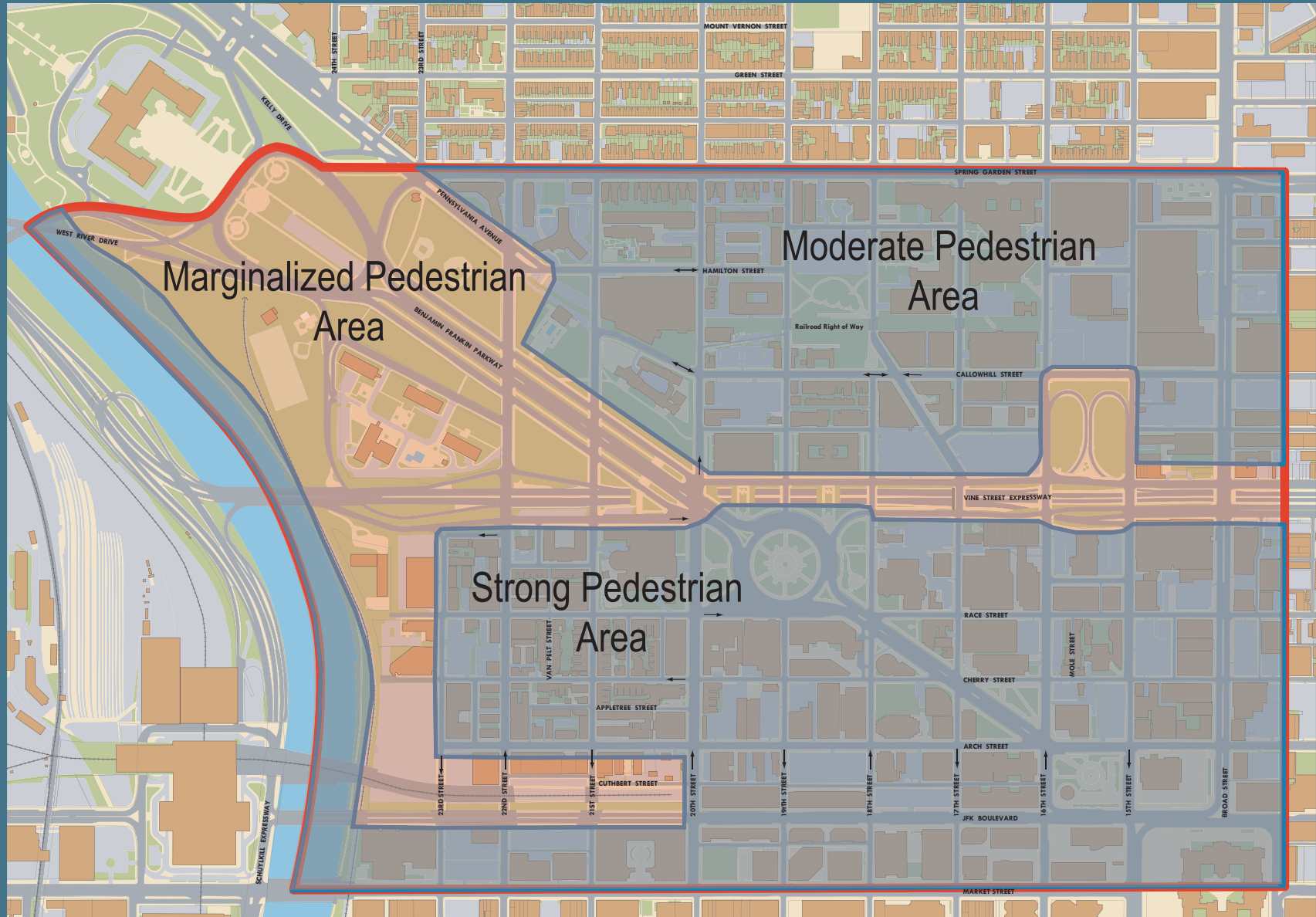
*Map 10* shows different pedestrian zones in the study area and their varying levels of success.

*Images 34 & 35. Vine Street Pedestrian Crossings* ►



*Map 10. Pedestrian Structure* ►

# PEDESTRIAN STRUCTURE



## Parking

Parking is intermittently available throughout the district in both public and private and on-street and off-street arrangements. The various configurations are summarized below:

### *Residential Parking*

Older residential buildings (primarily southwest of Logan Circle and in the northern end of the Logan Square District) do not have separate off-street parking facilities and rely mainly on the on-street parking supply. Many newer residential townhouses (primarily near 22nd Street south of the Parkway) contain their own small off-street parking facilities. Larger residential buildings rely on either surface lots or, particularly for newer buildings, podium garages beneath the residential floors of the building.

### *Institutional Parking*

While some of the major Parkway institutions have their own structured parking facilities (in particular, the Franklin Institute), the institutions, monthly parkers, rather than visitors, take up many of the spaces. The institutions also rely on metered on-street parking along the Parkway and in the

surrounding neighborhoods. The Philadelphia Museum of Art is in the process of completing its own parking garage. The City is working with the planners of the Free Library Expansion and the Barnes Museum to find an appropriate parking solution for these projects. The primary schools and churches in the district have limited off-street (surface) parking.

### *Business Parking*

Commercial land uses in the Market West office district rely heavily on dedicated and shared-use parking garages. Many of the large office/healthcare uses in the North Broad Street corridor have dedicated parking either in garages or in large surface lots. Smaller storefronts throughout the district generally rely on on-street parking, shared public parking, and foot traffic.

### *Public Parking*

In addition to the on-street supply, there are a number of shared public lots scattered throughout the district. The total public parking supply, in particular, the on-street supply, is very heavily utilized at various times of day.

*Map 11. Parking Issues* ►



## 76

## Parking Issues

Parking is often a critical issue in mixed-use urban neighborhoods, especially where alternative transportation options are not sufficient. Key issues include the following:

### *Private versus public parking*

Private parking is owned, maintained, and reserved for private use (such as a business, church, or apartment building). Often, private parking is underutilized at non-peak times unless arrangements have been made to share the spaces with other users. Public parking may be intended primarily for a particular use such as a business or residential development, but is not limited to specific users.

### *On-street versus off-street parking*

On-street parking is generally first-come/first-serve with metered operation and/or permit-based management for local residents. Off-street parking can be either public or private use. The cost of public

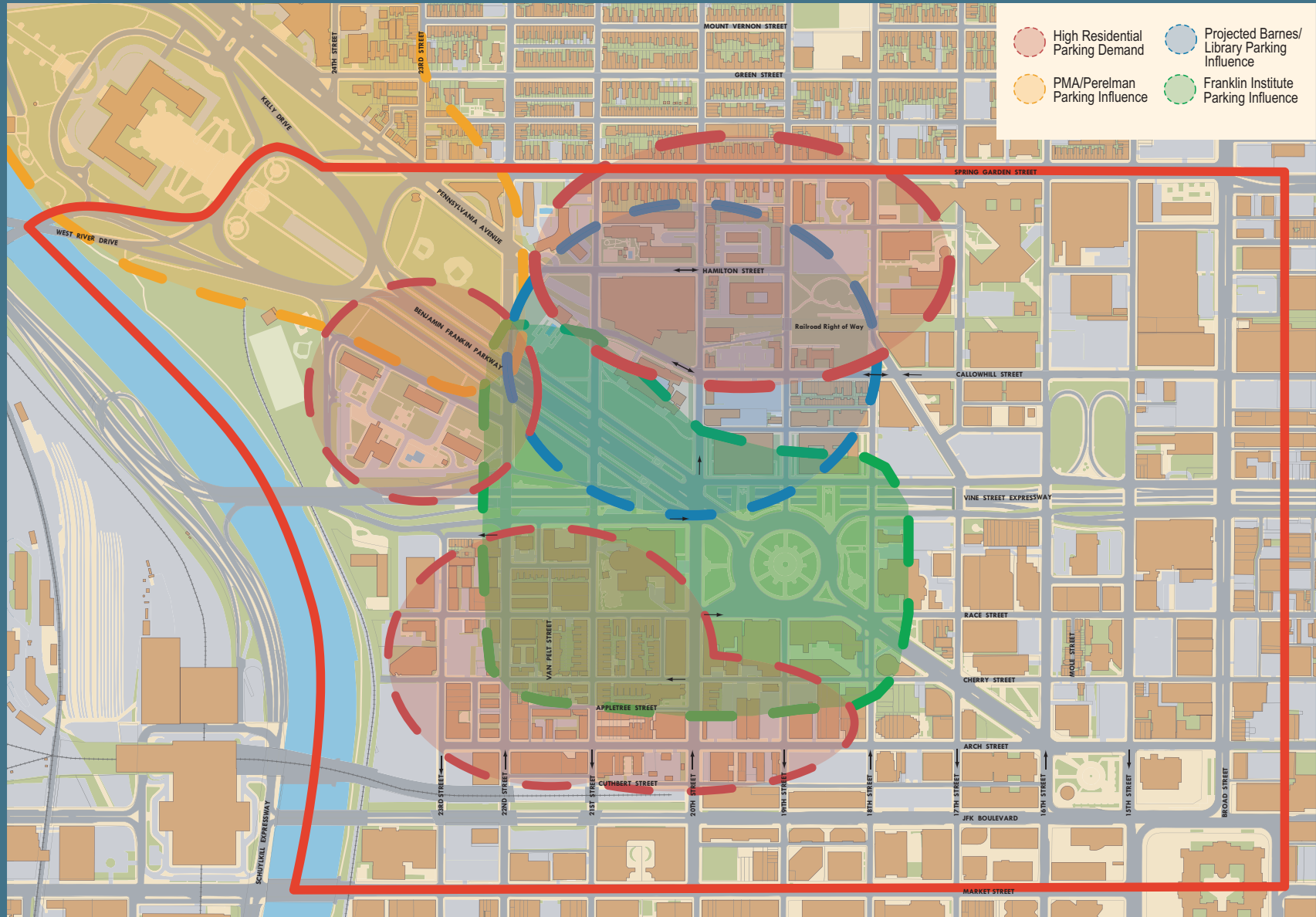
off-street parking is higher generally than that of on-street parking; consequently there is more consistent availability of spaces for those who are willing to pay a higher price.

### *Surface lots versus structured garage parking*

Surface parking is, relative to structured parking, inexpensive to build and maintain, yet is often unsightly and generally represents an inefficient use of urban land. Garage parking is expensive but represents a more efficient use of scarce urban land vis-à-vis surface parking. However, it is important to avoid negative visual impacts through location of parking underground, or screening by other uses, and careful attention to design detail.

Map 12. Weekend Parking Conflicts 

# WEEKEND PARKING CONFLICTS





### *Daytime (business) versus nighttime (residential) parking*

Daytime business parking can be short-term (retail or visitors) or long-term (office). Many neighborhood residents leave their cars at home during the day. Nighttime (also weekend) residential parking is classified as long-term (8 hours or more).

### *Short-term versus long-term parking*

Short-term parking (4 hours or less) is often effectively managed through use of meters, or parking pay stations. Long-term parking is generally supported through daily/monthly charges or residential permits. For public on-street parking, an optimized system allows most residents and visitors to locate a suitable parking space through moderate effort (less than 5-10 minutes search time) within four blocks of the home or destination. For public market-cost or private parking, the system is optimized when spaces are readily available within two blocks of the home or destination.

**Map 11** highlights existing parking issues within the Study Area.

**Map 12**, Weekend Parking Conflicts, highlights the specific issue of weekend parking, when high visitor demand for the area's institutional attractions overlaps with the base residential public parking supply. As indicated, this conflict is likely to present additional difficulties when the Barnes Museum and Library expansion projects are completed.

*Images 36, 37 & 38. Parking Options in the Study Area, clockwise from left: Structured, Surface and On-street Meters* ►







## Streets and Intersection Issues

### THREE MAIN ISSUES

- Geometric Design Quality
- Traffic: Volume and Speed
- Pedestrian Safety and Comfort

Geometric Design and Traffic Volume/Speed have a two-way, cause-and-effect relationship: high traffic volumes lead to generous geometric designs, which in turn lend themselves to higher speeds particularly when traffic is light.

Pedestrian Safety/Comfort is affected directly by both the Geometric Design and the Traffic Volume/Speed characteristics of particular streets. The desire for Pedestrian Safety/Comfort may influence the Geometric Design of streets (through dedication of more space to sidewalks and buffers) and the Traffic Volume/Speed.

The challenge in urban neighborhoods is to strike an appropriate balance among these often contradictory design criteria. **Map 13** highlights issues within the Study Area with respect to Geometric Design, Traffic Volume/Speed, and

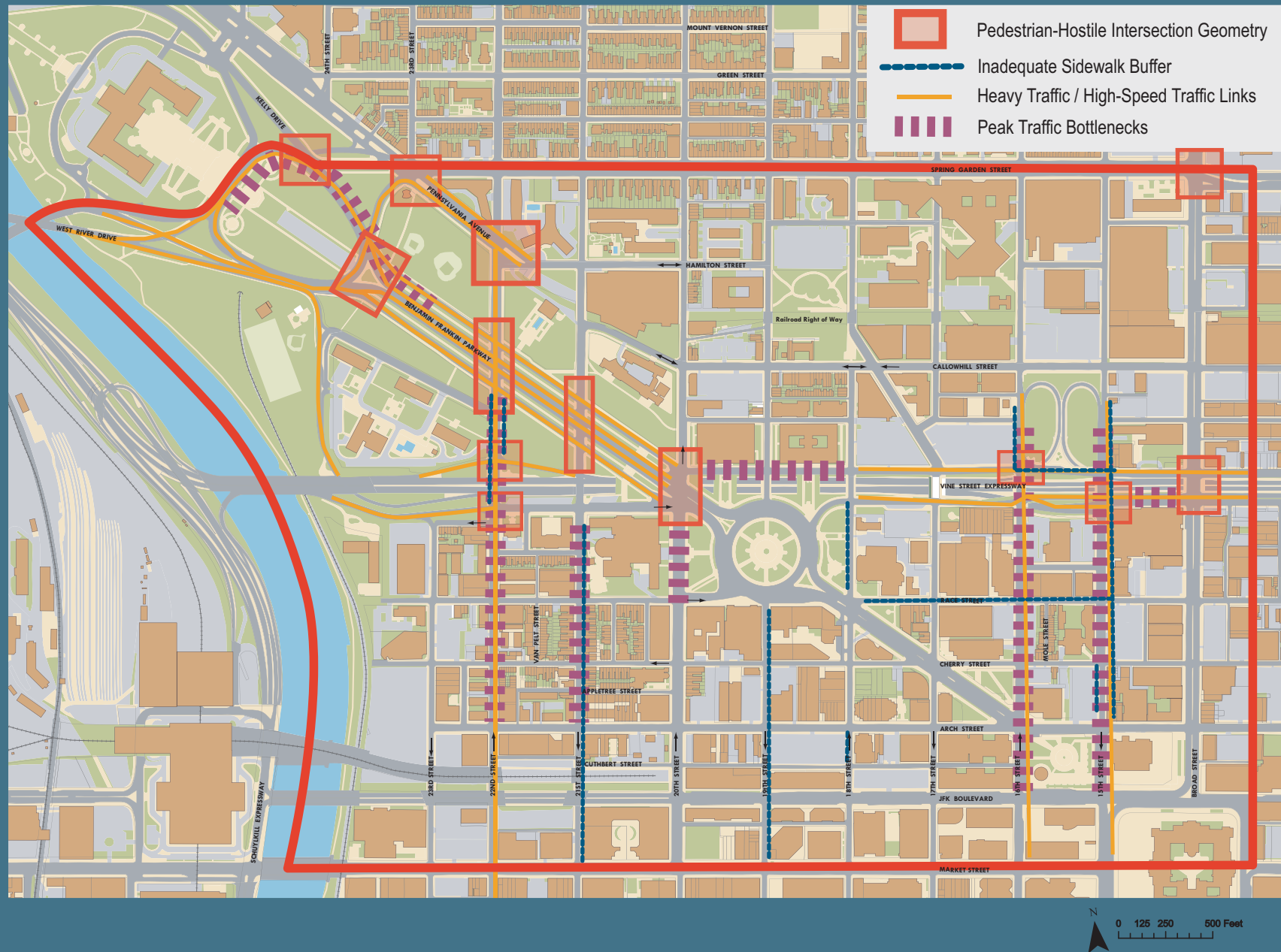
Pedestrian Safety/Comfort, and identifies areas where the physical characteristics of specific streets and intersections are out of balance. **Map 10**, Pedestrian Structure, focuses specifically on the effect of “unbalanced” corridors in terms of “breaking” the continuity of an otherwise pedestrian friendly neighborhood. Depending upon the specific deficiencies, potential solutions could include any of the following:

- reconfiguration of cross-sections for safer pedestrian corridors and/or 'calmer' traffic;
- geometric alteration of intersections for shorter pedestrian crossings, slower turning movements, and/or more efficient traffic flow;
- better control of pedestrian/traffic movements; and/or
- site-sensitive capacity increases, where possible.

**Map 13. Street & Intersection Issues** ►



# STREET & INTERSECTION ISSUES



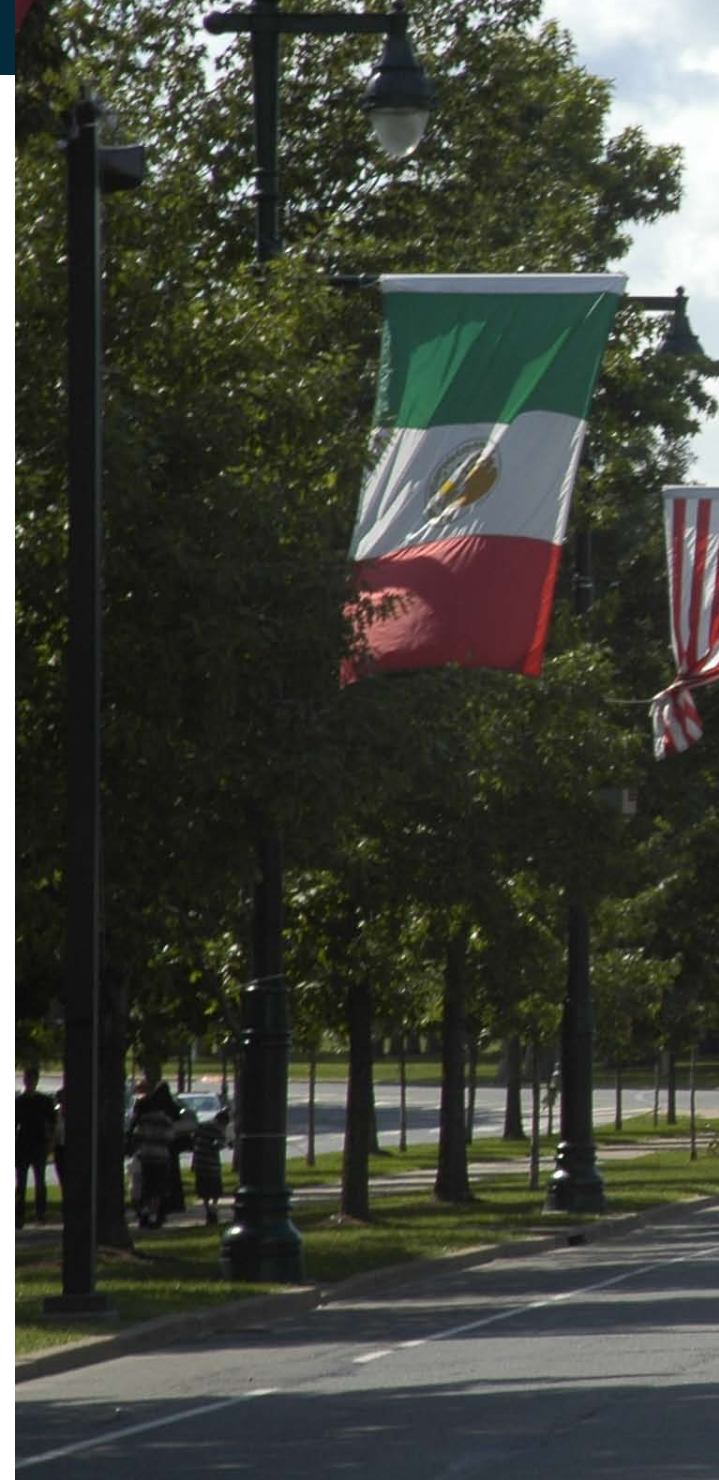
## Benjamin Franklin Parkway

The Parkway is home to a number of intersections (including 20th Street, 21st Street, 22nd Street, and all of Eakins Oval) where pedestrian conditions are significantly deficient relative to the rest of the Logan Square neighborhood.

The Benjamin Franklin Parkway has been the subject of numerous enhancement plans over the years. These are summarized in *Map 14*.

The institutions, as well as the residents, are focused on re-establishing the Parkway as a premier walking corridor, and to re-connect the neighborhood bifurcated by it. Moreover, the institutions and neighbors are each concerned about the effects of event-day Parkway closures on area circulation. *Map 15* highlights the alternate traffic patterns associated with such closures and their extended effects.

Image 40. Benjamin Franklin Parkway 

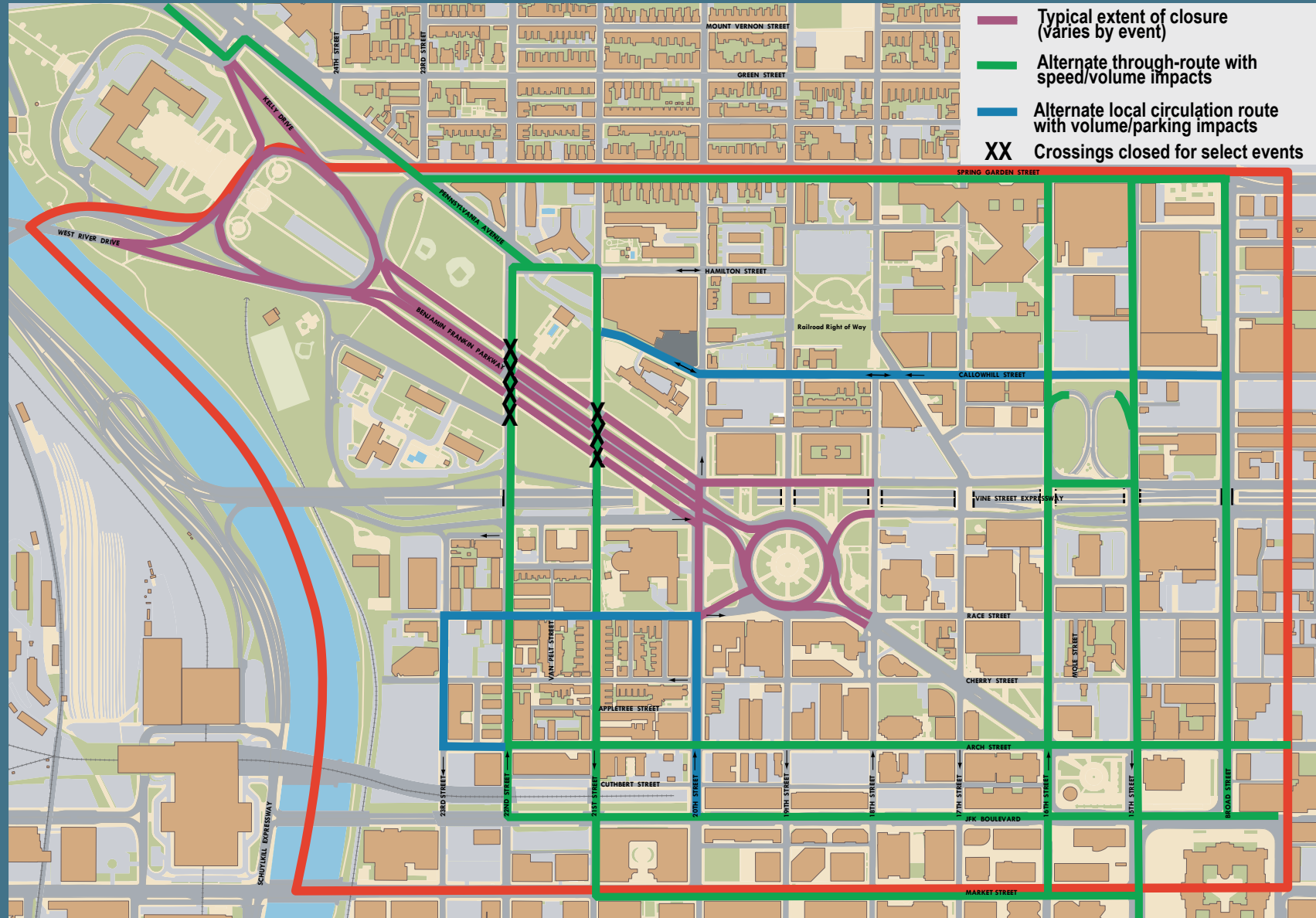








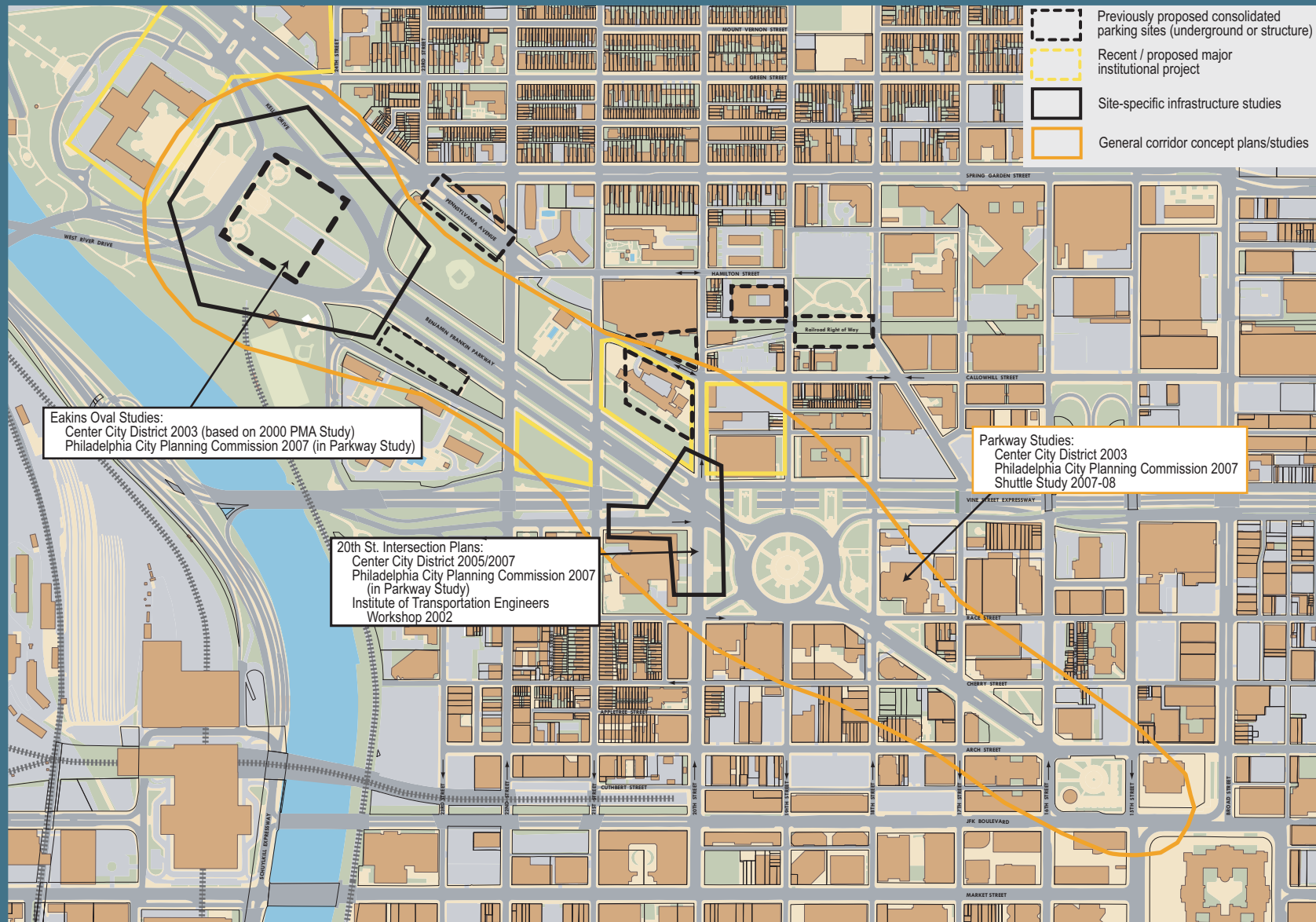
# IMPACT OF PARKWAY SPECIAL EVENTS CLOSURES



**Map 14. Impact of Parkway Special Events Closures**



# BENJAMIN FRANKLIN PARKWAY PREVIOUS STUDIES



**Map 15. Benjamin Franklin Parkway Previous Studies**



## Transportation Impacts of New Development

The Streets and Intersection and Parking sections highlighted specific challenges within the Study Area with respect to traffic volumes and parking. With new development planned for the neighborhood, these issues are likely to be further stressed in the absence of fundamental changes in travel behavior. However, given that any changes would take long periods to develop, as they would rely not only on individual behavior but significant upgrades to the transit and bicycle systems, it is important to address the degree to which the proposed developments will impact the near-term, day-to-day operability of the current transportation network.

**Maps 16 and 17** highlight proposed development in 2007 within the Study Area overlaid on a map of existing traffic trouble spots. Also indicated are the anticipated primary routes between each of the proposed developments and the regional freeway system trip patterns that are likely to represent the majority of automobile trips during the critical peak periods.

Also indicated in **Map 16** are estimated numbers of auto trips for each of the proposed developments for the AM and PM peak-hours.

The primary concern with respect to generated trips is the original River City proposal (now abandoned), which would have potentially generated 1,000 to 1,500 new trips during the critical peak hours. Also significant are the trips associated with the proposed Whole Foods mixed-use development, particularly for the PM peak hour, though its proximity to the Vine Street Expressway will limit much of the overall impact to the adjoining streets and freeway ramps.

**Map 17** overlays the same proposed developments on a map of current high-demand parking areas. Noted within **Map 17** are each development's proposed number of new parking spaces (where available) and the predicted number of spaces needed based on the trip generation analysis. For River City, the overall projected parking demand was calculated by the consultant team as roughly equal to the proposed supply.



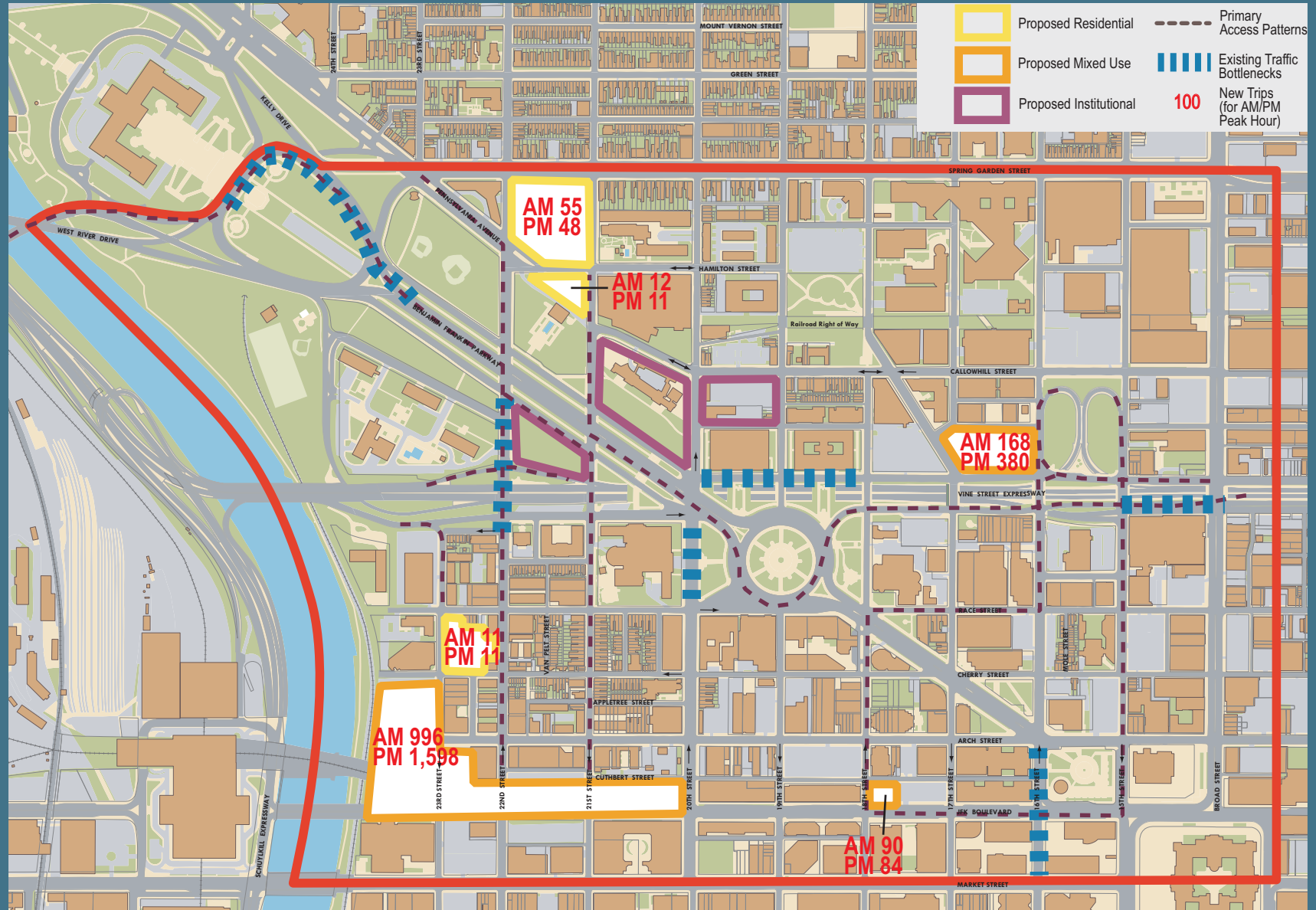
The purpose of this analysis is not to halt development, but rather to highlight potential actions that would help minimize its overall impact, such as:

- new shared parking facilities (publicly and privately funded);
- reconfiguration of streets for increased parking or traffic capacity, as needed and where possible;
- measures that support non-auto-based circulation (such as bicycle facilities and comfortable walking environments) to minimize unnecessary car travel;
- measures that support the circulation needs of existing residents, such as enhanced local transit services; and
- measures that will make the proposed developments ultimately less auto-dependent, including better regional transit connections.

*Image 41. Philadelphia Transit Station Sign* ►



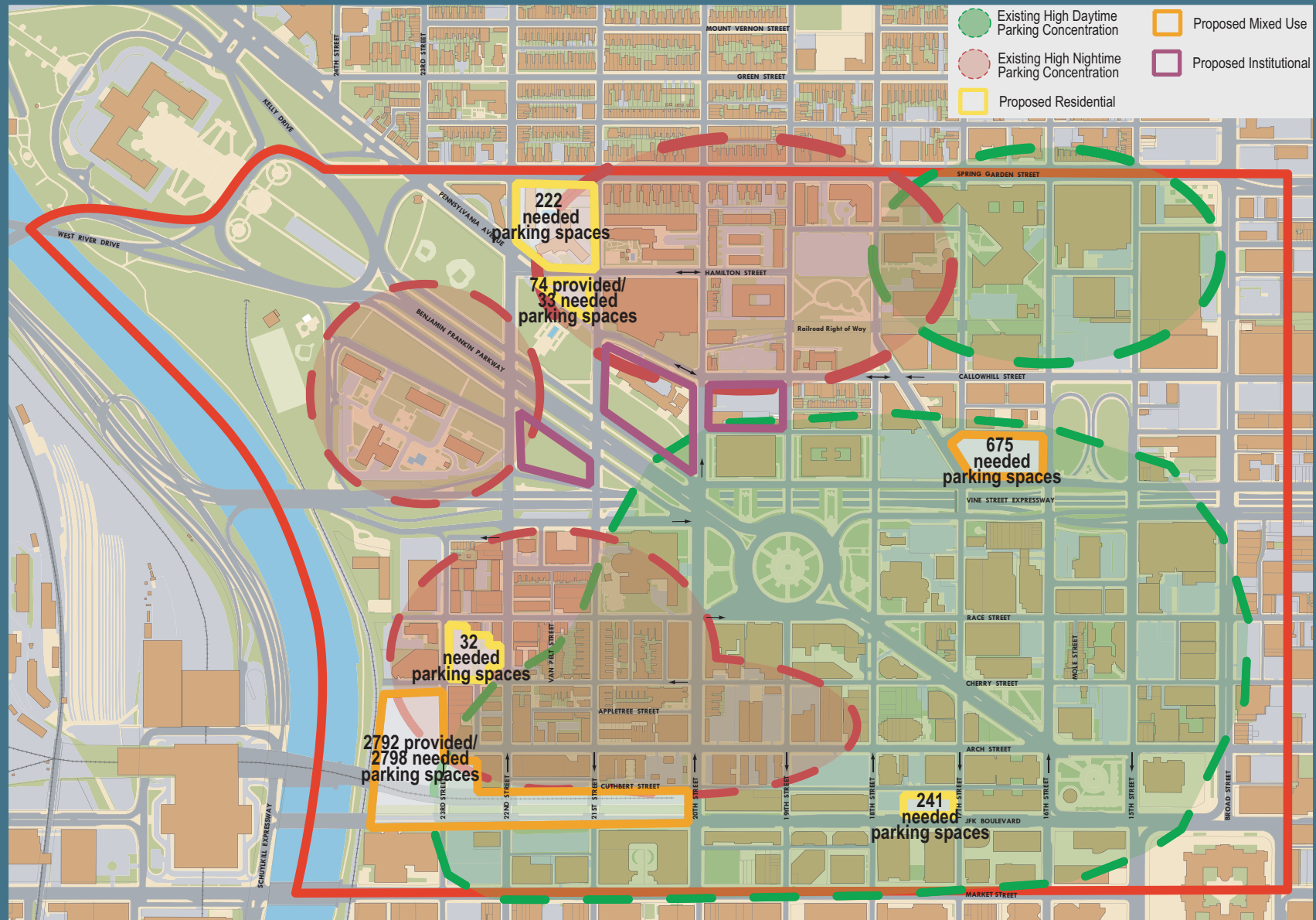
# PROPOSED DEVELOPMENT WITH TRAFFIC ISSUES



▲ Map 16. Proposed Development with Traffic Issues



# PROPOSED DEVELOPMENT WITH PARKING ISSUES



▲ Map 17. Proposed Development with Parking Issues





## Transit and Bicycle Systems

Enhancement of the transit and bicycle systems would benefit the Logan Square neighborhood in two primary ways: It would provide alternate transportation options for existing residents, primarily for medium-distance travel, and it would limit the overall dependency of new development on automobile access and parking.

The existing transit system is illustrated in *Map 18*.

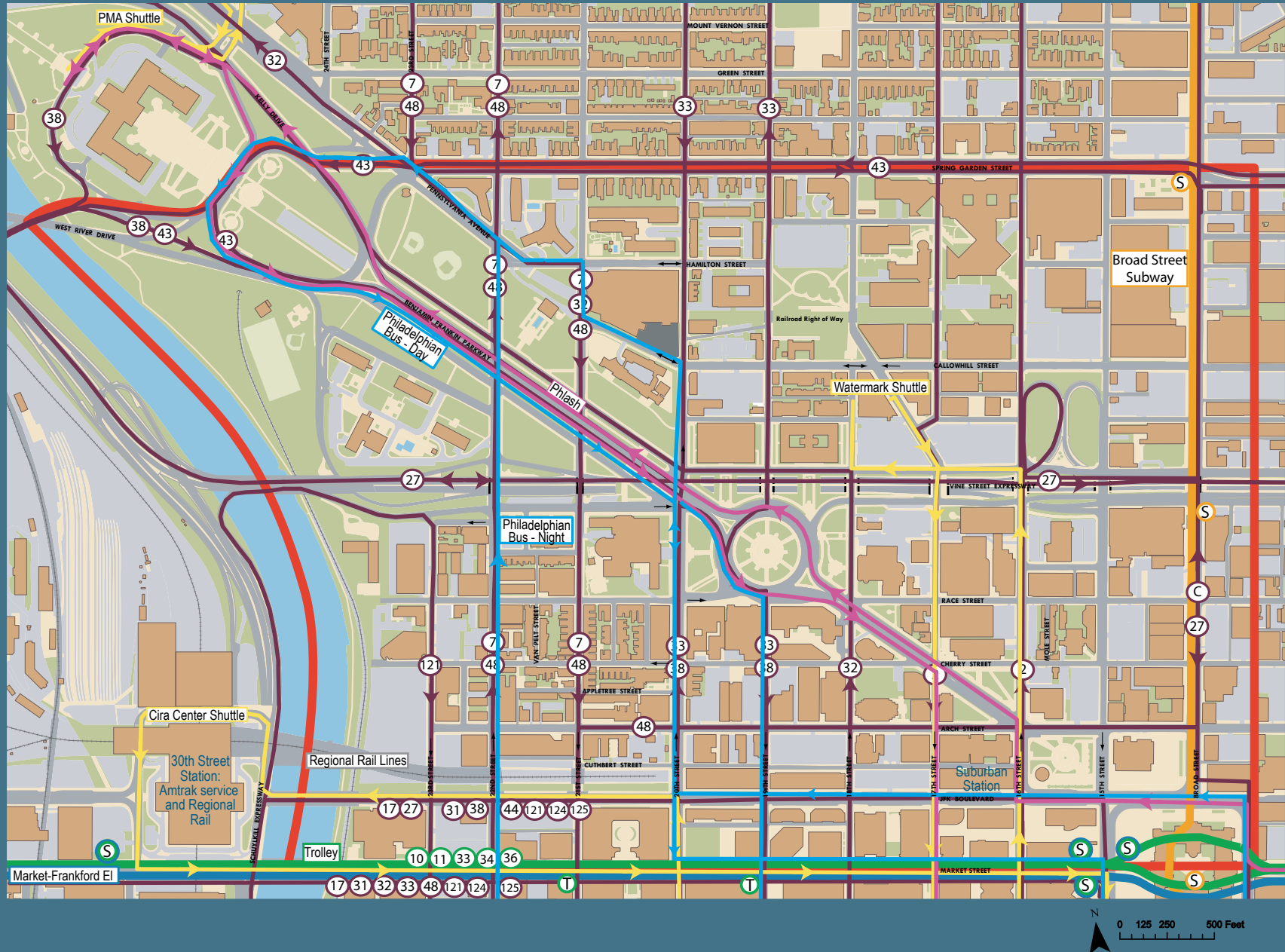
In the southeast corner of the study area, the convergence of the Broad Street Subway, Market-Frankford El, the trolleys, and regional rail service provides excellent regional and local transit access. The Broad Street Line has stops along the eastern edge of the study area at City Hall, Vine and Spring Garden Streets. Along Market Street there is a subway stop at 15th Street, and three trolley stops, at 22nd Street, 19th Street, and 15th Street (City Hall) that connect the neighborhood to 30th Street Station with access to regional rail and Amtrak. Regional rail service is also available at Suburban Station,

which was recently improved and now is the only rail service in the study area with elevators for handicap accessibility. SEPTA has studied providing rail service along the unused City Branch railroad line that runs below Pennsylvania Avenue into Center City and out to 52nd Street. Rubber-tired electric transit could also be considered for this corridor. A new Market Frankford El stop on Market Street at 20th Street has been proposed at various times in the past.

SEPTA bus service runs east-west on Market and Spring Garden Street across the entire study area. The 7 and 2 and 27 routes run north-south on 22nd/21st, 17th/16th and 16th/Broad Streets, respectively, through the study area and continue south of the study area. The 48, 38, 32, and 33 run north-south and then east-west along Market, JFK Arch and the Parkway. Many residents use bus service to commute to work, and issues such as sporadic service, crush loads, and a lengthy trip due to congestion can be discouraging for riders. However, dedicated funding has allowed SEPTA to add service in recent months on certain routes.

*Map 18. Existing Transit System* 

# EXISTING TRANSIT SYSTEM



A number of the large residential condominiums or apartment buildings offer shuttle service to Center City destinations, such as West Market Street, South Broad Street, and Walnut Street shopping district. The Watermark and the Philadelphian have set routes (*Image 42*), while the Park Towne Place provides a regular shuttle service.

Many visitors use the Phlash service to travel from the Historic District/Independence Mall up to the Parkway. The Parkway Council Foundation has been studying options for a transit circulator that could build on and expand the Phlash service.

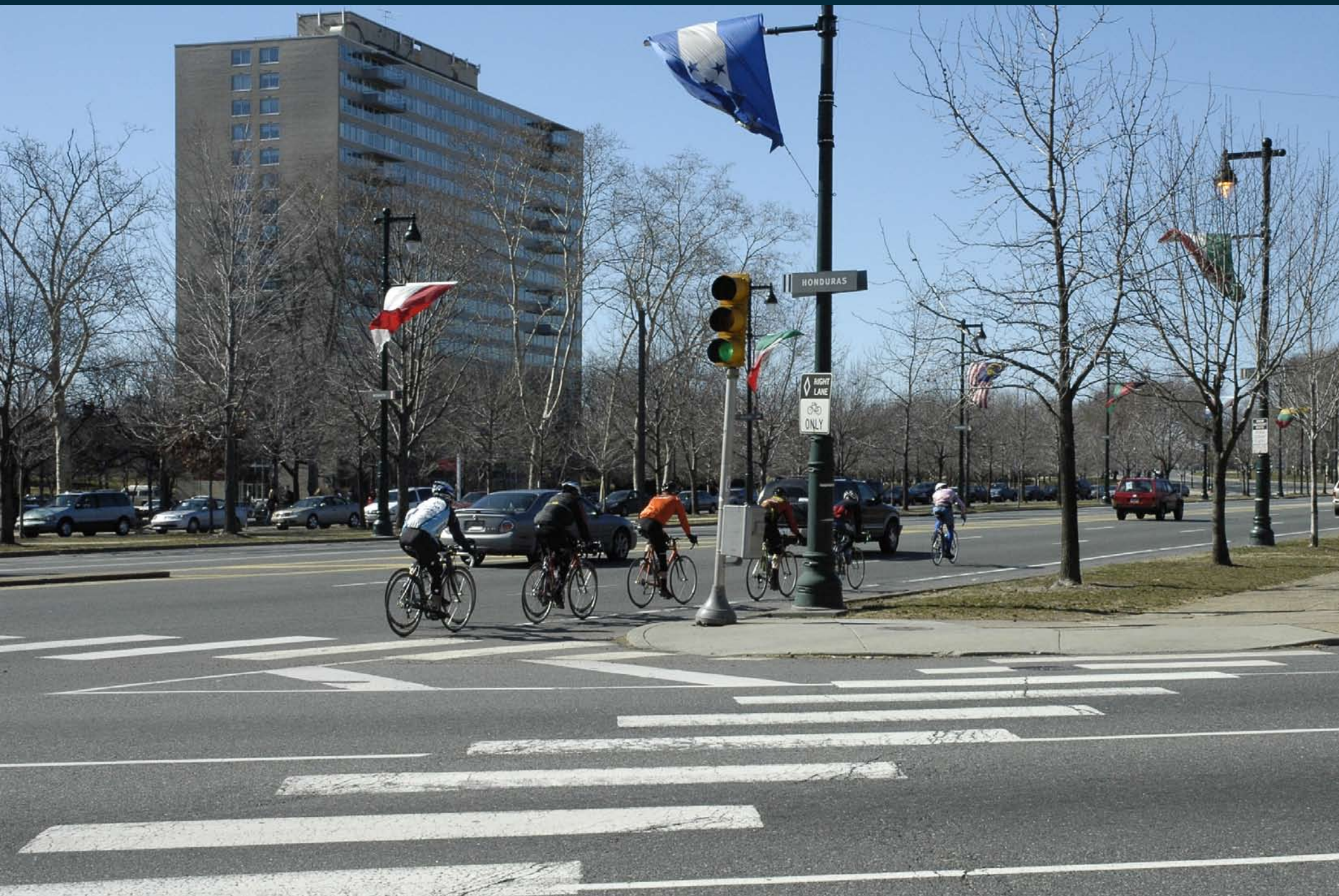
Increasing numbers of bicycle commuters and recreational bicyclists are using the neighborhood streets and the Schuylkill Trail. Bicycle lanes have been marked on some of the neighborhood streets, such as Spring Garden Street, Benjamin Franklin Parkway (*Image 43*), and much of 22nd Street. Bicycle access to the Schuylkill River Trail is available at Race Street, Market Street, and Martin Luther King Drive (formerly West River Drive).



▲ *Image 42. The Philadelphian Shuttle Bus*

*Image 43. Bicycles on the Parkway* ►









# 4

## PLANNING PRINCIPLES & GOALS

Plan goals and principles were developed in response to an evaluation of the existing conditions.

## 4.1 Principles

Goals were developed to address the following six broad neighborhood-wide principles.

1. Enhance the diverse functions of the Logan Square Neighborhood - Parkway as a thriving Center City residential neighborhood, downtown office and employment center, and international cultural destination.
2. Assure that any adverse impacts on the quality of life of the neighborhood that may result from each of these functions are adequately resolved.
3. Improve communications between the stakeholders representing the diverse functions of the neighborhood, so that cooperation can be enhanced and issues of conflict can be expeditiously resolved.
4. Conserve the character of established residential neighborhoods.
5. Encourage a diverse residential population, in terms of age, income and other attributes.
6. Create a sustainable community in terms of environmental impacts of buildings and infrastructure.

◀ *Image 44. Logan Square, Bird's-eye View*



## 4.2 Quality of Life Goals

1. Reduce the adverse impacts of Parkway events by limiting the number of events and assuring that permitted events contribute to the cultural life of the Parkway.
2. Improve recycling and trash management for both residential and commercial uses.
3. Address the problem of chronic homelessness in the neighborhood, in particular along the Parkway and North Broad Street areas, by encouraging evening activities, developing more active uses and programming of open space, and improving landscaping and lighting, as well as through appropriate interventions, including the provision of necessary services and enforcement of applicable laws and regulations.
4. Encourage high-quality neighborhood schools.
5. Encourage the continued development of the Parkway as an international cultural destination, the “Museum Mile,” with attractive landscaping and small scale-retail amenities (e.g. cafes).
6. Continue to improve access to and enhance recreational facilities along the Schuylkill River Trail.
7. Increase the utilization of FranklinTown Park and realize its potential as a community resource through encouragement of appropriately scaled residential and retail development on abutting sites.
8. Continue and enhance the successful program of public art within the Logan Square neighborhood.

Image 45. Coxe Park







9. Install trees, other landscaping, lighting, appropriately designed street furniture, and lighting to provide a greater sense of comfort, safety, and cleanliness to the neighborhood.
10. Encourage more restaurants and other neighborhood-oriented commercial uses within the Logan Square-Parkway community.
11. Establish “Green Streets” that link the Squares of Center City and connect public spaces within the Logan Square neighborhood.
12. Increase community-oriented recreational space (*Image 45*).

### 4.3. Traffic and Transportation Goals

1. Minimize adverse impacts of traffic and parking at museum attractions, through improved transit service to Parkway institutions and adequate parking, including possible weekend use of nearby non-residential parking facilities.
2. Enhance the walkability and pedestrian safety of the neighborhood.
3. Reduce the barrier effect of the Benjamin Franklin Parkway on pedestrian movement between residential areas to the north and south by reducing the number of vehicle travel lanes, as well as by providing pedestrian-oriented landscaping and appropriate small-scale single-story commercial development (e.g. cafes) at north-south streets.
4. Reduce the barrier effect of the Vine Street Expressway on north-south pedestrian movement by making streetscape improvements at overpasses and by covering open sections of the depressed expressway.
5. Improve pedestrian connections, including across North Broad Street to the Callowhill neighborhood and Convention Center expansion, and reduce the barrier effect of JFK Boulevard and the SEPTA tracks west of 20th Streets, which separate Logan Square from neighborhoods to the south, through streetscape improvements, vertical connections, and development design controls on north-south streets (*Image 46*).
6. Enhance transit service by increasing the frequency of bus, regional rail, subway and trolley routes; improving station areas; enhancing bus stops with seating, real-time information, and shelters; and supporting new transit initiatives serving the Parkway.

*Image 46. SEPTA tracks underpass* ►







7. Improve bicycling conditions by providing continuous east-west and north-south bike lanes, bicycle racks, and access to the Schuylkill River Path.
8. Decrease traffic speeds on streets through the Logan Square neighborhood, including the Parkway.
9. Address traffic congestion at critical intersections and corridors throughout the Logan Square neighborhood, including the Parkway and the intersection of I-676 West at 22nd Street.
10. Address parking deficiencies in the neighborhood through encouraging car share and improving management of on-street parking spaces.
11. Encourage Parkway Institutions to collectively address parking issues by creating new parking facilities or setting up alternative access options.

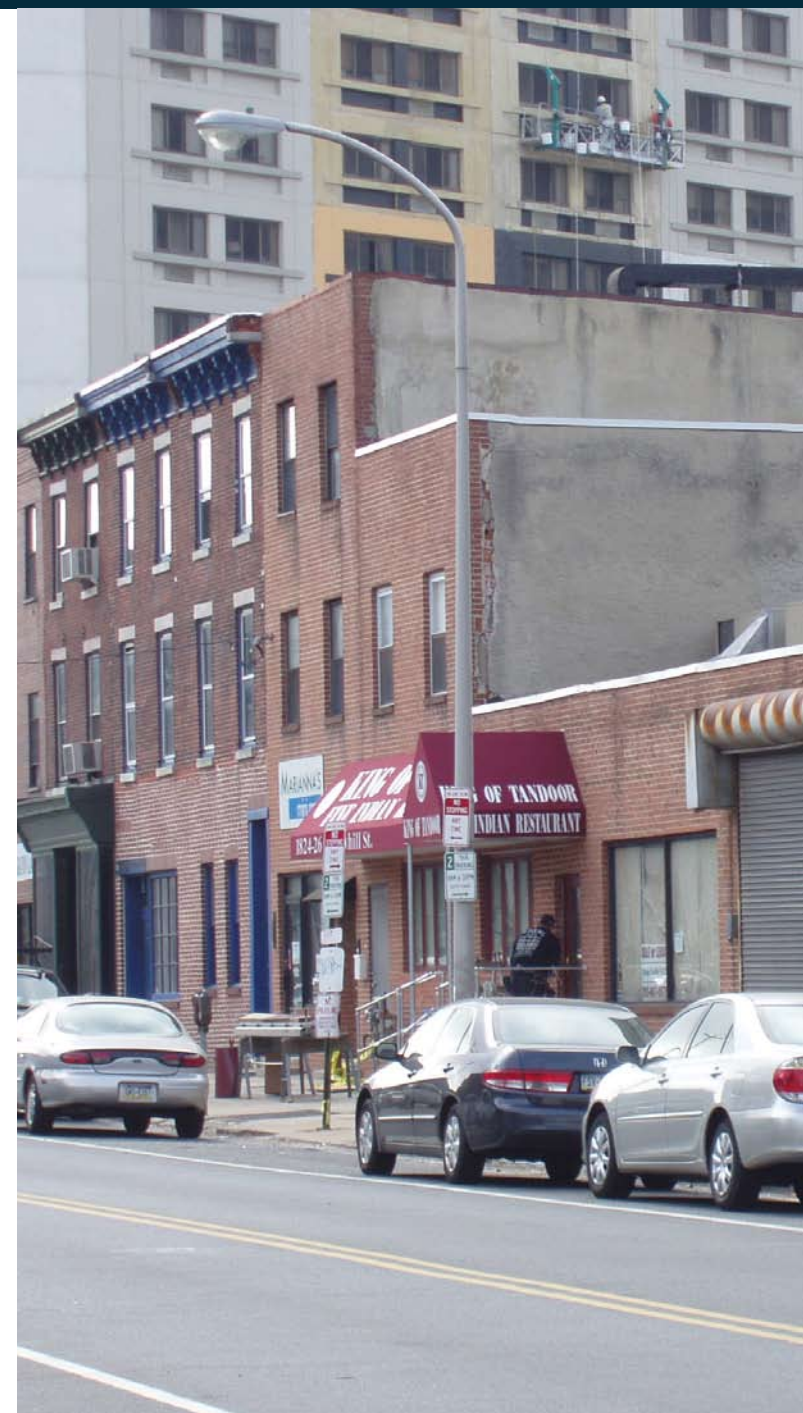
## 4.4 Development Goals

1. Encourage a balanced variety of housing types, including housing types that can attract more families with school age children, to increase age diversity within the community.
2. Ensure that new development enhances the existing community and minimizes the adverse impacts on daylight and sunlight access, nighttime light pollution, and views enjoyed by existing residential buildings through appropriate development density, building height, massing, building spacing and location, and street level design.
3. Encourage highest-intensity commercial mixed-use development along the Market Street and JFK Boulevard corridor south of (and including) the SEPTA tracks, in areas east of 19th Street and south of Race and east of 18th between Race and Callowhill (including air rights development over the Vine Expressway), and along the North Broad Street corridor west to 16th Street.
4. Discourage high-intensity office-core-type commercial/mixed-use in other areas of the community not included in (3) development, and instead encourage appropriate density residential or mixed-use developments.
5. Enhance and protect the small-scale low-rise residential character of the historic residential core of Logan Square.
6. Encourage the development of Callowhill Street between 18th and 21st Street as a lively dining and arts-oriented, pedestrian-scaled commercial corridor that can serve both neighborhood residents and visitors to Parkway institutions.
7. Eliminate the adverse aesthetic impacts of

*Images 47 & 48. Tivoli Townhouse Condominiums (left); and Callowhill Street (right)*







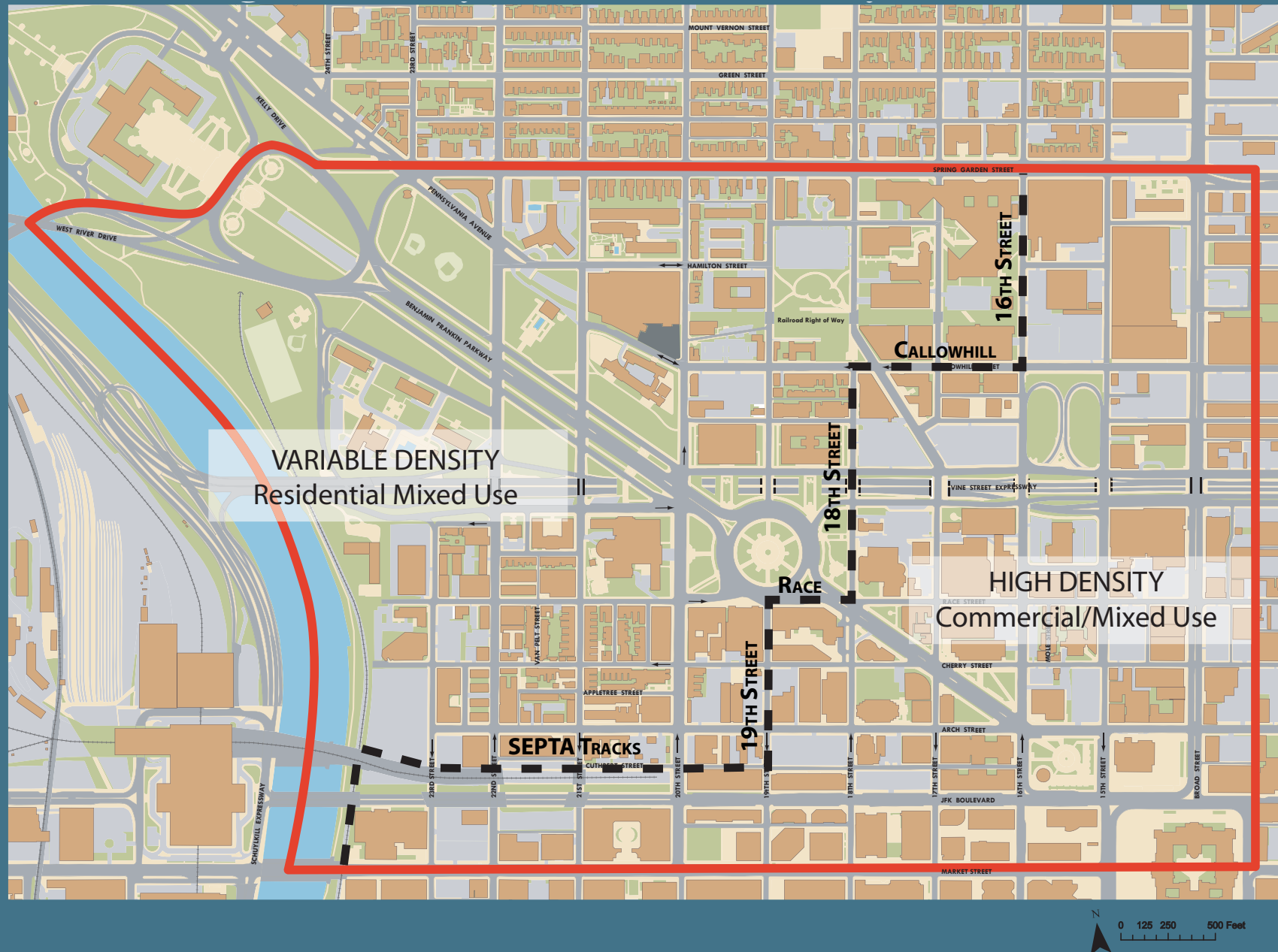


exposed above-ground parking garages by requiring that all parking be underground, or wrapped in active uses.

8. Protect the unique character and historic resources of the Benjamin Franklin Parkway and Logan Square, through continued application of the longstanding Benjamin Franklin Parkway height controls.
9. Provide the opportunity for limited small-scale cafes and similar uses to enliven the Benjamin Franklin Parkway, while maintaining the long established green and open character of the Parkway.
10. Assure the preservation of historic resources in the Logan Square Neighborhood - Parkway, including preservation of the historic integrity of the Family Court building, in any potential reuse plan.
11. Assure that adequate private and/or public usable open space is provided in all new residential developments.
12. Encourage pedestrian-friendly streetscapes in all new development, through high-quality pedestrian-scale architectural and landscape design elements, and through active uses at street level.
13. Mitigate construction impacts with standards for street closures, street lane and sidewalk closures, noise pollution, and site cleanliness.
14. Establish a neighborhood design review process for all major projects (public, private, etc.), to assure community input into the development approval process.
15. Incorporate sustainable design principles in all developments.

*Map 19. Limit of High Intensity Commercial Development* 

# LIMIT OF HIGH INTENSITY COMMERCIAL DEVELOPMENT









## 4.5 Community Empowerment Goals

1. Ensure that all organizations in the Logan Square neighborhood, especially LSNA and the Parkway Council Foundation, come together to work on issues of mutual concern and to implement the recommendations of the Plan.
2. Provide a consistent review process for infrastructure and development projects that ensures appropriate input from developers, public entities, and neighborhood stakeholders.
3. Establish funding mechanisms for community amenities through a variety of sources.
4. Enhance LSNA's role as an important clearinghouse for information of interest to the community.
5. Encourage all neighborhood stakeholders to become more involved in LSNA, and use it as a resource.
6. Enhance linkages between the Parkway institutions and North Broad Street institutions, including the Pennsylvania Academy of Fine Arts and Pennsylvania Convention Center.
7. Ensure appropriate community and stakeholder input into all actions impacting the neighborhood.
8. Enhance cooperation between LSNA and community organizations representing abutting neighborhoods to address issues of mutual concern.



*Image 49. Philadelphia Family Court Building - a reuse opportunity*



5

## PLAN RECOMMENDATIONS

## 5.1 Quality of Life

| Parkway   | Dilworth Plaza  | Other Open Space   | Homelessness   | Sustainability   | Light/Noise Pollution   |
|---|---|--|--|--|---|
| Additional cafés along Parkway.                         | Rehabilitate plaza.   | More pocket parks (like Coxe Park).                            | Adequately enforce laws and regulations addressing public behavior and encampments.  | Increase tree canopy (500 trees in 3 years).                             | Minimize impacts of outside light sources on interior residential spaces.           |
| Infill openings over Vine Expressway.                   | Reduce barriers to City Hall views.                                       | Open Science Park for neighborhood children.                   | On Parkway, cease outdoor food distributions and install public restrooms.   | Promote green streets.   | Minimize noise impacts of building mechanical equipment.                            |
| Widened bridge crossings over Vine Expressway.          | <b>Streetscape</b>  | Require new development to provide open space                  | Reevaluate all policies and activities that contribute to the disproportionately large chronic street homeless population in the neighborhood. | Encourage alternate energy systems.                                      | <b>Parkway Events</b>   |
| Better landscape on north-south streets across Parkway. | Green Cherry Street between Convention Center and Parkway.                | Provide dog parks.   | Inform community of proposals for homeless/social service facilities.  | Encourage green roofs.   |   |
| New sidewalks and benches.                              | Green Race Street between Convention Center and Schuylkill River.         | <b>Active Open Space</b>                                       | Encourage the hiring of the formerly homeless.   | Encourage rainwater capture (rain barrels).                              | Create committee to evaluate number and type of events.                             |
| <b>Schuylkill River Park</b>                            | Enhance neighborhood gateways (Vine Expressway to 22nd and 23rd Streets). | Picnic grove on trail at Martin Luther King Drive.             | <b>Education</b>   | Discourage private auto ownership/encourage transportation alternatives. | Limit events to signature events (Thanksgiving Parade, July 4th, etc.)              |
| Wayfinding signage from Art Museum.                     | Enhance historic small streets.   | Reprogram existing playing fields/add new fields for soccer.   |  | Community gardens on neighborhood side of CSX tracks.                    | Provide shuttles to off-site parking areas.   |
| Bicycle link between Rivers Edge and Cherry Street.     | Add trash receptacles in commercial areas.                                | Construct tennis courts (under the Expressway at 24th Street?) | Support expansion of charter and private schools.  | Promote recycling/composting.  | Encourage alternatives to driving to events.  |
| Restroom on River trail.                                | Improved transit information at bus stops.                                | Complete Paines Park skateboard venue.                         | Support expansion of higher education facilities.  | Require trash recycling for multi-family developments.                   | Extend parking time limits in residential areas to weekends/holidays during events. |
| Bridge over rail tracks to trail from 24th Street.      | Improve JFK/SEPTA track underpasses.                                      | Community use of Roman Catholic High playing fields.           |  | Require car share and bicycle facilities.                                |   |
| Public dock between Arch and Vine.                      | Pedestrian lighting throughout neighborhood.                              | Community use of rooftop tennis courts at Community College.   |  | Promote LEED for new developments.                                       |   |
|   | Green parking garages with vertical gardens.                              |  |  |  |   |



### 5.1.1 Passive Open Space and Streetscape

#### *Logan Square-Parkway*

- Locate a limited number of cafes similar to Cafe Cret at 3 Parkway (**Image 46**), kiosks and other site-appropriate, small-scale retail (e.g., bookstalls) along the Parkway (for example, in front of Park Towne Place, in front of the Library, and in front of the play equipment at Von Colln Field), as well as public restrooms.
- Infill openings over the Vine Street Expressway at the Free Library and Family Court Building and north of Franklin Institute to create passive landscaped open spaces, integrated with the green spaces of the Parkway and Logan Square.
- Enhance proposed replacement north-south bridge spans over the Vine Expressway with widened bridge structures and extensive landscaping, to create more pedestrian-friendly crossings.
- Install pedestrian-scale landscaping on adjacent north-south streets within the Parkway as a

welcoming tool and to link areas north and south of the Parkway.

- Replace sidewalks and benches along the Parkway from Logan Square to the Art Museum.

#### *Schuylkill River Park*

- Provide more way-finding signage for pedestrians and bicyclists connecting the Art Museum with the Schuylkill River Trail.
- Create a pedestrian/bicycle link on the city-owned parcel on the east side of the CSX tracks between CSX tracks and Rivers Edge Condominium, to connect the Schuylkill River Trail to Cherry Street via the Race Street crossing. This will allow westbound cyclists legal access to the trail. This link should be extended to Arch Street, with development of the sites at 23rd and Arch Streets.
- Locate a water fountain and public restroom

between Vine Street and Locust Street along the Schuylkill River Trail to support trail users in the area.

- Build a bridge across the CSX tracks at the Schuylkill River Park on piers already owned by the City to make a connection at 24th Street (*Image 50a*).
- Build a public dock for small craft (e.g. kayaks and canoes) with a secure storage facility along the Schuylkill River within the Logan Square-Parkway area.
- Investigate feasibility of installing swimming and recreation barges at Race Street.

#### *Dilworth Plaza*

- Rehabilitate Dilworth Plaza to make it more user-friendly and enhance pedestrian connections to the subway concourse.
- Reduce barriers to views of and from City Hall, including removal of dense tree cover.
- Rehabilitate and enhance landscaped areas with a mix of attractive low seasonal plantings and planters for additional seasonal interest.



▲ *Image 50a. Location of proposed pedestrian bridge over CSK tracks at 24th Street*

▼ *Image 50b. Cafe Cret at 3 Parkway*



## Streetscape

- Streetscape and “green” Cherry Street from Broad to 16th Street with street trees and planters to provide a better connection from PAFA and the Convention Center Expansion to the Parkway.
- Streetscape and “green” Race Street from Broad Street to the Schuylkill River Trail to provide a green link between the Convention Center Expansion to the Schuylkill River and Logan Square.
- Enhance the gateways to the community with landscaping and lighting, such as the exits from I-676 on 23rd Street and 22nd Street from Park Towne Place to Race Street.
- Enhance and extend the “small streets” of Logan Square as an interconnected pedestrian walkway system with street trees, pedestrian-scaled street lights, appropriate paving, and improved sidewalks.
- Locate trash and recycling receptacles throughout commercial areas, along the

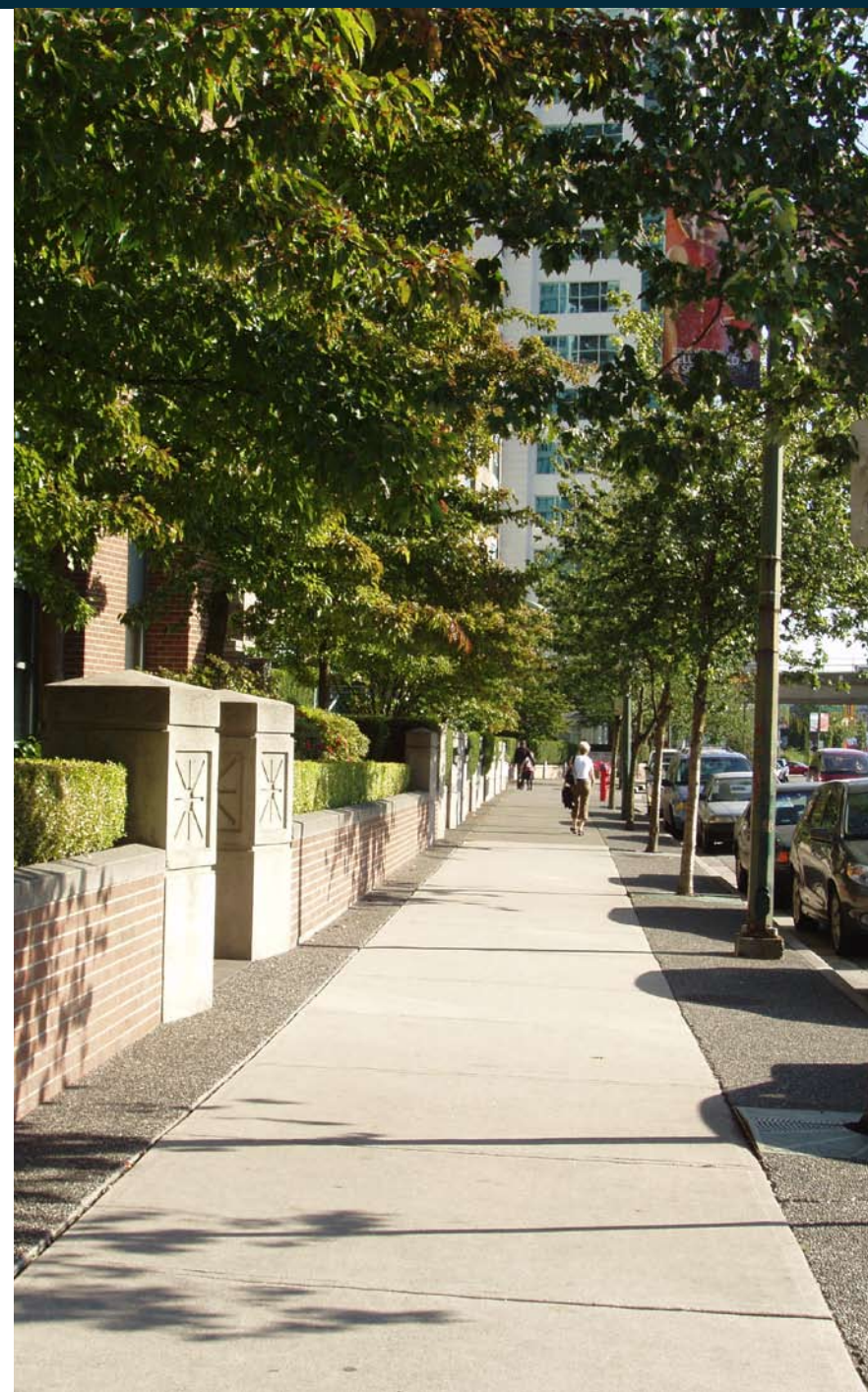
Parkway, in public parks, and at bus shelters. Trash cans in residential areas are not appropriate, except for small pet waste disposal systems.

- Locate transit shelters with flip-down seating, graffiti resistant surfaces, real-time information, and city wide transit maps throughout the neighborhood.
- Repair leaking underpasses and provide better lighting at 23rd, 22nd, and 21st Streets between Market and Arch Street. Provide pedestrian connections between JFK Boulevard and numbered streets in conjunction with development of adjacent parcels.
- Install more pedestrian-scaled street lighting throughout Logan Square to improve safety and livability.
- Encourage the installation of “vertical gardens” as a means of beautifying and “greening” parking garages that lack active first-floor uses or any screening of the parking decks and serving as an alternative to murals.

*Images 51a & b. Well-streetscaped corridor in the study area (left) and; and example of high quality streetscape (right)*









## Other Open Space

- Create small pocket parks, similar to Coxe Park, with play equipment for children ages 5 -12.
- Work with the Franklin Institute to open the Science Park for neighborhood children.
- Require new development to provide passive private and public open space such as plazas, enhanced streetscape, balconies, and courtyards. Establish long-term maintenance agreements between developers and the neighborhood in order to provide the ongoing care these parks require.
- Improve the deteriorated and derelict area under the Vine Expressway Bridge in the vicinity of 24th Street and the CSX tracks to connect open space areas north and south of the elevated expressway.
- Identify locations for neighborhood dog parks, possibly including one in the vicinity of 24th Street at Vine Street.

## 5.1.2 Active Open Space

- Create a picnic grove in the area of Martin Luther King Drive and the Schuylkill River Trail for grilling and gathering after race events in coordination with the location of Paine's Park.
- Reprogram existing fields in the neighborhood (without significantly detracting from public use) and/or add soccer fields to accommodate neighborhood schools' need for soccer fields.
- Determine locations to incorporate tennis courts into the neighborhood, possibly under the Vine Expressway in the vicinity of 24th Street.
- Support the creation of Paine's Park as a skateboard venue.
- Work with Roman Catholic High School and the City to establish community use of Fairmount Park athletic field, currently leased to the school, west of 24th Street, including tennis courts and other facilities.
- Work with Community College to establish community use of rooftop tennis courts.

*Image 52. Area under Vine Street Expressway offers an opportunity for recreational development* 







### 5.1.3 Homelessness

- Increase public awareness of and support for effective, “best practices” responses to chronic homelessness (e.g., the “Housing First” approach) and panhandling by conducting public forums with professional homeless services providers and other public education.
- Remove all homeless encampments throughout the Logan Square Neighborhood - Parkway, including but not limited to those located at Vine Street and 16th Street, along the Parkway (especially in front of the future Barnes site), and beneath the underpasses of JFK Boulevard.
- Cease food distribution to the chronic street homeless in outdoor public spaces through:
  - (a) continued discussions with religious and other volunteer groups distributing food to build their awareness of best practices for addressing chronic homelessness and the importance of linking food recipients with other needed services in dignified, indoor settings;
  - (b) continued efforts to pair food distributors with Center City churches and other indoor sites; and
  - (c) regulatory action to protect public health and ensure appropriate siting of distributions.
- Install site-appropriate public restroom facilities at key locations along the Parkway, provided there are funds for their continual maintenance.
- Enforce all existing laws and park regulations in the Logan Square Neighborhood - Parkway, especially along the Parkway and in all parks and plazas.
- Encourage businesses, institutions, and government agencies located in the Logan Square Neighborhood - Parkway to employ suitable individuals who are homeless or formerly homeless.
- Respond to proposals to locate homeless or other social services facilities within the Logan Square Neighborhood - Parkway by notifying the surrounding community at the earliest opportunity and facilitating appropriate dialogue.

- Critically reexamine all policies, activities, and laws/regulations (or enforcement thereof) that contribute to the disproportionately large chronic street homeless population in the Logan Square Neighborhood - Parkway, recognizing its critical role as a principal destination for local, national, and international visitors.

#### 5.1.4 Education

- Support the expansion of public, charter, and private schools in the neighborhood to provide an asset for residential development and encourage an age-diverse population. Sites that could be considered for expanded educational facilities include the parcels on the south and north sides of the 1900 block of Arch Street, currently occupied by surface parking.
- While respecting the existing residential fabric, support the expansion of Community College, PAFA (*Image 53*), Hahnemann Hospital, Moore College, Drexel University, Temple University, and Strayer University as providers of higher education in the neighborhood, consistent with the goals of the adjacent residential community.

*Image 53. Philadelphia Academy of Fine Arts* ▶





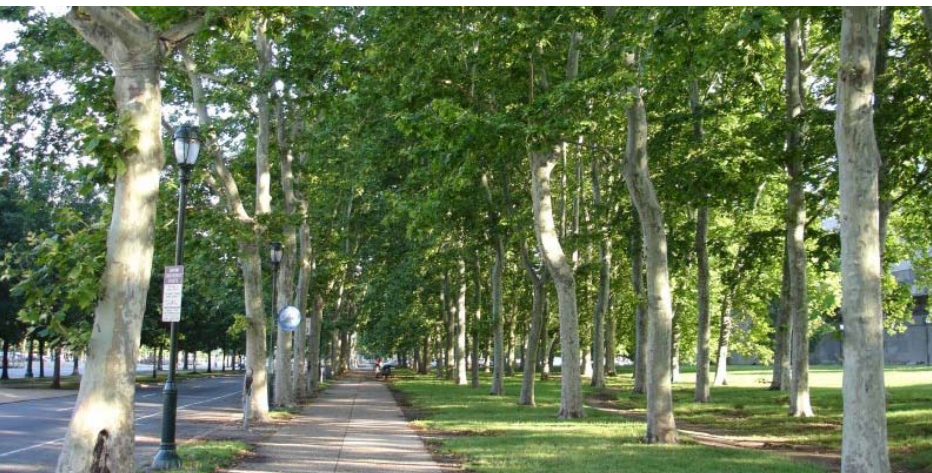
### 5.1.5 Sustainability

“Sustainability” is defined as meeting the needs of the present without compromising the ability of future generations to meet their own needs. It involves the integration of environmental, social, human, and economic goals in policies and activities. Within the context of sustainability “sustainable design” is defined as design that seeks to avoid depletion of energy and water and raw material resources; prevent environmental degradation caused by facilities and infrastructure development over their life cycle; and create environments that are livable, comfortable and safe, and promote productivity.

Neighborhood sustainability initiatives should include measures that enhance the quality of life in the neighborhood, including the following:

- Increase the tree canopy in the neighborhood by planting 500 new trees over the next three years.
- Promote “green streets” where current sidewalks are replaced with porous surfaces, such as the historic brick paving found on several blocks of Logan Square; plantings are added in space that does not impede pedestrians; and traffic or parking lanes are replaced with bus or bike lanes.
- Encourage new developments and owners of existing buildings to install solar and alternative energy systems to reduce power demands from existing utilities.
- Encourage “green” roofs, particularly accessible green roofs that would provide an open space recreational resource as well as a visual amenity.

▼ *Images 54-57. Neighborhood Sustainability Initiatives, left to right: tree canopy, “green” roofs, community gardens, car share*





- Support rainwater capture techniques (rain barrels) for new and existing construction townhouse projects and promote the use of rainwater for watering plants and street cleaning.
- Discourage private auto ownership and encourage transportation alternatives including car share, bicycling, walking, and enhanced transit.
- Establish community gardens along the neighborhood side of the CSX tracks at the river.
- Aggressively promote trash reduction through recycling and composting of organic material.
- Require trash recycling for multi-family developments.
- Require car share and bicycle facilities in total

required parking provision of new developments (1 per 50 units)

- Promote LEED certification or equivalent for major new developments
- Encourage the installation of “vertical gardens” to “green” and beautify parking garages and blank walls.



### 5.1.6 Parkway Events

The concentration and collection of museums and institutions along the Parkway’s “Museum Mile” makes this a unique area in the country. The area is a major draw for residents, as well as tourists and visitors. The City should promote this area and connect these institutions in a meaningful way to capitalize on the cultural and educational offerings located in Logan Square.

With that in mind, the Parkway has also come to be known as an event location, with many events each year causing traffic and congestion for institutions and local residents and resulting in trash and noise. Institutions along the Parkway have reported reductions in visitorship during these major events, despite a marketing campaign designed to let visitors and residents know about ongoing events and encouraging them to “Make it a Day on the Parkway.”

- Parkway institutions should be in communication regarding upcoming events and potential connections that can be made between them.
- Create a committee comprised of various stakeholders (institutions, residents, and businesses) to meet regularly with City officials

to evaluate the number and type of events that will utilize the Parkway and to plan the street closures and other logistics necessary.

- Major events that currently occur on the Parkway should be reviewed to determine if they would be better served in other areas of the city.

Currently, there is little overlap of function between events and the institutions on the Parkway, as those participating in charity runs do not tend to support the museums during the same visit.

- The Parkway should not be used as a fairground, but rather host events that include signature events (such as the Thanksgiving Day Parade, Welcome America festivities, July 4th celebrations, and the June bicycle race) and Parkway-oriented events like the Parkway Fun Day, which are institution-focused, family-oriented events that highlight the beauty of the Parkway and provide opportunities for the institutions and neighborhood to relate to the larger community. Institutions should similarly see these events as opportunities to increase their visibility to a larger audience.

## EVENTS IN OR NEAR THE LOGAN SQUARE NEIGHBORHOOD, 2008

- January  
Mummers Parade — Broad Street
- March  
Saint Patrick's Day Parade — The Parkway
- April  
Turkish Festival and Parade — The Parkway  
Walk America — The Parkway  
Armenian Genocide Walk — The Parkway  
Kick Butts Day — JFK Plaza
- May  
Broad Street Run  
Dad Vail Regatta — Kelly Drive  
Susan G. Komen Race for the Cure — Fairmount Park  
World Lupus Day — JFK Plaza  
Jewish Martyrs Memorial  
Israel Independence Day
- June  
Wachovia US Pro Cycling Championship — The Parkway
- June–July  
Welcome America Festival — The Parkway
- August  
Unity Day — The Parkway  
Recreation Department Summer Olympics — JFK Plaza
- September  
Philadelphia Distance Run — The Parkway  
General von Steuben Parade — The Parkway  
Puerto Rican Festival Parade — The Parkway  
Ratha–Yatra Parade/Festival  
Heal Our Lord Parade/Festival  
CollegeFest
- October  
Campus Philly Kick Off — The Parkway  
Pulaski Day Parade — The Parkway  
AIDS Walk  
Step Out Diabetes
- November  
Philadelphia Marathon — The Parkway  
Thanksgiving Day Parade — The Parkway
- December  
Tree Lighting Festival and Parade — City Hall / Broad Street

- A single contact at the City level should be in place to coordinate and serve as a liaison for all aspects of events on the Parkway. This individual should be in contact with the LSNA and the Parkway Council to inform them of upcoming events and related issues.
- Trash should be cleaned up promptly after events. Parties sponsoring events on the Parkway should be required to contract for cleaning services. Fines should be assessed if trash is not picked up within 24 hours of the event.

Traffic issues resulting from these events can be mitigated in several ways.

- Traffic behavior should be closely monitored before, during, and after Parkway events with a highly visible enforcement presence.
- Alternatives to driving should be advertised in print and broadcast media as well as aggressively encouraged on the web sites of event sponsors, LSNA, Parkway Council, SEPTA, and Parkway institutions.
- Transit services should only be detoured as a last resort. When transit is rerouted it should be the



burden of the event coordinator to ensure that proper rerouting information is placed at each affected transit stop. Detours of transit services should be consistent for all events, allowing residents of the area to maintain access to their homes and destinations. Detours should be prominently advertised in advance, including on those websites listed above.

- Shuttles to offsite parking areas should be implemented as another alternative for driving to Parkway events. These have been used effectively at other sites, including Eastern State Penitentiary.
- During Parkway events and on holidays in residential areas, standard weekday parking time limits should be enforced in residential areas, so that on-street spaces “turn over” regularly and are available for residents with parking permits.
- When appropriate, utilize the “No drive zone” policy similar to that used during Live 8.

*Image 58. Event on the Parkway* ►



### 5.1.7 Light and Noise Pollution

- Sources of light, including street lights, parking lot lights, and illuminated works of public art, should be designed to minimize adverse light impacts on interior residential spaces and the public environment. Direct light sources should be screened and “cut off” fixtures used where feasible.
- Noise generated by exterior building mechanical equipment can have an adverse impact on quality of life in a dense residential neighborhood. Equipment such as air conditioning condensers vary in the amount of noise they generate. Low decibel rated equipment should be required. Condensers should not be located in side or rear yards where they can adversely affect neighbors’ enjoyment of their yards; instead they should be roof-mounted. Large roof-mounted mechanical equipment should be screened with enclosures to reduce noise impacts. Exhaust fans should extend to top roofs not through building walls, to minimize noise impacts.

### 5.1.8 Community Organization

- Establish a seat on the Parkway Council Foundation for an LSNA Board member.
- Regularly provide minutes of the LSNA board meetings to the Parkway Council Foundation.
- Seek adjacent neighborhood organization’s support for relevant projects undertaken by LSNA.
- Participate in building a coalition of support for large projects or initiatives to improve Center City.
- Consider sharing other neighborhood group staff time to support grantwriting and public relations efforts.
- Seek grants and other funding for community amenities.
- Assure that the community is fully informed about city regulations and guidelines that impact the physical environment of the neighborhood.
- Provide a consistent review process for infrastructure and development projects that includes developers, public entities, and residents.



## 5.2 TRANSPORTATION

### 5.2.1 Circulation

#### *Capacity Adjustment*

The Logan Square Neighborhood - Parkway has matured beyond the point where traffic bottlenecks can be permanently solved by common traffic engineering “quick fixes” such as road widening and additional turn lanes. This is due to space constraints that preclude such actions and the ephemeral benefits of such fixes in light of continuing development.

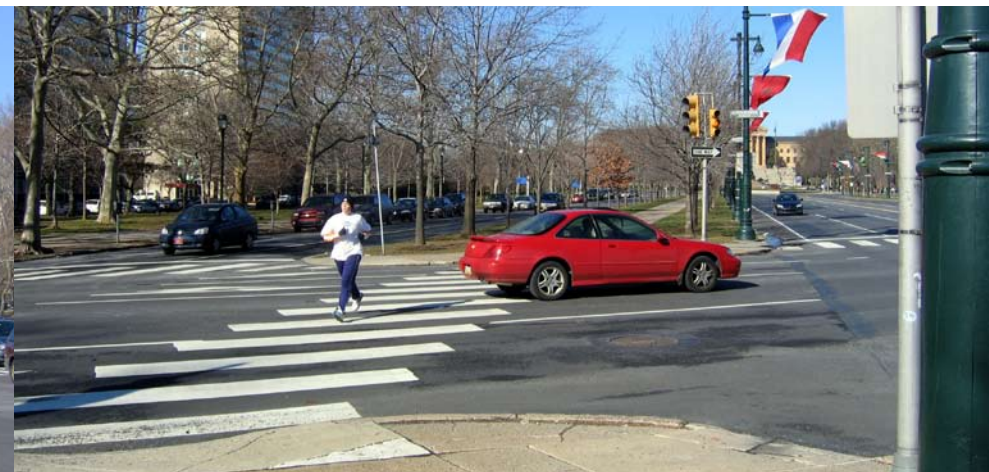
Attempts to “force” conventional traffic-engineering solutions run a strong risk of doing irreparable

harm to the walkability that is a core asset of the neighborhood and that is targeted along with the bicycle network for further strengthening through this neighborhood plan.

A number of potential engineering-based measures could help alleviate several trouble-spots identified in the course of this study without damaging the underlying urban structure. While the “built-out” nature of the neighborhood makes substantial traffic-capacity enhancement impossible, there are several locations where adjustments to signal timing or intersection geometries could potentially alleviate specific traffic bottlenecks:

- Signal timing adjustments at 22nd/Parkway, 16th/Spring Garden, and Broad/Callowhill

▼ *Images 59-62. Traffic enhancement locations, left to right: 20th/Parkway, 22nd/Parkway, Eakins Oval, 18th St/Callowhill*





(acknowledging potential for two-way traffic operation on the latter—see below).

- Rationalization/simplification of intersections at 20th/Parkway, 15th/Vine, and Broad/Vine.
- Conversion of Callowhill Street between Franklinton Boulevard and Broad Street or 15th Street to two-way traffic to support proposed redevelopment goals, pending the results of DRPA's Center City Access Study (which may affect Callowhill Street's relationship with the Vine Street on/off-ramps).
- Introduction of a traffic signal on 22nd Street at the I-676 on/off-ramps, to better meter traffic flows and to accommodate safer pedestrian crossings.

- Major overhaul of traffic/pedestrian patterns at Eakins Oval.
- Reestablish the 18th Street corridor north of Callowhill Street and eliminate the "jog" at Callowhill Street. This would require the elimination of Franklin Town Boulevard north of Callowhill Street as a through street, reconstruction of the section of 18th Street between Callowhill Street and the former rail right-of-way, and conversion of the former street bed in front of the Community College building at the corner of Callowhill Street to a pedestrian plaza.

Furthermore, the potential conversion of Market Street and JFK Boulevard to two-way traffic should be seriously considered through further study. The



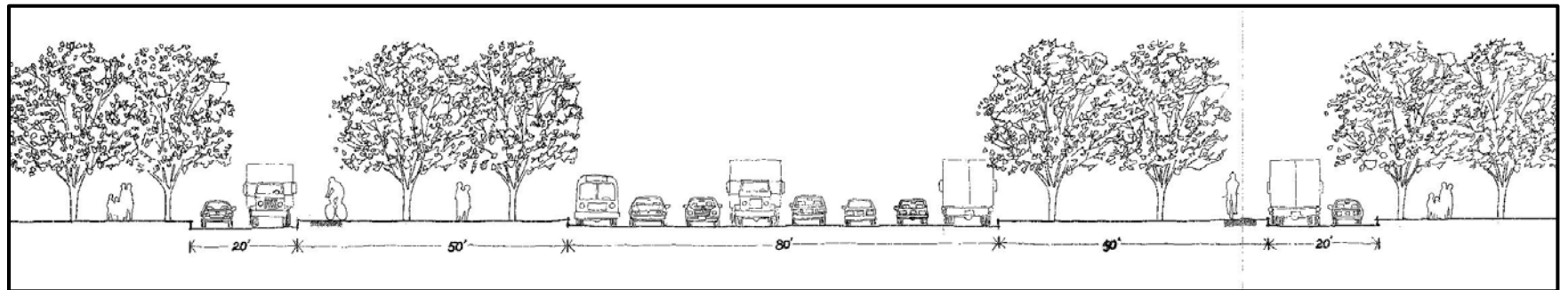
primary benefits of such an action would be the simplification of circulation patterns, traffic calming, the provision of opportunities for a better retail environment with increased visibility for purveyors, and the potential for attractive “boulevard-type” amenities such as a planted median. The primary challenges to this notion are complication of intersection movements and signal timings, prohibition of left turns, rerouting of buses, uncertain impacts on traffic flows and altered access to several existing parking structures which could require entry/exit modifications. The Center City District conducted a very preliminary study of this idea in 2005, yet it would require a more formalized traffic/access study before it could be acted upon.

A major defining element of the Logan Square neighborhood is the Benjamin Franklin Parkway. It is essential that any redesign effort for this corridor reflect the key neighborhood transportation principles of improved pedestrian safety, better-managed traffic circulation, and controlled speeds. The most recent planning studies for redesigning the Parkway—by Central Philadelphia Development Corporation (CPDC) in 2003 and the City Planning Commission (PCPC)/Commerce Department in 2007—have adhered to these principles in their proposals for a modified cross-section, illustrated in

**Figure 4.**

The most notable elements of these proposals, currently undergoing further evaluation by the Fairmount Park Commission, are:

- reduction of “outer” Parkway sections to function as two traffic lanes during the peak periods and one traffic lane plus parking lane in the non-peak periods, leading to a calmer, slower-speed traffic environment that is easier to cross;
- relocation of the bicycle lanes to the rebuilt, traffic calmed, outer sections; and
- widened/lengthened center median islands for better pedestrian refuges.



▲ *Figure 4. Proposed Cross-Sections of Eakins Oval: PCPC 2007 (top) and CPDC 2003 (bottom)*



The Parkway is framed by two complicated junctions at Eakins Oval and 20th Street. Design solutions for these areas—in early phases of evaluation—include a number of alternate concepts.

### *Site Analysis: Eakins Oval*

Eakins Oval represents a complex circulation challenge. In its current condition, it “fails” by any definition for all modes of travel: automotive, pedestrian, and bicycle. The main facets of the problem for each of these elements are:

- **Automotive Traffic:** Heavy flows and failing levels-of-service at various locations in the morning and afternoon peak periods; and uncomfortable, uncontrolled “merging” and “weaving” movements required for most trips.
- **Pedestrian Traffic:** Pedestrian crossings are missing at several key desire-lines.
- **Bicycle Traffic:** Uncomfortable shifting and merging across unpredictable traffic lanes.

Over the years, Eakins Oval has been studied in several contexts with two concepts having emerged that would calm traffic by tightening up intersections and that would add crosswalks at key locations, including signalized pedestrian crossings to the Art Museum steps. These concepts, each of which requires further study, are illustrated in **Figure 5** and described below.

*Eakins Triangle (Origin: Philadelphia City Planning Commission Benjamin Franklin Parkway: Circulation, Parking, and Transit Study, 2007)*

In the Eakins Triangle concept, traffic circulation would function similar to the way it does now, but in a smaller space and at a slower pace. The roadway that currently travels between the Art Museum and Eakins Oval would be relocated to the east side of the Washington monument and fountains. This plan creates a continuous public open space between Art Museum steps and the Washington Monument, and rationalizes and tightens several intersections. However, the concept requires detailed intersection-by-intersection traffic study or modeling.

**Figure 5. Previous Eakins Oval Site Analysis: PCPC 2007(left) and CPDC 2004 (right)** 

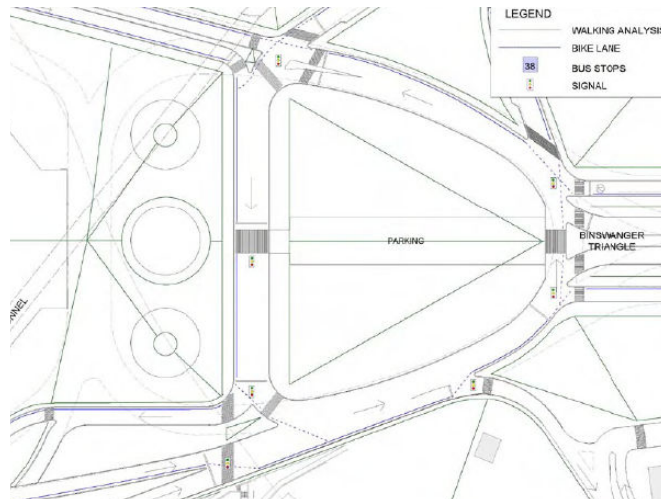
*Greber Oval (Origination: Philadelphia Museum of Art Concept Study Restoring Greber's Plan for Eakins Oval, 2000; Central Philadelphia Development Corporation Public Improvements and New Urban Design Framework for the Benjamin Franklin Parkway, 2004)*

The Greber Oval concept attempts to re-create the original 1917 plan for the Parkway, retrofitted for modern conditions. The concept de-concentrates traffic by re-rerouting cars to/from Martin Luther King Drive along the restored Crescent roadway. In addition, it urbanizes” the Oval and its intersections through tighter design and right-angle street approaches. However, connection of the Crescent with Martin Luther King Drive requires major reconstruction, and it conflicts with existing and planned recreational amenities along the river.

In addition, overall vehicular service levels are not significantly improved, as several congested movements continue to encounter bottlenecks (including a difficult left-turn movement from the Parkway outbound to the Crescent/Martin Luther King Drive).

While the redesign of Eakins Oval represents a major effort requiring a dedicated traffic study, it is important that the Logan Square neighborhood be involved in future planning efforts to ensure that the quality-of-life principles documented in this report are reflected in the eventual Eakins Oval solution.

Any redesign of Eakins Oval should include elimination of the unsightly surface parking, and focus on the principle goal of improving the safety and number of pedestrian crossings.



### *Site Analysis: 20th Street/Parkway Intersection*

As with Eakins Oval, two distinct concepts have emerged from two separate efforts (again managed by the Philadelphia City Planning Commission and the Center City District) for improvement to the intersection of the Parkway and 20th Street (*see Figure 6*). These have resulted in very similar conclusions about appropriate design solutions to achieve the goal of pedestrian safety.

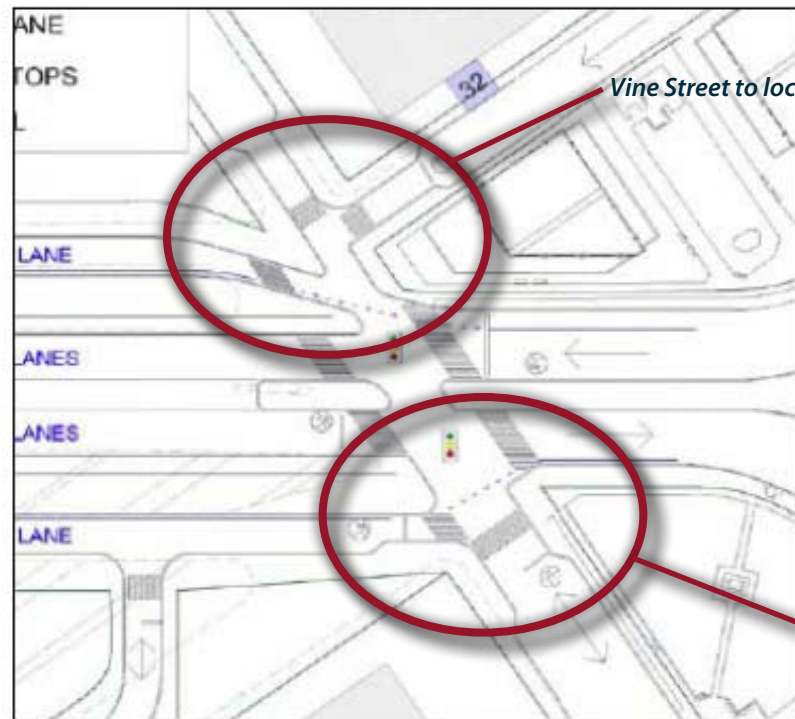
The main difference is that the PCPC concept is more aggressive in its treatment of traffic restrictions at westbound Vine Street and the eastbound Parkway (local lanes) as they approach the intersection, limiting these movements to right-turn only to achieve a simpler signal operation and more crossing time for pedestrians.

It is recommended that the Logan Square Plan support the implementation of one of these concepts as they are studied further and reviewed with the City Streets Department, mindful of the overarching objectives of reducing pedestrian crossing distances and improving visibility. It should be noted that any action here requires coordination with two separate additional efforts:

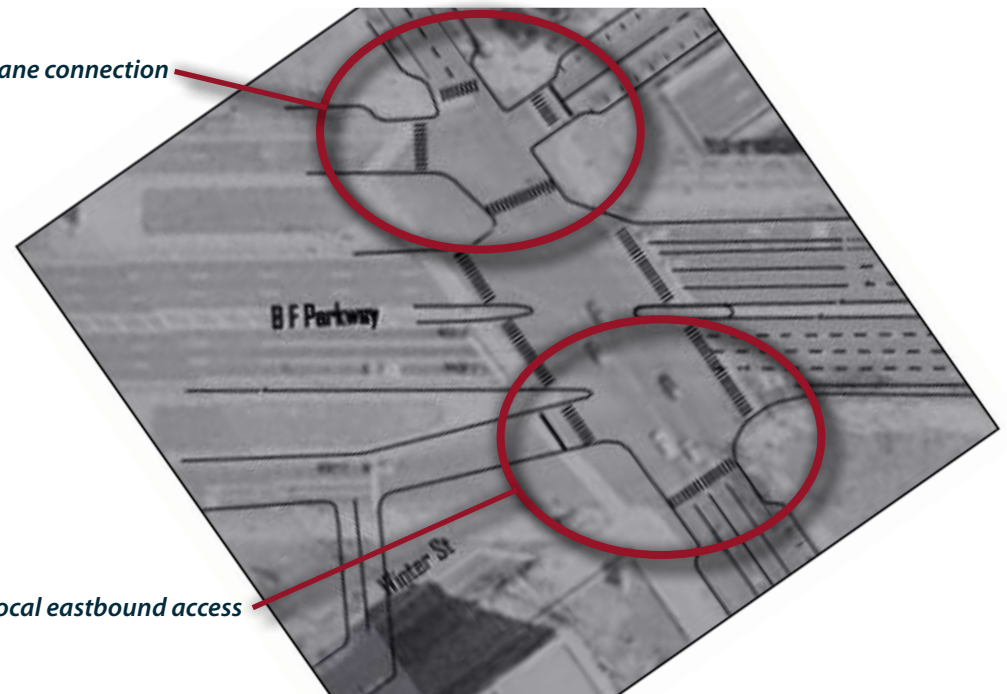
- The proposed Benjamin Franklin Parkway reconstruction which would affect the curb alignments on the west side of the intersection (*see Figure 4 for proposed cross-sections*).
- The programmed reconstruction of the overpasses of the Vine Street Expressway. If a final design for the 20th Street/Parkway intersection were complete prior to bridge replacement at 20th Street, the proposed changes to the intersection could be implemented as the bridge reconstruction is completed and the surface treatments restored, resulting in minimal cost and disruption of traffic for the combined projects.

It is important that LSNA monitor developments on this issue and participate in the redesign process to ensure adherence to the goals and objectives of the neighborhood plan, including enhancement of pedestrian safety and convenience.





*Philadelphia Planning Commission Plan*



*Center City District Plan*

▲ *Figure 6. Intersection Improvement at Pennsylvania Avenue and 20th Street.*

### *Demand Reduction*

An important component to addressing the neighborhood's traffic issues is to strive to make Center City less traffic-dependent in the longer term, i.e. to establish the pedestrian, bicycle, and transit systems as stronger options for discretionary travel. These efforts must be sufficiently aggressive that a discernible percentage of local trips within the neighborhood and to/from other areas of Center City are shifted to these alternate modes, such that street capacity is freed up for trips having no practical modal alternatives.

A further benefit of this strategy is that it would provide residents with viable, safe options to car travel for a variety of shorter-distance trip-types.



▲ *Image 63. Bike lanes in Copenhagen*

*Images 64, 65 & 66 . Current bicycle and pedestrian conditions in the study area.* ►





## 5.2.2 Parking

Potential parking improvement measures include actions that could increase the parking supply, decrease the parking demand, and improve the efficiency of existing parking spaces.

- Support the construction of a new institutional parking facility within the area bounded by 21st Street, Hamilton Street, Vine Street, and 18th Street. An appropriate location would be a multi-level underground parking facility beneath the new Free Library Expansion or under the new Barnes Museum, or as a potential redevelopment scheme for the Whole Foods site, incorporating parking serving mixed uses, including commercial uses on the ground floor.
- Replace underutilized traffic lanes with parallel parking lanes.
- Remove unnecessary parking restrictions on various blocks.

### Potential Parking Spaces

- East side of the 100 block of N 22nd Street just below Race Street
- East side of the 200 block of N 22nd Street between Race and Winter streets
- North side of the 2100 block of Race Street just East of 22nd Street
- South side of the 1600 block of Arch Street
- North side of the 1600 block of Arch Street
- West side of 19th Street between Logan Circle/Race Street and Cherry Street

- Maximize the number of viable parking spaces (through re-striping and re-signing) on stretches of road where some parking is restricted.
- Incorporate car share into developing parcels as well as into areas currently not optimally served (i.e. gaps between car share pods of greater than three blocks).
- Create safer perceived connections between the northeast quadrant of the neighborhood (at Philadelphia Community College and the adjacent office buildings to the east) and nearby residential areas to encourage evening and weekend use of parking resources in these areas.
- Rehabilitate the deteriorated parking garage under JFK Plaza/Love Park, including installing new ADA-accessible elevators.

Overall, the parking situation can be isolated into four components: residential parking; business

parking; institutional parking; and special-event parking. Strategies for addressing these issues are partly physical and partly related to management. The recommendations described below include specific strategies for each component.

### *Residential Parking*

The following are recommendations with respect to residential parking.

- Request that the City Streets Department review the site-specific on-street parking opportunities identified in **Figure 2-1**. (Several of the noted areas of parking restrictions are due to current or recent construction; attention should be focused on these to ensure they revert to the allowance of parking as soon as possible after construction is completed.)
- Maintain the existing parking-space-per-unit ratio (0.7) for new development with a maximum of one per unit (0.5 south of Vine and east of 20th streets).

- Encourage the use of car sharing through:
  - ◊ An “overlay” requirement of 1 car share space (any operator) per 50 units.
  - ◊ An allowance of a reduction of 4 general-use spaces for every car share space provided, within reasonable limitations and in coordination with other such City programs.

The purpose of the car share incentives would be to maximize the number of new residents who do not bring their cars (and associated parking/traffic demands) to the Logan Square neighborhood.

- Extend the provisions of § 14-1631 Off-street Parking Control District to the Logan Square Neighborhood - Parkway. The provision prohibits off-street parking for any one or two-family dwelling, unless accessed by a common driveway easement that serves three or more abutting properties. It thus prohibits garage-front townhouses, which eliminate an on-

street public parking resource. The provision currently applies to Center City south of Chestnut Street and the Northern Liberties area.

- Make available temporary visitor parking permits under the residential permit parking program.

Also, several concepts exist for a potential reconfiguration of Pennsylvania Avenue, intended to boost on-street parking by altering the alignment of traffic lanes and parking bays. Although the changes would be primarily outside the Logan Square neighborhood, they would extend as far east as 22nd Street and could provide an opportunity for the redesign of the 22nd and 23rd Street intersections.



### *Business Parking*

- Business parking should continue to operate in its current manner, with garages in the Central Business District charging graduated rates for daily and hourly parking. However, the overall pricing structure should be reconsidered to reflect the recommendations of the Planning Commission's 2005 Center City Parking Policy Evaluation.
- In terms of neighborhood retail parking, it is recommended that new parking associated with the proposed business corridor on Callowhill Street be fitted with meters with a 2-hour time limit, effective until 6 PM.
- Adequate provision is required for business support service parking, including FedEx and UPS to minimize double parking problems.
- Though often tempting as a quick-fix solution, it is strongly recommended that creation of “temporary” surface parking lots through demolition of underutilized buildings not

be permitted. Such action strongly detracts from the consistency of the neighborhood's pedestrian-oriented character and could therefore lead to greater parking dependency in the longer term. Any durable solution to the neighborhood's parking challenges must be comprehensive and multi-modal in nature to ensure lasting effectiveness.



▲ *Image 67. Metered parking.*

### *Institutional Parking*

- The Logan Square Neighborhood - Parkway Plan supports the main parking recommendations of the City Planning Commission's 2007 report Benjamin Franklin Parkway: Circulation, Parking, and Transit, including the construction of an underground parking facility beneath the Free Library expansion or the new Barnes Museum.
- Eakins Oval should no longer be used for unsightly surface parking, as the new institutional parking facilities should compensate for the current above-ground parking, which would enable this central public space and critical area in the Parkway viewshed to be used for ceremonial and recreational purposes.
- The proposed parking lanes along the Parkway should be for non-peak periods only and should largely serve institutional uses, consistent with the Philadelphia City Planning Commission's plan for reconstruction of the outer lanes to provide a safer environment for pedestrians and cyclists.
- Signage to parking facilities for visitors should

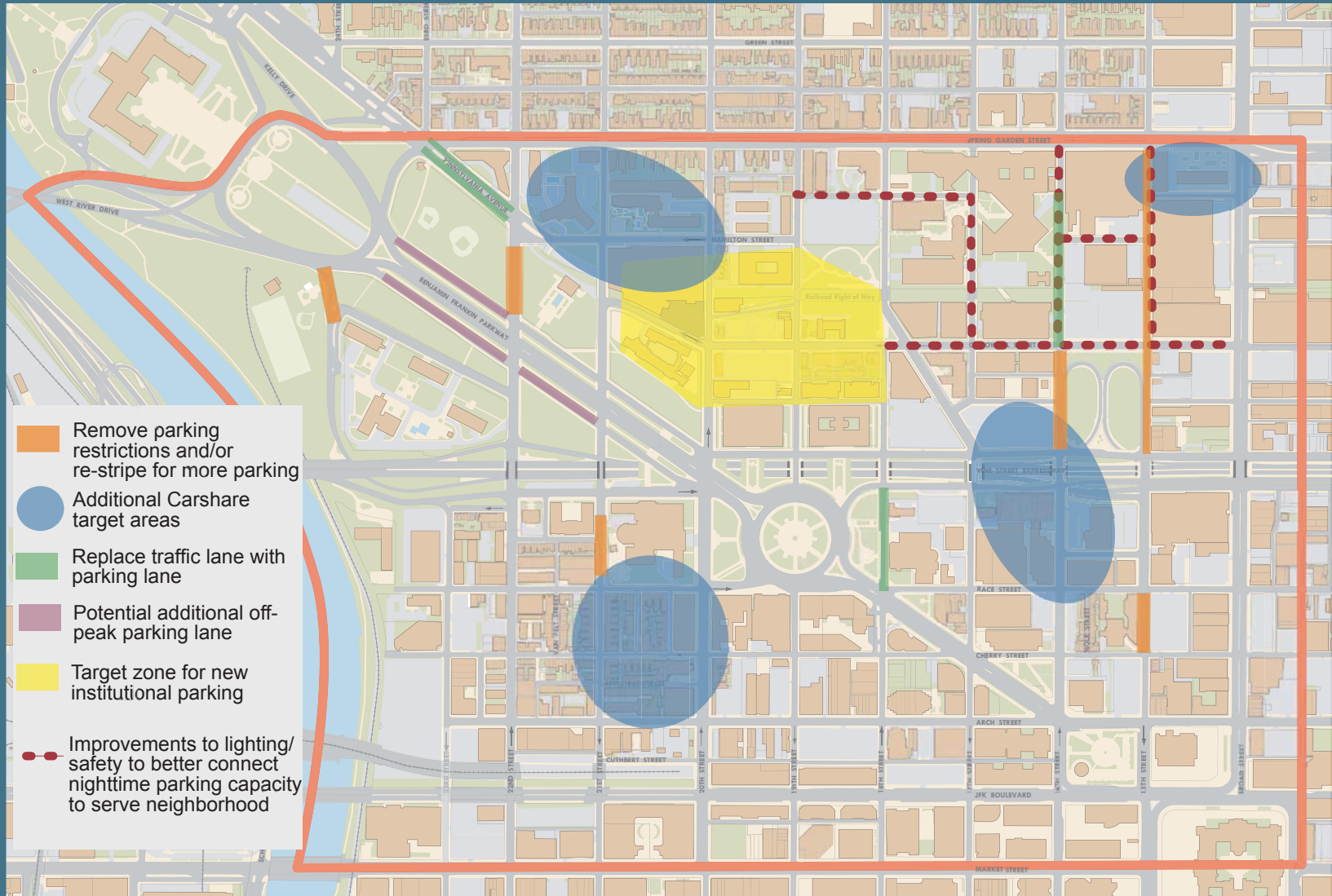
be improved with a coordinated program.

### *Special-Event Parking*

- Philadelphia Community College has the largest available parking supply likely to be used on special-event days. Coordination, management, and improved wayfinding to the college's facilities could help alleviate the pressure on the on-street parking supply.
- The applicability of permit parking restrictions should extend to all days, including Sundays and holidays. Moreover, the "blanket" parking restrictions often implemented on event days on some streets should be revised to allow parking by residents with permits.
- Event-day enforcement procedures should be modified to ensure that local parking-permit holders should be permitted to bypass roadblocks to get to their homes.
- When appropriate, utilize the "No drive zone" policy similar to that used during Live 8.

*Map 20. Proposed Parking Opportunities* ►

# PROPOSED PARKING OPPORTUNITIES





### 5.2.3 Pedestrians

In light of the overall goal of automobile “demand reduction,” one of the primary concerns with the street system should be to ensure that it is safe and comfortable for pedestrians, especially where this can be achieved without significant impact to traffic capacity. **Map 21** identifies a program of pedestrian enhancements that could help to fill the voids in the neighborhood’s strong historic walkable urban fabric.

Pedestrian system enhancements are aimed at balancing the needs of motorized vehicles, in terms of maneuverability and efficient flow, with the critical need for consistent pedestrian safety.

- Rebuild unsafe, traffic-centric intersections to “urbanized” standards.
- Add bulb-outs and tighten the curb radii along the Parkway (specifically at 21st and 22nd Streets) to slow the vehicle turning movements and shorten the pedestrian crossings.
- Add a four-way stop sign at 22nd and Cherry Streets, to slow traffic on 22nd Street.
- Prioritize traffic calming along Callowhill Street to support redevelopment goals; also prioritize traffic calming on streets with three or more traffic lanes and little/no protective pedestrian buffer, including 22nd Street south of Vine Expressway.
- Install a “thin buffer” at the curb where narrow sidewalks are not protected by parallel parking. Bike racks and honor box corrals can serve this purpose well.
- Advocate for a major overhaul of Eakins Oval (although this is not the best place for an underground garage as it would serve only PMA and would not prove to be cost-effective).
- Improve the intersection of 24th Street and Eakins Oval to eliminate the large radius, free flow right turn onto 24th Street. Create an urban intersection with a crosswalk to encourage pedestrian-access across 24th Street to the Schuylkill River Trail from the Parkway.
- Improve the intersection of 23rd Street and Vine Expressway off-ramp to eliminate the free

right turn and provide a crosswalk.

- Eliminate the right angle parking on Hamilton Street at 21st Street in front of the WaWa. (*Image 68*). The current location of the parking at the intersection causes congestion and has eliminated a section of the public sidewalk.

Traffic/Street design should accommodate vehicle movements in a “controlled” manner while establishing a consistent safety standard for pedestrians. Best practices to this end would include the following:

- Minimal curb radius to ensure slow vehicle turning movements and minimal length of pedestrian crossings. This minimal radius should assume that the designated design vehicle could encroach upon adjacent lanes and turn into the entire receiving area.
- Curb radius of zero (or as close as possible given site-specific drainage requirements) on corners that are not subject to turning movements.

- Bulb-outs across parking lanes wherever possible, to slow vehicle turn movements and shorten pedestrian crossings.
- Extension/maximization of all traffic islands where possible, to exert maximal control over traffic/turning speeds and to optimize the pedestrian refuge areas.
- Minimal cartway widths and elimination of excess (“unused”) paved space to better channelize—and improve the predictability of—vehicle movements.
- Crosswalks striped across all corners regardless of pedestrian volumes, partially as a visual traffic calming feature and partially to discourage unexpected pedestrian movements (i.e. jaywalking) that could otherwise take place.

See *Figures 10a, 10b and 10c* for prototypical examples of the application of these principles to several sample intersections. *Map 20* shows the location of these examples in the study area.

Additionally, the following opportunities should be explored with respect to traffic signals:

- Install pedestrian countdown timers on all crossings greater than three uninterrupted traffic lanes (specifically, the Benjamin Franklin Parkway, JFK Boulevard, Market Street, and Broad Street).
- Evaluate the potential for incorporating non-peak-period pedestrian-only signals at crucial locations where geometric considerations make it difficult to control the speeds of turning traffic.

The pedestrian environment on the sidewalk also needs protection from encroachment by physical obstructions, such as vending, street furniture, etc., and incursion by vehicles, in the form of driveways and lay-by lanes. Finally, there should be a strong emphasis on improving the overall level of pedestrian connectivity throughout the area. To this end, the potential for adding ramp and stair connections

to/from JFK Boulevard at 21st, 22nd, and 23rd Streets should be evaluated, as well as additional opportunities for safe street crossings in the Art Museum area.

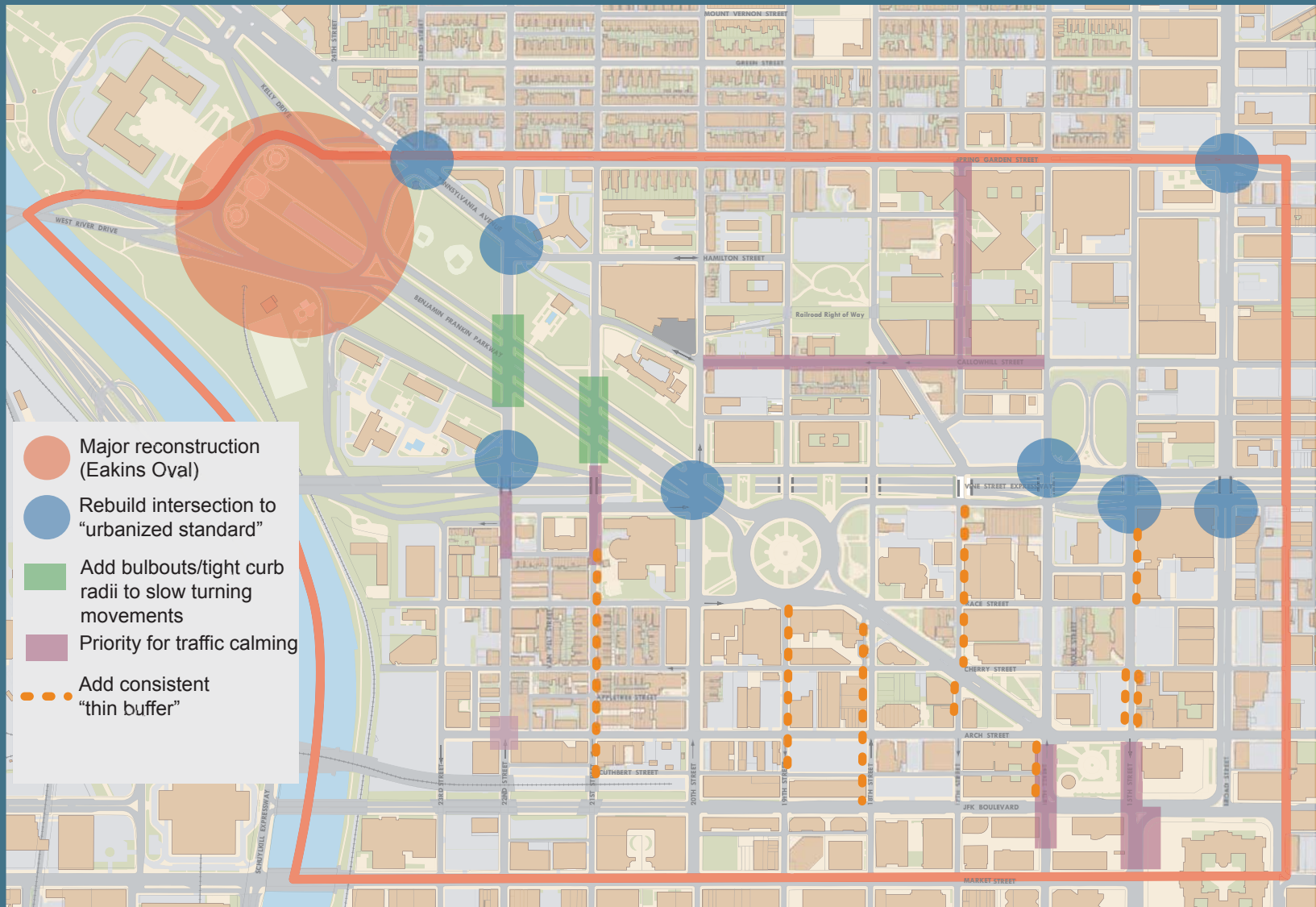


▲ *Image 68. Right Angle Parking at Hamilton and 21st Street impedes pedestrian sidewalk*

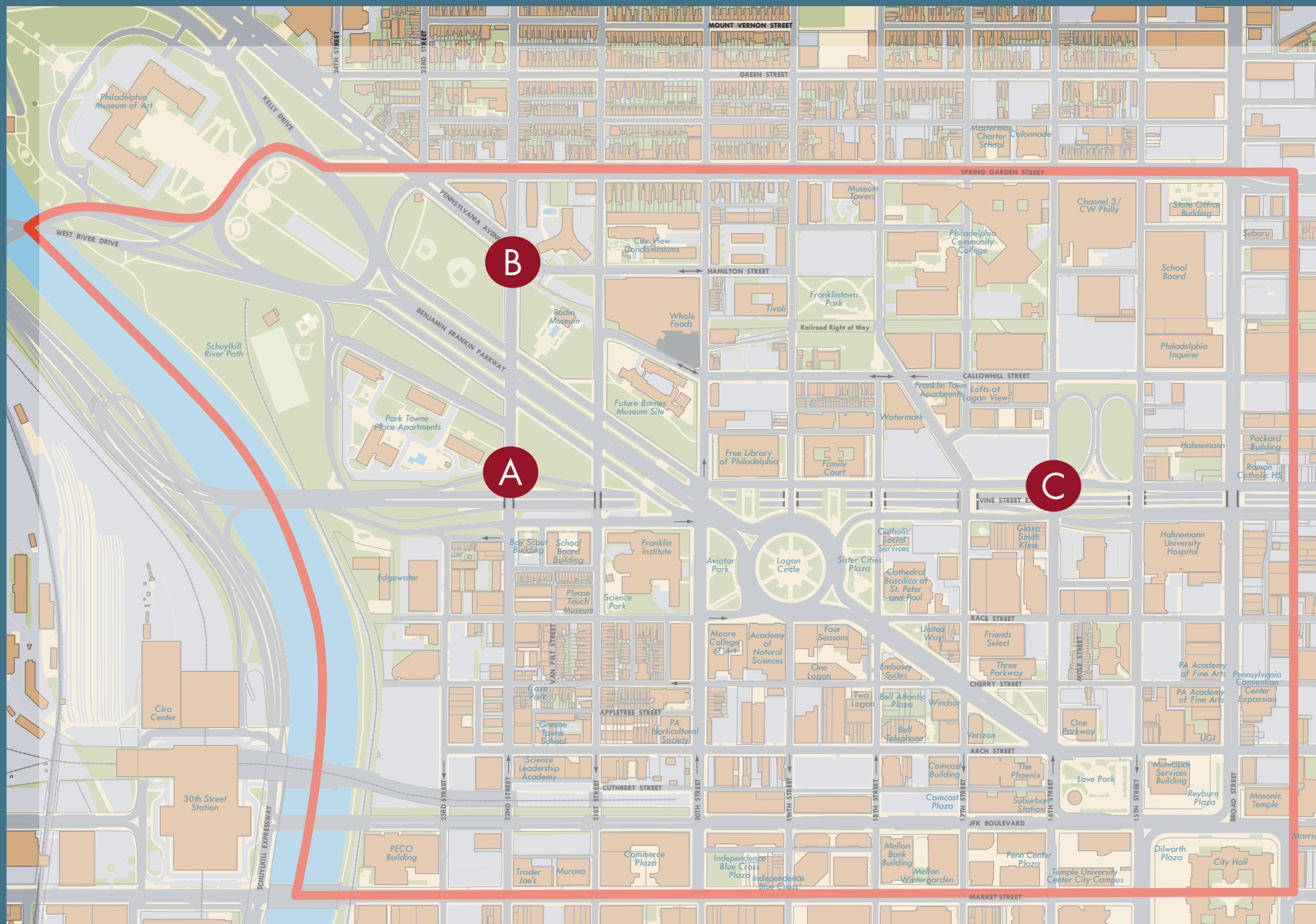
*Map 21. Proposed Pedestrian Enhancements* ►

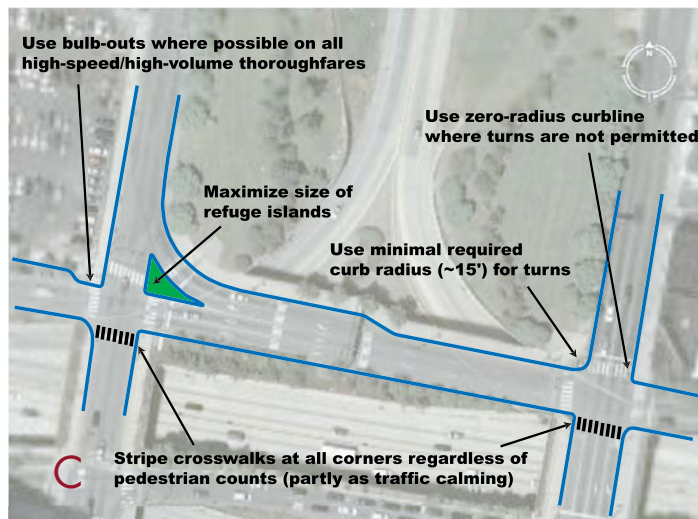
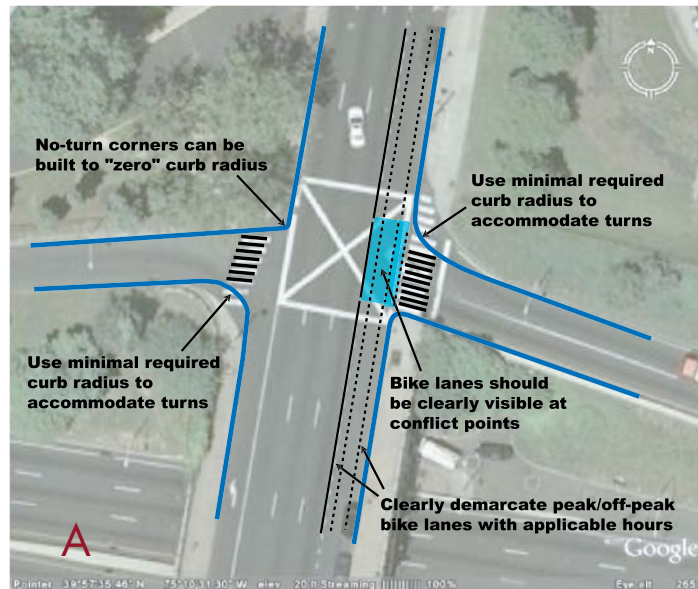


# PROPOSED PEDESTRIAN ENHANCEMENTS



# PEDESTRIAN INTERSECTION IMPROVEMENT LOCATIONS





◀ **Figures 8a, 8b and 8c. Intersection examples for Pedestrian Improvement: Clockwise from left: 22nd at I-676; 22nd at Pennsylvania Avenue; Vine Street at 15th and 16th Streets.**



## 5.2.4 Bicycles

The current bicycle system incorporates bike lanes into the neighborhood, but these have historically been limited to streets where they could be comfortably fit and thus suffers from the several deficiencies, including “gaps” where streets become too tight for bike lanes and awkward transitional geometries, particularly along the Benjamin Franklin Parkway.

The proposed bicycle system enhancements shown in **Map 22** are aimed at filling “gaps” in the existing network, and are focused primarily on streets where such enhancements are achievable given the available roadway width:

- Complete the bicycle lanes on 22nd Street as the primary northbound connector.
- Install bicycle lanes on 20th Street south of the Benjamin Franklin Parkway as the primary southbound connector, with 19th Street providing southbound circulation above the Parkway.

- Establish the Pennsylvania Avenue/Callowhill Street corridor as a contiguous neighborhood-serving east-west alternative to the Parkway, with Callowhill Street as a shared, heavily-traffic-calmed “destination” zone.
- Correct the existing problematic transitions—primarily along the Parkway—by revisiting their geometric alignments and increasing their visibility.
- Relocate Parkway bike lanes to the outer, traffic-calmed sections of the Parkway.
- Consider the installation of off-street bicycle pathway connections along the north/east edges of Love Park and the northern apron of City Hall Plaza, both of which consist of underutilized spaces.
- There is potential width for supporting northbound bicycle lane (feeding the Parkway) on 16th Street by eliminating the existing exclusive right-turn lane to Arch Street.

*Image 69. Insufficient bicycle parking capacity* 





- Add substantial bicycle parking capacity at Callowhill Street (to support the proposed neighborhood-scale retail), the Van Colln ballfields, and the Spring Garden subway station.
- Install combined bicycle parking/rental facilities at Race Street/Schuylkill River Park, Logan Square at the Free Library, and JFK Plaza/Love Park.
- Clearly mark (and maintain) specially colored bicycle lanes at all conflict points, i.e. wherever motor vehicles regularly cross the alignment of the bicycle lane.

The bicycle system recommendations represent actions that may be do-able, with some adjustment of traffic patterns except where indicated in **Map 22**. In the longer term, these actions could be further complemented with aggressive implementation of a “Green Streets” program with supporting routes. However, virtually all remaining routing options except those indicated would require the elimination of either a traffic lane or parking lane in order to create sufficient width for a continuous bike lane.

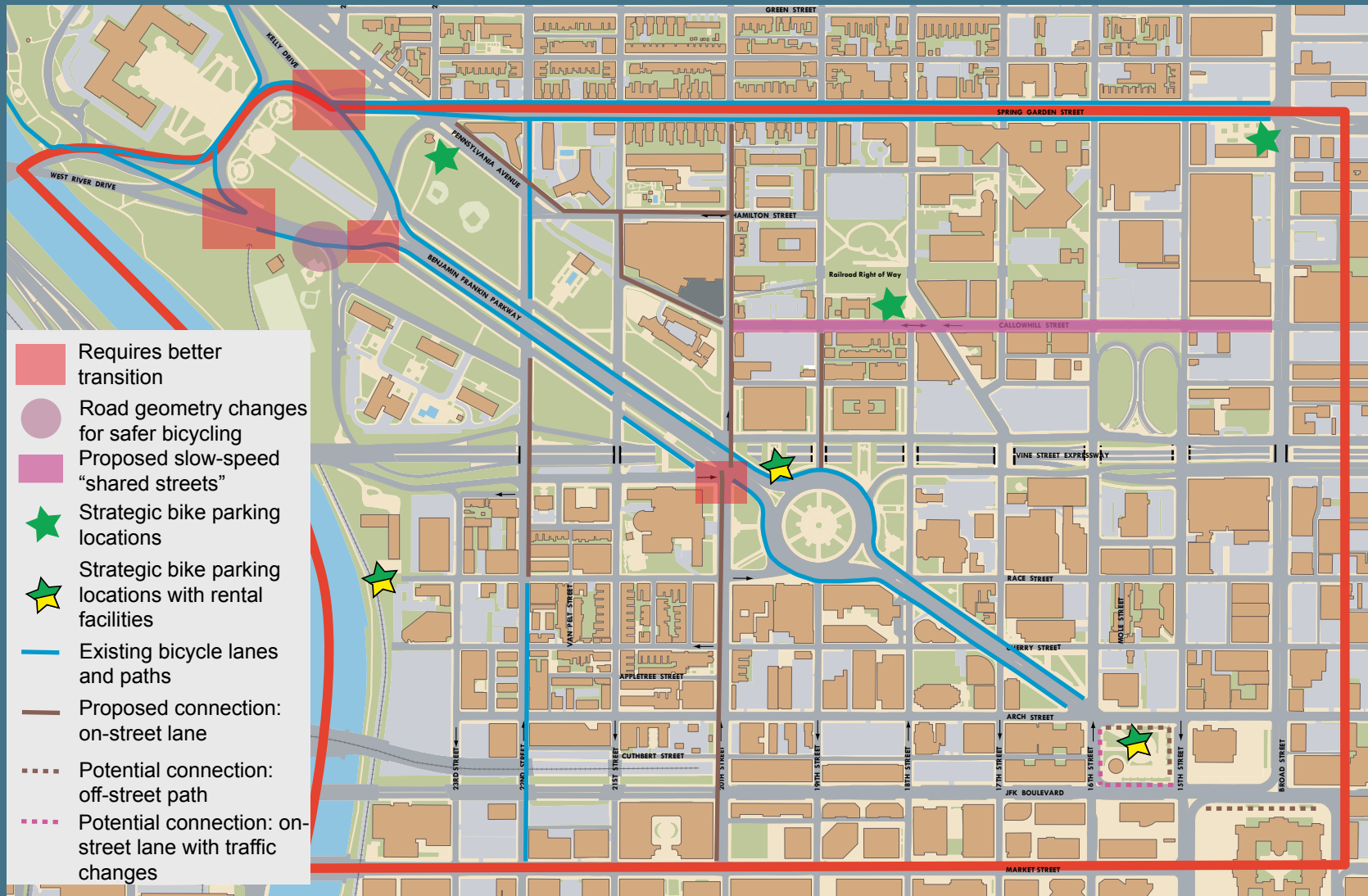
It should also be noted that tour/school buses often idle in bike lanes when staging for pickups from institutions and hotels; fixing this issue requires better coordination and control of bus access patterns.

Also shown in **Map 22** is a proposed “shared” bicycle zone on Callowhill Street, with an emphasis on traffic calming at locations where the addition of a bike lane would be impractical due to physical constraints and/or alternative planning goals (i.e. the establishment of a two-way, retail-oriented destination).

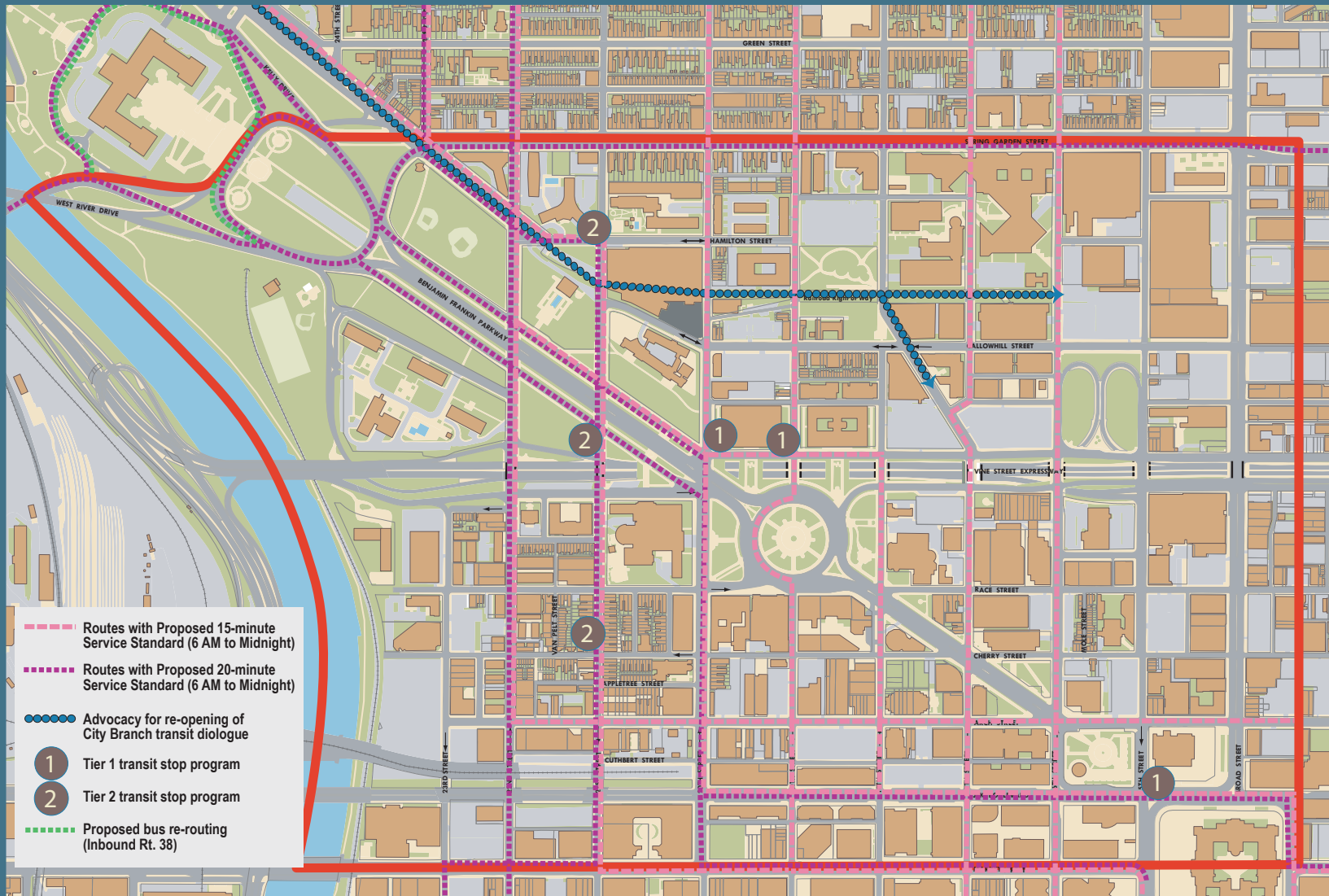
**Map 22. Proposed Bicycle System Enhancements** 



# PROPOSED BICYCLE SYSTEM ENHANCEMENTS



# PROPOSED TRANSIT SERVICE ENHANCEMENTS



### 5.2.5 Transit

The primary recommendations for transit shown in **Map 23** include service enhancements, infrastructure investment, and informational improvements:

#### *Service Enhancements*

Given the nature and growth of the Logan Square neighborhood and its surroundings, the LSNA plan should call for a review of SEPTA service levels with the following potential improvement measures:

- Peak service targets of 10 minutes or better on Routes 17, 32, 33, and 48, and 20 minutes on Routes 7, 38, and 43.
- Off-peak service targets of 15 minutes (consistent frequencies) on all main routes connecting the Logan Square neighborhood with the central business district (Routes 17, 32, 33, and 48). This minimal “service standard” would also apply to Saturdays and Sundays.
- The principle of “clockface” schedules for all

routes during off-peak periods, i.e. such that all buses on a particular route would pass a certain stop at standardized arrival times; for instance, :07, :27, and :47 past each hour.

- Replacement of 40-foot buses with 60-foot articulated vehicles where feasible, which could potentially ease overcrowding on Routes 17 and 32.

These types of enhancements would require advocating for a transit-planning policy change from a “reactive” demand-responsive paradigm to a “proactive” demand-generating paradigm. The actions most dependent on this type of evolution include:

- Negotiating for a 15-minute “service standard” on radial routes 2, 32, 33, and 48, with minimum service frequencies of fifteen minutes every day (including weekends) between 6 AM and midnight.
- Negotiating for a 20-minute service standard on peripheral/supporting routes 7, 38, and 43.

#### **Map 23. Proposed Transit Service Enhancements**



While SEPTA's budget is limited, recent support for the agency has opened the door to the potential for system-wide enhancement. Meanwhile, Mayor Nutter's commitment to sustainable design provides further impetus for shifting to a more proactive stance with respect to transit planning. The purpose of establishing consistent, minimum baseline "service standards" on all routes would be to actively draw people to the service, and also potentially to influence the basic lifestyle decisions (i.e. car or no-car) of new residents.

A final service issue is the ongoing operation of the Phlash Shuttle along the Benjamin Franklin Parkway. While this will likely continue to be a tourist-oriented daytime shuttle for the next three years due to its funding stream, there may be a subsequent opportunity for it to evolve into a more multi-dimensional transit-type service with benefit to residents as well as visitors. As this opportunity presents itself, it is important that the Logan Square neighborhood participate in discussions about Phlash's future, advocating key usability elements such as minimal service hours and fare/Smartcard compatibility.

Private shuttle services should be encouraged to work together to coordinate routes and schedules, to reduce the overall number of shuttles in the area and/or to provide a more consistent service.

### *Infrastructure Investment*

Infrastructure enhancements would consist of the following recommendations:

- Advocate for the use of the City Branch right-of-way as part of a high-frequency premium transit corridor (*Image 70*).
- Advocate for frequent, year-round transit service to access and link the Parkway institutions, the Please Touch Museum, the Philadelphia Zoo, Boathouse Row, Penns Landing, Independence Hall, and other tourist attractions along this route.
- Establish “Tier 1” transit stops at key locations, with the following amenities: substantial shelters, effective lighting, seating for four or more persons, and real-time information.
- Establish “Tier 2” transit stops at other important key locations, with the following amenities: shelter, effective lighting, and seating for two or more persons.
- Ensure that all transit stops include at least the following basic elements: clear signs identifying the route; adequate lighting and posted timetables and route maps (pole- or shelter-mounted).
- Increase the “reach” of the Race-Vine subway station by ensuring that Smartcard-operated “rotogates” are installed as part of the planned upgrade of SEPTA’s fare collection system, which would allow access into the station from Race Street as well as Vine Street.
- Support efforts to remake City Hall Station and Dilworth Plaza into a more welcoming gateway, using environmentally sustainable design principles.

With respect to potential premium transit service along the City Branch right-of-way, it should be noted that rapid development of the Logan Square neighborhood and West Fairmount Park—as well as the Art Museum and Free Library expansion projects and relocation of the Barnes Museum—have

increased the potential viability of a dedicated transit service along this route as a Center City “Culture Express”. LSNA should support a reexamination of this issue.

It should also be noted that this right-of-way (*see Map 23*) has significant historical value in that it was used as a direct link into the iconic Baldwin Locomotive Works on Broad Street. The potential reinvention of this corridor as a passenger route could derive added cultural benefit from incorporation of historic rail/industrial heritage elements into its stations. Intermodal connections to the Broad Street Subway and with bus service will be an important consideration.

### *Informational Improvements*

Finally, there are a number of basic informational improvements which could improve the convenience of using the current bus system, in particular:

- All bus stop signs should include the “STAR location number” which allows people to make a telephone call to an automated system to obtain an estimate of when the next bus will arrive.
- The automated system allowing people to call about their bus’ status should be simplified and made available 24 hours/day, 7 days/week.
- Bus detours for events should be consistent and well-advertised. The detour route for buses 32 and 38, for instance, could follow Spring Garden Street and 19th/20th Streets for all events, rather than the ad hoc detours representative of the current system dependent upon which streets are closed for which specific events. Permanent signs showing “Bus 32/38 Detour Route” could be installed given a consistent detour plan, along with signs along the regular routes indicating the event-day detour routes.

*Image 70. City Branch Right-of-Way offers the opportunity for a new culture-oriented transit service.* 







## 5.3 DEVELOPMENT









### 5.3.1 Neighborhood Conservation

Neighborhood Conservation Districts provide for control over the alteration of facades of existing buildings, and the design of new buildings in the District, to preserve the aesthetic fabric of distinctive Philadelphia neighborhoods. The controls affect the appearance of buildings, not the density of development, which is governed by the underlying zoning.

Consideration should be given to the adoption of a Neighborhood Conservation District for the historic core of Logan Square, within the area generally bounded by 20th, 23rd, Winter and Arch Streets, and/or other areas that meet the requirements of the City ordinance, to help maintain the character and quality of the residential district. A conservation district would involve the creation of community-based design guidelines developed in conjunction with the City Planning Commission. The design of any proposed exterior alterations to existing buildings, or design of new buildings in the district, would be subject to Planning Commission review for compliance with the guidelines, and issuance of a “Certificate of Compliance” prior to issuance of

a building permit. Some residents of the Parkway North area had suggested that a Neighborhood Conservation District should also be established for the residential area north of the Parkway, but the area does not seem to include the consistent physical characteristics required for a Neighborhood Conservation District.

To create an NCD, the Logan Square Neighborhood Association will need to submit a petition adopted by the Board to the Planning Commission requesting that they initiate the process. The Planning Commission then has up to 6 months to work with LSNA to establish the NCD boundaries and develop design guidelines, and an enabling ordinance. After a public meeting within the neighborhood, the ordinance would be forwarded to the District Councilperson, for introduction into Council. Before the council public hearing, the City Clerk’s office is required to inform the owners of all properties within the NCD boundaries. The ordinance would not be enacted if more than 51% of all property owners file statements of opposition to the designations.

*Image 71. Overview of Potential Neighborhood Conservation District*  
*Photo source: Phillyskyline.com*





### 5.3.2 Historic Resources

#### 1. *Survey of Historic Resources for Local Register Listing*

A survey of potential additional historic resources within the Logan Square-Parkway district should be completed, to determine eligibility for listing in the Philadelphia Register of Historic Places. Listing would encourage preservation of existing low-rise historic buildings in the neighborhood that might otherwise be candidates for demolition and development. Examples of buildings that should be evaluated for local listing include the 4-story loft buildings at 1926-36 Arch Street and the 4-story townhouse building at 1924 Arch Street. 1924 Arch Street is already listed on the National Register. Historic listing of buildings on this block could provide the basis for an integrated development of the block that would include both new and existing buildings. Preservation of these low-rise structures could encourage a development that maintains views to the north from Kennedy House, while also permitting a development that is consistent with existing base zoning density.

#### 2. *Transfer of Development Rights for Historic Preservation*

The C4, C5 and RC4 zoning districts permit additional floor area, on a one for one basis, for additional development rights transferred from a locally certified historic building, located in these zones. However, this provision has not been utilized. To qualify, the historic resource must have been designated by the Historical Commission as “threatened.” Currently, the Commission has no procedure for such a designation. The Planning Commission should develop detailed regulations to clarify this provision and encourage its use, as this transfer of development rights can potentially support the work of the Historical Commission by providing additional protections to historic resources.

The “threatened” designation requirement should be eliminated from the code. Designation of additional buildings as local historic resources would also potentially increase the number of buildings eligible for transfer of development rights. A land bank of develop right should be established, perhaps through the Preservation Alliance, so that such rights are available for purchase as needed by developers.

*Image 72. Example of Historic Resources to be Surveyed* ►







### 5.3.3 Zoning Map

The Logan Square Neighborhood - Parkway zoning map is generally consistent with existing and proposed land use. Zoning supports high-rise, high-density office and mixed-use development along the east-west spine of Market Street and Kennedy Boulevard, and in the area between 18th and Broad Street, north of Kennedy Boulevard. It supports medium to high-density residential and mixed-use development adjacent the Parkway, north of Vine Street and west of 18th Street, in the Parkway North area. It supports low-rise townhouse development in the historic core of the Logan Square residential neighborhood, between 20th and 23rd Street and between Winter and Arch. However, there are some areas of inconsistent and inappropriate zoning that should be changed to reflect desired development goals. Where map zoning changes are proposed, they should serve a broad public interest that encompasses the “general welfare” of the population, and should be consistent with overall neighborhood planning goals.

Instead, the plan recommends changes that impact the types of permitted uses, eliminate obsolete zoning, reduce bonus density provisions or change bulk controls. These areas where changes are proposed include the following:

1. The parcels fronting the south side of Arch Street north of the SEPTA tracks, between 20th and Schuylkill River, currently zoned C4, should be rezoned to RC4, similar to the parcels west of 23rd Street, to create a medium to high-density residential transition between the historic townhouse core of Logan Square and high-rise high intensity commercial development along JFK Boulevard and Market Street.
2. The G2 industrially zoned parcels, located between Callowhill and Spring Garden and between 16th and Broad Streets, should be rezoned C4, to support medium-intensity commercial development along the Broad Street corridor north of Callowhill Street.

*Map 24. Proposed Zoning Changes* 

# PROPOSED ZONING CHANGES





3. The block between 20th and 21st Streets, Hamilton Street and Pennsylvania Avenue, currently zoned C4, should be rezoned RC4, consistent with the adjacent Franklin Town development parcels, to encourage residential development, with supporting commercial uses. This would not result in an increase in allowable density. The residents of the high-rise City View Condominiums would like the block to be down zoned to C2, low-density commercial, with a 35' height limit to assure protection of their uninterrupted views to the Center City skyline to the south. However, such a change would not be supported by the city. Appropriate bulk controls with the proposed RC4 zoning could limit impacts on views.
4. Franklin Town Park, currently zoned R10A, should be rezoned Recreation, to reflect its use as permanent open space.
5. JFK Plaza/Love Park, currently zoned C5, should be rezoned Recreation, to reflect its use as permanent open space (with underground parking).
6. Coxe Park, currently zoned R10A, should be rezoned Recreation, to reflect its use as a permanent open space.
7. Parcels along the Broad Street corridor should be rezoned to higher intensity C5 to encourage highest-intensity development in this area close to transit.
8. The City-owned Boy Scout headquarters buildings at 22nd and Winter Streets should be rezoned from R14 to R10A to protect this publicly-owned historic resource.

*Image 73. Whole Foods Parking Lot at 20th/Pennsylvania Ave* 



9. The 1900 and 2000 blocks of Spring Garden should be rezoned from R15 to R10 to reflect the existing 45' height limit and the fact that both blocks are within the Spring Garden Historic District.
10. The small triangular parcel and air rights parcel at 21st and Hamilton Streets behind the Rodin Museum should be rezoned from R15 to R10, to limit development on this small sensitive site.
11. The G2 parcel on 19th Street that includes the Granary Building could logically be rezoned to RC4. Since the parcel is already developed with a multi-story structure, rezoning would not result in additional development on this site. G2 does not reflect existing or likely future use. However, the adjacent residents have proposed that the parcel be rezoned C2, to assure that any future development is limited in height.
12. Possible rezoning of the north side of the 1800 and 1900 blocks of Callowhill Street from C2 to RC4 was discussed. The C2 zoning reflects

former 3-story commercial structures that occupied these blocks. Most have since been demolished, and there are many vacant lots. The current zoning no longer reflects existing land use. Such a rezoning to RC4 could encourage higher density mixed-use development with first floor retail that could help invigorate this neighborhood retail corridor. While this proposal is supported by the Planning Commission, it is opposed by residents in the vicinity, who would like to retain the 3-story height limit and low-density commercial development on this section of Callowhill Street.

***Proposed zoning map changes are included on Map 24.***

Some residents in the Logan Square-Parkway area had suggested that mandatory Transfer of Development rights be used as a tool to reduce permitted potential development density in certain areas zoned for high-density, such as Arch Street and the North Parkway area. However, Transfer of Development Rights is only appropriate where there is a significant broad public benefit served by the transfer provisions, such as the preservation of historic resources, or valuable farmland, or other public amenity.



### 5.3.4 Specific Zoning District Controls

The specific zoning district controls for several of the zoning districts that affect the Logan Square Neighborhood - Parkway are inadequate to assure that new developments enhance the community and minimize adverse impacts on the established residential community. The neighborhood believes that some of the zoning classifications lack adequate controls on density, building bulk, and building spacing. It is likely that within the next 5 years, the existing zoning classifications will be modified, under zoning reform. However, in the interim, a zoning overlay should be established for the Logan Square neighborhood that incorporates specific controls only applicable to the Logan Square neighborhood, while eliminating the existing blanket 125' height limit provision in some areas as described below. The Planning Commission does not support this recommendation at this time, but is committed to an ongoing discussion with community members about development in the area. However, in this case the overlay would be the most effective short-term way of addressing community concerns over development density and bulk.

The following changes to specific district controls are proposed.

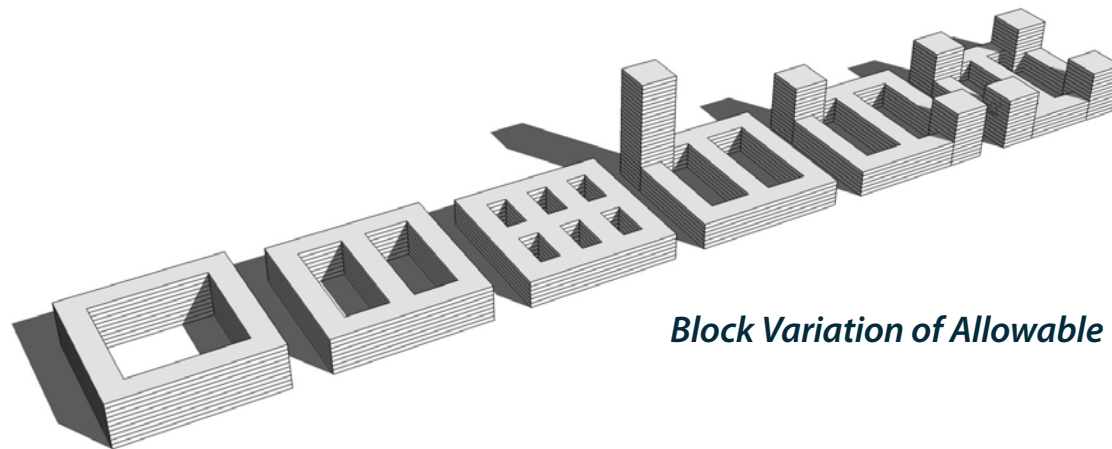
#### *1. Excessive Additional FAR (Floor Area Ratio) Provisions in C4 and RC4 Zoning Classifications*

The C4 and RC4 commercial and mixed-use zones in Logan Square function as a transition from the high-intensity C5 zoned office core, to the residential neighborhoods. The base FAR in the C4 and RC4 zones is 5, compared to 12 in C5. However, additional floor area of 8 FAR is granted in the C4, RC4, and C5 zones, for developments of larger parcels that meet specific requirements as outlined in the code and include public open space and other amenities. While a bonus FAR of 8 represents a 66% increase in FAR in C5, it represents an excessive 160% in C4 and RC4 zones. The base FAR of 5 in the C4 and RC4 zones provides an appropriate density of development to function as a transition from the C5 zone to townhouse residential neighborhoods and discourages encroachment of Center City office core functions on the residential neighborhood. The additional FAR provisions should be significantly

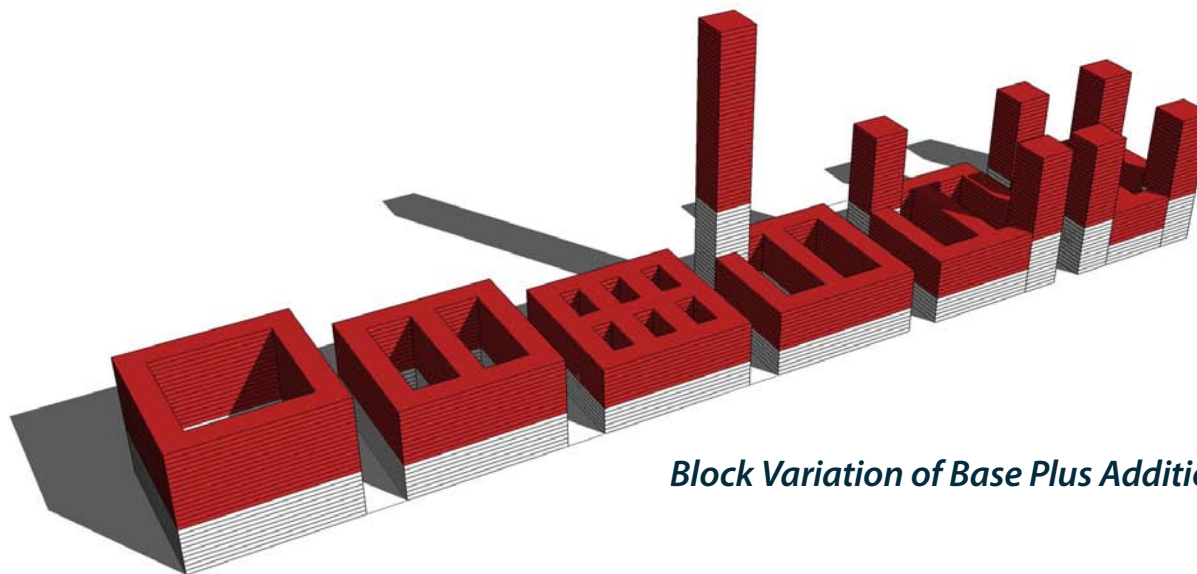
reduced for the RC4 zones in the Logan Square neighborhood. The additional FAR should be limited to an FAR of 2, representing 40% of the base FAR. The additional FAR in the C4 zone should also be limited to an additional FAR of 3, representing 60% of the base FAR. A maximum FAR of 7 (RC4) or 8 (C4) would be consistent with the maximum FAR achievable under the existing 125' height ordinance. The Planning Commission does not generally support reductions in the bonus provisions of the code. However, the River City proposal clearly illustrated the potential adverse neighborhood impact of development built to a FAR that includes all available density bonuses.

These changes should be addressed in the neighborhood overlay zone.

*Figure 9. FAR 5 vs. 13 Diagrams* 



*Block Variation of Allowable Base Floor Area Ratio: FAR 5*



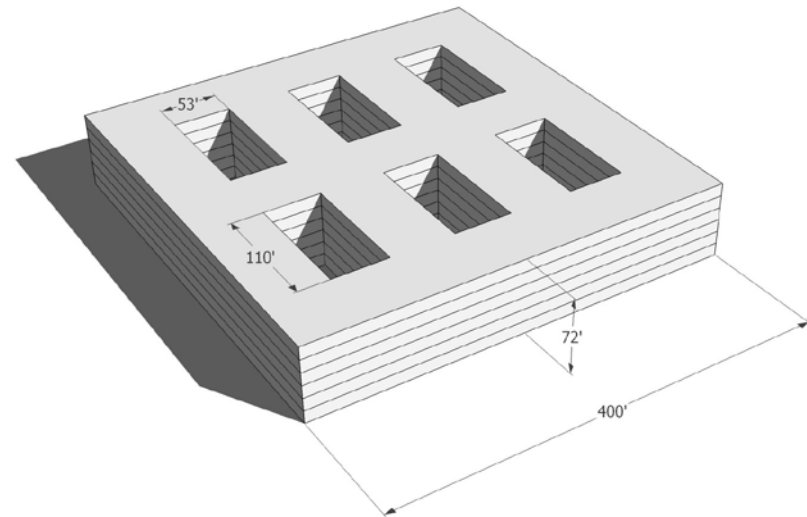
*Block Variation of Base Plus Additional Floor Area Ratio: FAR 13*



## FAR 5

6 story  
53' x 110' courtyards

A typical 400'x400' city block can be developed at FAR 5 with 6-story, 60-foot deep apartment buildings, facing public streets and small interior courtyards. This type of development is common in many historic European cities, such as the Medieval core of Barcelona, but is also found in the U.S., in areas such as the tenant blocks of the Lower East side of New York. There is a limited amount of open space, and views, sunlight, and daylight are restricted, but the pattern of development encourages active street life.



▲ Figure 10. FAR 5 Diagram: 6 Stories with Courtyards



▲ Image 74. Aerial of Medieval Barcelona, Spain



▲ Image 75. Ground level images of Medieval Barcelona, Spain

## FAR 5

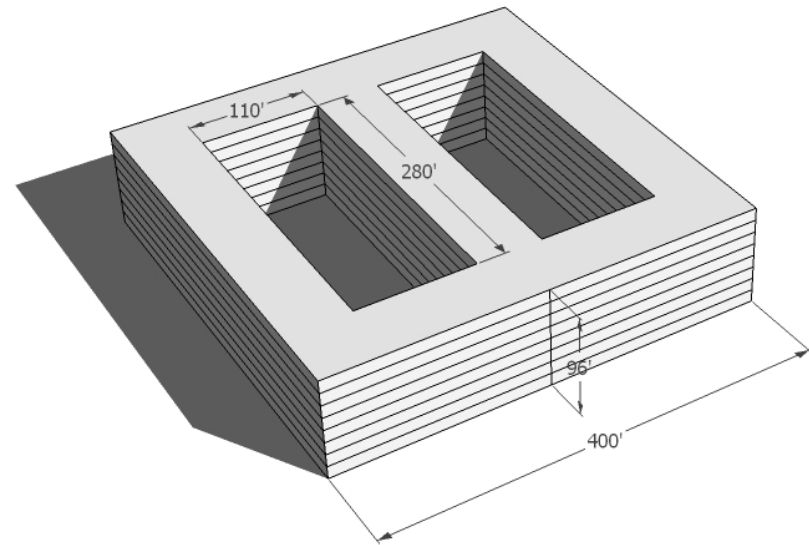
8 story

110' x 280' courtyards

The same city block developed at FAR 5 with 8-story, street facing apartment buildings offers the opportunity for more generous courtyards and open spaces, allowing greater penetration of light and air. However, views are limited, and the 8-story buildings require wider public streets to allow the penetration of daylight and sunlight. This type of development is typical of the 19th century Eixample district of Barcelona, where 7-8-story buildings front wide boulevards, but is also typical of new apartment developments in Seattle.



▲ Image 76. Aerial of L'Eixample, Barcelona



▲ Figure 15. FAR 5 Diagram: 8 Stories with Courtyards



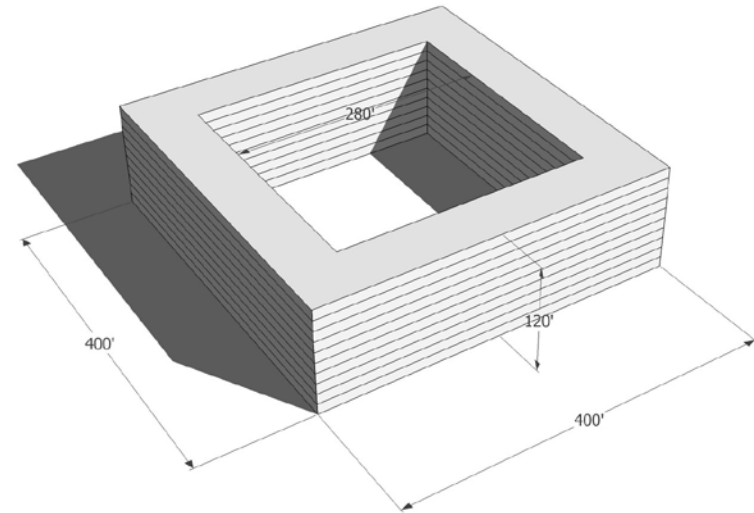
▲ Image 77. Image of L'Eixample district, Barcelona



## FAR 5

10 story  
280' x 280' courtyard

Ten-story street facing apartment buildings provide the opportunity for significantly larger semi-private courts that can promote the opportunity for a wide variety of activities, and good daylight and sunlight penetration. However, 10-story buildings require significantly wider streets – up to 100' wide, to assure adequate daylight and sunlight penetration. This type of development is typical of Washington, D.C.



▲ Figure 11. FAR 5 Diagram: 10 Stories with Courtyard



▲ Image 78. City Vista Development, DC



▲ Image 79. City Vista Development, DC



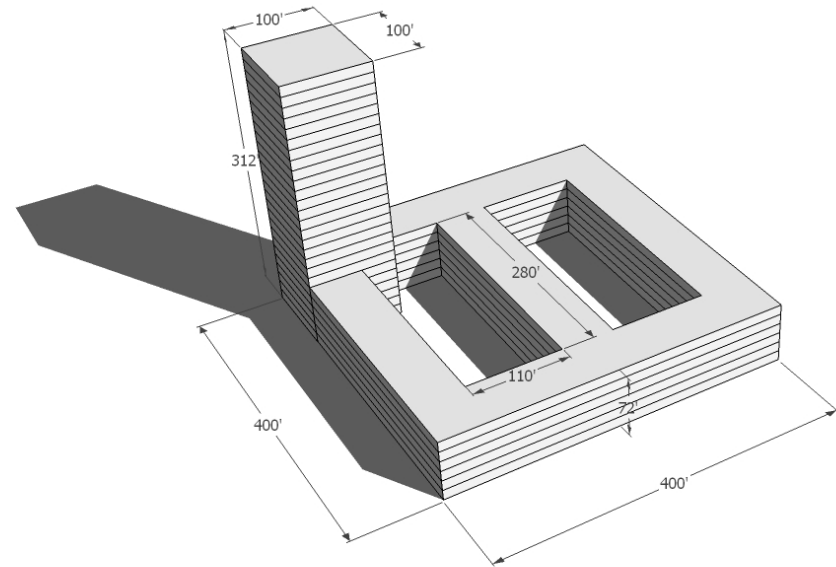
## FAR 5

6 story podium  
One 26 story tower

An FAR of 5 can also be achieved with a combination of a low-rise 6-story residential/mixed use podium and a small footprint (100'x100') high-rise resident building, 26-stories high. This configuration provides an attractive low-rise streetscape that can work on Philadelphia's narrow streets, but also provide the opportunity for widely spaced elegant high-rise towers that can offer views across the city, without blocking sunlight and daylight to adjacent low-rise buildings. This type of development is common in cities such as Vancouver, BC.



▲ Image 80. Aerial of Vancouver, CA



▲ Figure 12. FAR 5 Diagram: 6 Stories with Tower



▲ Image 81. Ground level images of Vancouver, CA

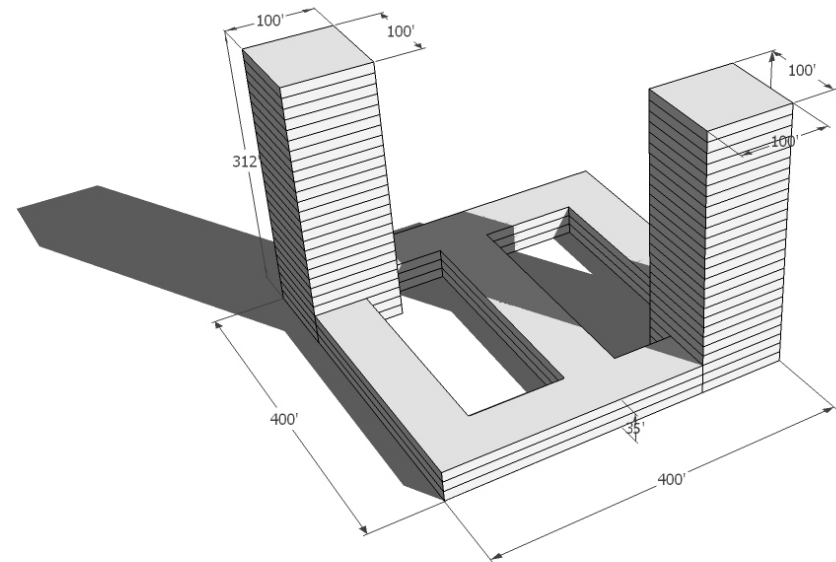
## FAR 5

*3-story townhouse podium  
Two 26-story towers*

An FAR of 5 on the same block can be achieved with a combination of 3-story single-family townhouses, and 2 small footprint residential apartment buildings, 26-stories high. This configuration provides the opportunity for both an attractive low-rise streetscape, with plenty of sunlight and daylight access, as well as for individual townhouses with private yards, in place of apartments. Widely spaced small footprint high-rise buildings, promote the opportunity for views, without creating adverse shadow impacts on adjacent low-rise development.



▲ Image 82. Aerial of Vancouver, CA



▲ Figure 14. FAR 5 Diagram: 3 Stories with 2 Towers



▲ Image 83. Ground level images of Vancouver, CA

## 2. *Lack of Adequate Building Bulk Controls in C4, RC4, and C5 Zones*

There are no height limits in the C4, RC4 or C5 zoning classifications. Under the existing code provisions, building footprint, setback and building spacing requirements are provided only for lots fronting the south side of JFK Boulevard, between 15th Street and the Schuylkill River, and south side of Market Street between 15th Street and 24th Street. On both the south side of JFK Boulevard and the south side of Market Street, the width of any building over 65' high is limited to 250', or 66% of the block frontage, whichever is less. The minimum spacing between buildings over 65' high is 75 feet. These requirements were established to maintain sunlight penetration to the sidewalks of Market Street and JFK Boulevard. These requirements do not apply to the north side of the street, and thus do not address daylight penetration to the street. While the requirements would prevent a block-long high-rise building such as Kennedy House on the south side of the street, they would not prevent additional such block-long buildings on the north side of the street, with consequent potential adverse impacts on

daylight access (*Image 80*).

While the existing RC4, C4 and C5 regulations require that buildings occupy no more than 75% of the lot area above 65 feet, up to 300 feet, throughout the districts, there are no further restrictions on building height, width or spacing under these individual zoning classifications.

The Logan Square Neighborhood - Parkway is covered by the Special Height Control District, passed in early 2007, which imposes a building height limit of 125', on much of the neighborhood west of 18th Street. However, the bill was passed as a height moratorium measure in response to the Barnes Tower/Parkway 22 controversy, with the anticipation that the height control issue would be more fully addressed as part of the Logan Square Neighborhood - Parkway Plan.

The 125' height restriction, absent bulk controls, could potentially create greater adverse neighborhood impacts than no height limit on some blocks, as the developer would need to build substantially a larger, bulkier building to achieve the same FAR, potentially



resulting in a 125-foot, 12-story street wall building that would block views, and limit daylight and sunlight access to floors in adjacent buildings below 12-stories. In contrast, a small-footprint higher tower, coupled with a low-rise 4- to 6-story podium building would have some impact on views, but permit daylight and sun penetration, as well as views between buildings.

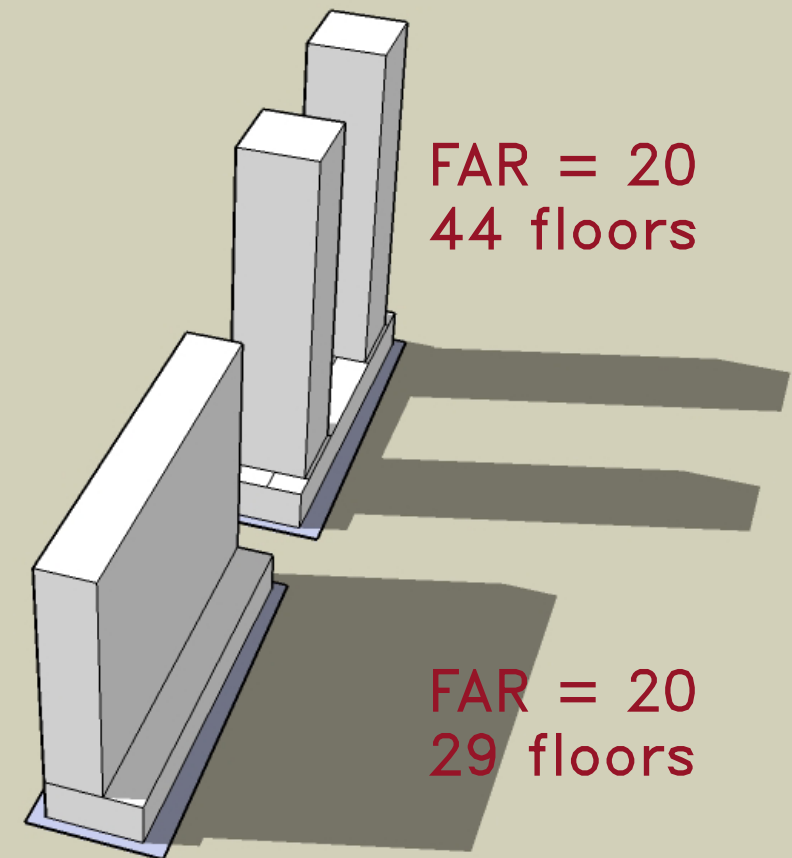
Many blocks in the Logan Square Neighborhood - Parkway are zoned C4 or RC4, and have a base FAR of 5. An FAR of 5 can be achieved with a variety of building forms, each with distinct advantages and disadvantages. Theoretically a 5-story building that covers the entire site could achieve an FAR of 5, but such a building would not be feasible for most uses, except perhaps for a parking garage, because of the need to provide light, and air and open space for residential uses. Examples of 6-story developments that achieve an FAR of 5 include the medieval section of Barcelona, or the tenements of the Lower East Side of New York, where daylight and air to the interior of the block is provided by small interior courts and light wells. Contemporary building forms, such as double loaded corridor apartment developments can achieve an FAR of 5 with a street wall building of a

minimum 7-8 stories, while providing some open space. Alternatively an FAR of 5 can be achieved with a mix of 3-4-story single-family townhouses, coupled with widely spaced small footprint high-rise buildings, up to 30 stories.

Advantages and disadvantages of alternative development configurations to achieve an FAR of 5 are listed in **Table 4**. An 8-story street wall development will protect views from most existing high-rise developments (above 8 floors). In contrast, a mix of 3-4 story townhouses, with an occasional small footprint high-rise building can provide a wider variety of unit types, provide a development more conducive to attracting families, and result in a higher value development. While a 7-8 story street wall development can work well on wider streets, such as Arch Street, or in cities such as Washington, DC, with wide streets, a 7-8 story street wall development on both sides of the street can create a dark streetscape on Philadelphia narrow streets. While on some blocks a low-rise townhouse podium and high-rise configuration may be appropriate, on other sites a mid-rise “street wall” configuration may be appropriate.

*Image 84. Kennedy House.*





Alternate configurations for  
an FAR 20 developed on the  
Kennedy House site

In many other cities that place a high value on the quality of the private and public built environment, there is careful control of building height and bulk on a block-by-block basis to protect daylight access and views. For instance, the San Francisco Zoning Code includes two separate zoning maps, one covering zoning use districts, and one for building height/bulk controls districts. Within any one zoning use district, bulk controls vary on a block-by-block basis, depending on the specific physical and contextual character of the block.

In San Francisco, under the height and bulk limitations, each block is assigned a numeric and letter designation. The numeric designation refers to the permissible building height in feet, while the letter designation relates to a table that provides a maximum building length and diagonal building plan dimensions that apply above a certain height. Some districts also provide for maximum spacing requirements between buildings above building podium height..

Similarly, in Seattle, the use district classification text in the zoning code does not include either building bulk or FAR restrictions. The building bulk

restrictions are instead indicated on the zoning map, where for instance, an area designated for downtown residential mixed-use is further subdivided into various height districts based on building height restrictions, designed to protect views. Permitted FAR varies within each zoning use district, depending on the specific height sub-district.

Key development opportunity sites in the Logan Square Neighborhood - Parkway were tested under various redevelopment scenarios, to develop appropriate guidelines for building height, massing, spacing and building location. Various alternative scenarios were analyzed for the same FAR, based on retaining the existing 125' height limit, and eliminating the 125' height limit, but imposing other building height/bulk and spacing provisions.

The parcels used to test these options included:

- 1900 block of Arch, through to Cuthbert
- 23rd and Arch parcels through to the Schuylkill River
- 2000 block of JFK Boulevard/north side/ SEPTA air rights



**Table 4: Development Options for Achieving an FAR of 5, and Evaluation of Each Option**

| BLOCK CONFIGURATION  | EVALUATION CRITERIA              |  |   |  |                |             |                                    |                                |                |                              |   |   |
|--|----------------------------------|--|---|--|----------------|-------------|------------------------------------|--------------------------------|----------------|------------------------------|---|---|
|  | Potential for Private Open Space | Potential for Street Setback Landscaping | Potential for Common/ Public Open Space | Suitability for Families with Children | Value of Units | Street Life | Daylight/ Sunlight Access to Units | Views from Units               | Units Per Acre | Daylight/ Sunlight at Street | Views from Lower Levels of Existing High-Rise | Views from Upper Levels of Existing High-Rise |
| 1. 6-story full block buildings with light wells                                     | Poor                             | Poor                                     | Poor                                    | Poor                                   | Poor           | Excellent   | Poor                               | Poor                           | 180+           | Fair                         | Poor (below 6th floor)                        | Excellent (above 6th floor)                   |
| 2. 8- to 12-story full block buildings with central court (existing height controls) | Poor                             | Fair                                     | Excellent                               | Poor                                   | Fair           | Excellent   | Fair/ Good                         | Fair/Good                      | 180+           | Poor                         | Poor (below 12th floor)                       | Excellent (above 12th floor)                  |
| 3. Low-rise townhouse buildings (4-story) with one small footprint high-rise tower   | Excellent                        | Excellent                                | Excellent                               | Excellent                              | Excellent      | Fair        | Excellent                          | Good (excellent for high-rise) | 130+           | Good                         | Good  | Good  |
| 4. Low-rise townhouse buildings (4) with 2 medium height larger footprint towers     | Excellent                        | Excellent                                | Excellent                               | Good                                   | Good           | Fair        | Excellent                          | Good (excellent for high-rise) | 130+           | Fair                         | Fair  | Fair  |

▲ **Table 4. Development Options for Achieving FAR5**

- 2000 block of Hamilton/Pennsylvania Avenue (Whole Foods site)
- 1800 block of Hamilton Street, facing Franklin Town Park

Possible building height, bulk and spacing restrictions were developed based on the outcome of these studies.

The 3-dimensional models illustrate the impact of potential developments using the same FAR, with and without the 125' height restriction.

A possible building height and bulk district map for the Logan Square-Parkway area, as an overlay to the revised zoning use district map, is illustrated in **Map 25**. The height and bulk district map would include the following controls:

- Maximum threshold building height
- Heights above which maximum building footprint diagonal and spacing dimensions apply

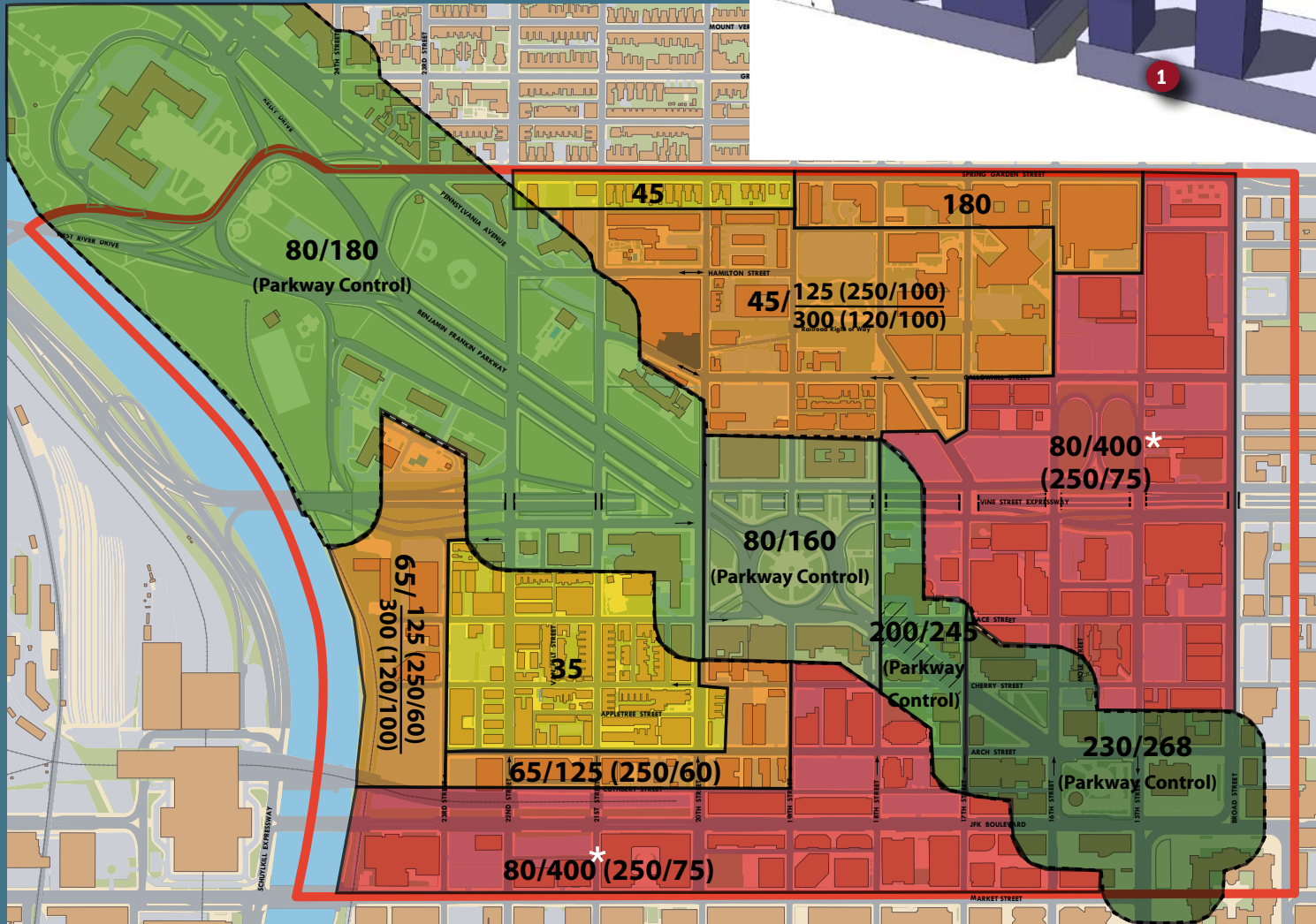
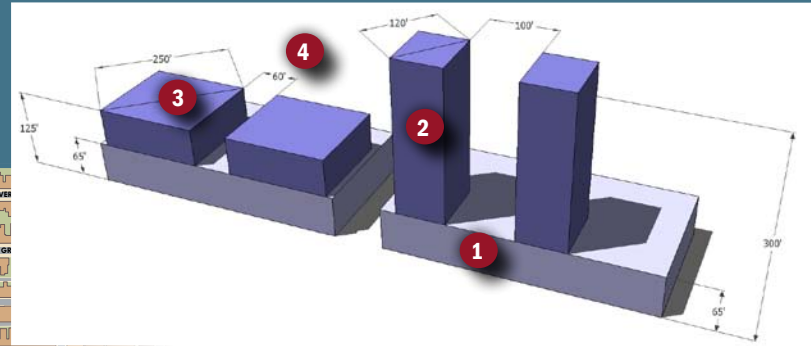
- Maximum building footprint and diagonal dimension for each height limit
- Minimum spacing between buildings above heights at which maximum building dimensions apply

The details of the height and bulk controls would need to be developed in conjunction with the Planning Commission staff. Potential building height and bulk restrictions assume elimination of the existing 125' height limit. The conceptual height and bulk district map recognizes that the appropriate building footprint dimensions for an office building in the C4 and C5 zones are different than those for a residential building in the RC4 zone. Different bulk controls are appropriate for the RC4 zones abutting the historic residential core of Logan Square (where zoning providing a transition between the high-density office core, and low-rise residential neighborhoods), and the Parkway North area, where there is a mix of low-rise townhouses and high-rise buildings. Different height and bulk controls are appropriate for sites facing a wide street compared to sites facing a narrow street.

**Map 25. Proposed Building Height & Bulk District** 

# PROPOSED BUILDING HEIGHT & BULK DISTRICT MAP

- |                 |                      |                                 |  |
|-----------------|----------------------|---------------------------------|--|
| <b>1</b>        | <b>2</b>             | <b>3</b>                        | <b>4</b>                                 |
| 65/125 (250/60) | MAX HEIGHT OF PODIUM | MAX DIAGONAL DIMENSION OF TOWER | MAX BUILDING HEIGHT LIMIT                |
|                 |                      |                                 | MIN SPACE BETWEEN BUILDINGS ABOVE PODIUM |



\* THRESHOLD HEIGHT LIMIT. GREATER HEIGHT MAY BE ALLOWED SUBJECT TO PLANNING COMMISSION REVIEW

0 125 250 500 Feet

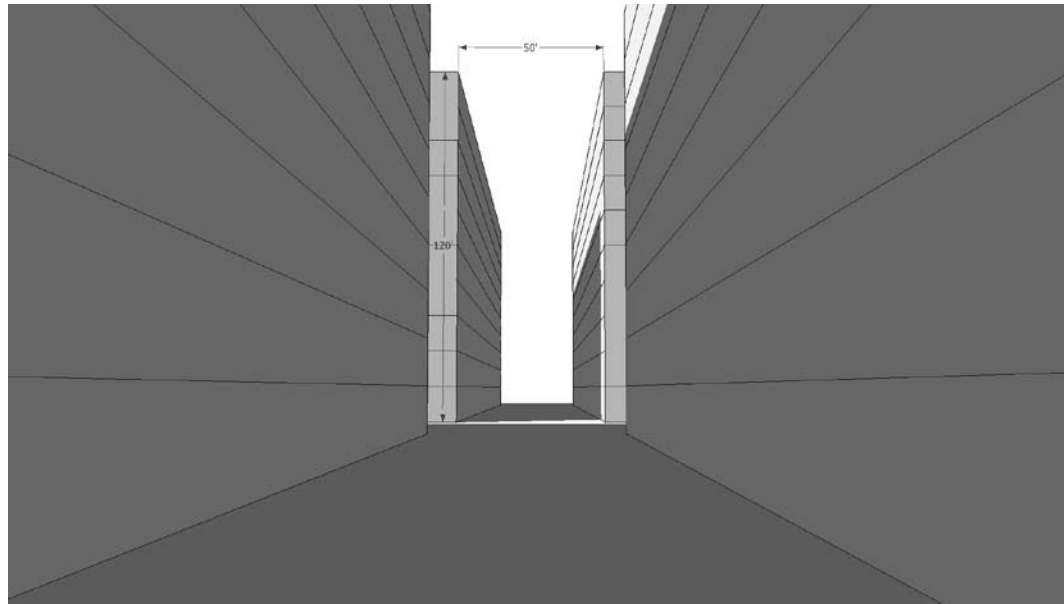


For high-rise commercial buildings in the C4 or C5 zone (above the 80' high podium), maximum building dimensions of 250', with a minimum 75-80' between buildings are appropriate. A maximum 250' building dimension would permit a typical office building footprint of 25,000 sq.ft., while a minimum 75-80 foot spacing would assure adequate daylight penetration between buildings and long distance views.

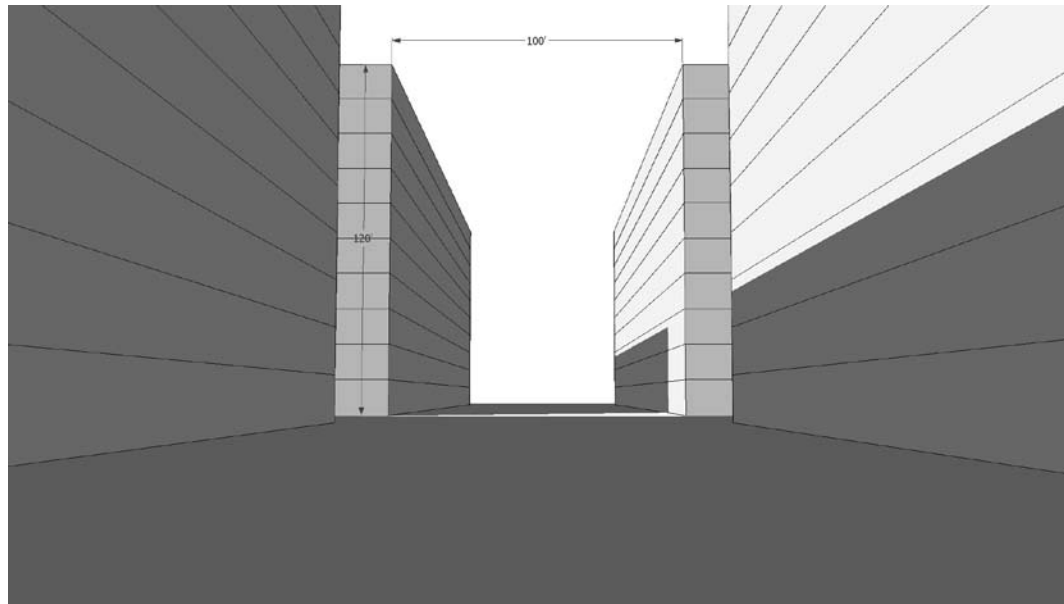
Maximum building diagonal dimensions and spacing in the RC4 zones are proposed to help to assure daylight and sunlight penetration between buildings, and maintenance of views. For high-rise residential buildings in the RC4 residential/commercial zone, maximum building dimensions of 100', with a minimum spacing of 100' between buildings are appropriate. A 100' building dimension would permit a small footprint 10,000 sq.ft. apartment tower with 4-8 units per floor, while a minimum 100 foot spacing would assure adequate daylight and sunlight penetration between buildings, provide privacy between residential units, and protect views (**Figures 15a and 15b**). For the RC4 Parkway North area, the height of buildings for which there

are no proposed bulk controls could be limited to 50 feet (4 stories), to encourage townhouse-style development. The appropriate maximum diagonal dimension of a building between 50' and 125' in height could be 200' (consistent with the Tivoli), with a minimum building spacing of 100'. For buildings between 125' and 300' in height, the appropriate maximum diagonal dimension could be 100' with a minimum building spacing of 100'. Such controls would prohibit the block-long slab buildings such as Kennedy House and the Sterling, which have had such an adverse effect on the character and quality of the environment of JFK Boulevard, or the City View Condominiums, which create a block-long visual barrier. They would encourage small footprint, fine-grained residential towers such as those in Vancouver (typically 85'x85', 7,300 sq.ft. footprint) or the classic Society Hill Towers (109x73, 8,000 sq.ft. footprint).

In the short-term, these requirements should be refined and included in the overlay district for the Logan Square Neighborhood - Parkway.



◀ **Figure 15a. 10 story building on 50' ROW limits daylight penetration.**



◀ **Figure 15b. 10 story building on 100' ROW permits good daylight penetration.**

### 3. *Inappropriate Area Bonuses for Setbacks in R15 Zone*

Typically, buildings should be built at or close to the street line (back of sidewalk) to maintain an attractive, lively, urban streetscape, or setback no more than 10' to create a widened sidewalk area for sidewalk cafes, stoops, small landscaped boxes, etc. However, zoning controls in R15 provide FAR bonuses of two sq. ft. for each square foot of setback for buildings setback a minimum 10' from the street line. The result is to encourage high-rise high-density developments, setback from the street, without any connection to the city, such as Park Towne Place. For R16 districts, Section 14-1303 of the Zoning Code, Additional Floor Area, permits an additional 15 sq.ft. of floor area for each square foot between the street line and building line. While these incentives may be appropriate for other areas of the city, both of these incentives encourage undesirable development forms in Logan Square. In the zoning overlay, the applicability of provisions of Section 14-208 relating to additional floor area in R15 for buildings setback from the street line 14-208(3)(a) and (b) should be

deleted, as well as provisions of Section 14-1303 relating to R16 buildings constructed back from street lines. Incentives for modest setbacks of up to maximum of 10' should be included in the bonus provisions for public open space.

**Figure 16** shows potential development using the FAR bonuses currently available in the study area.

*Figure 16. Potential Development based on allowable base FAR & FAR bonuses* 

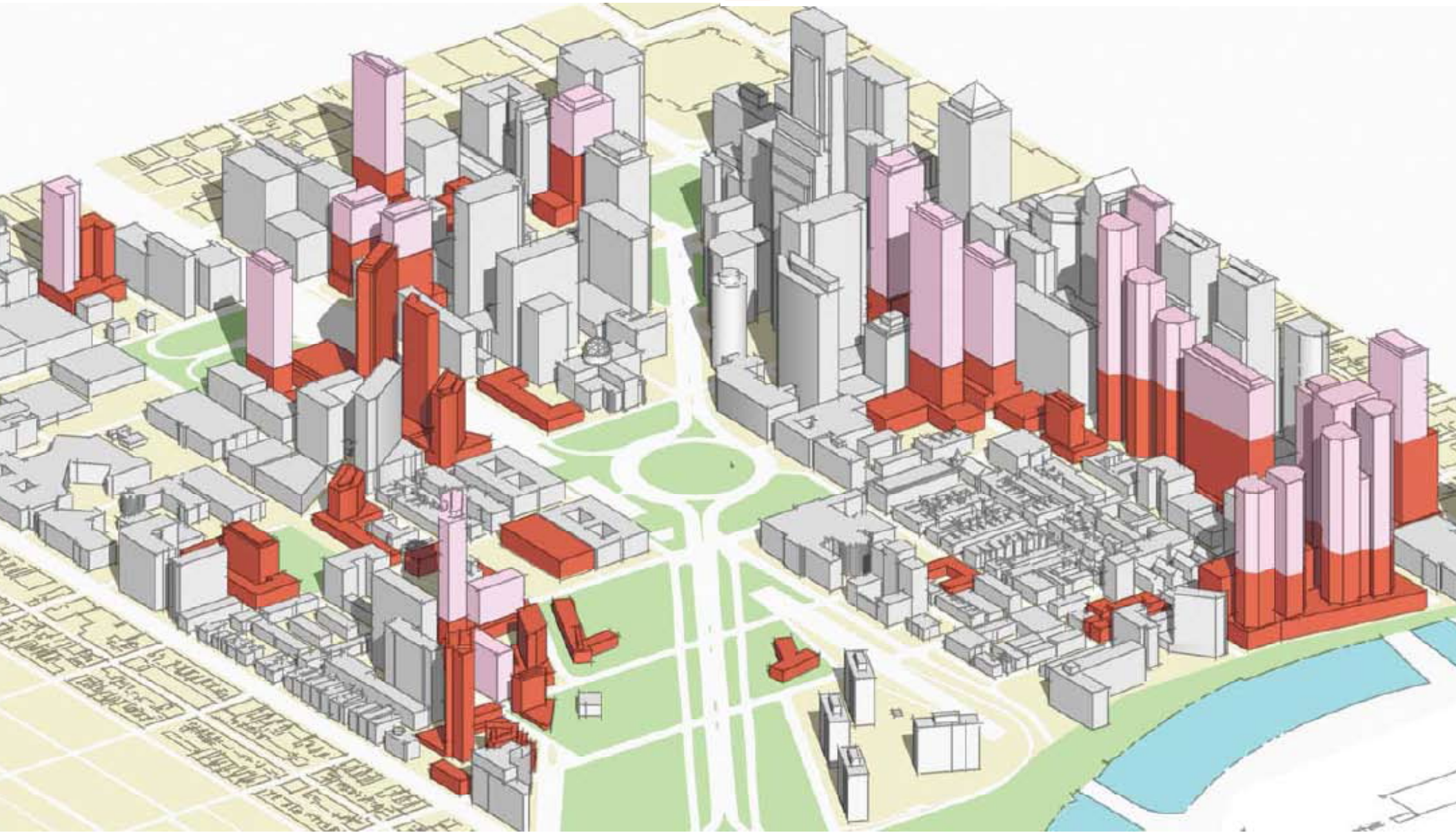


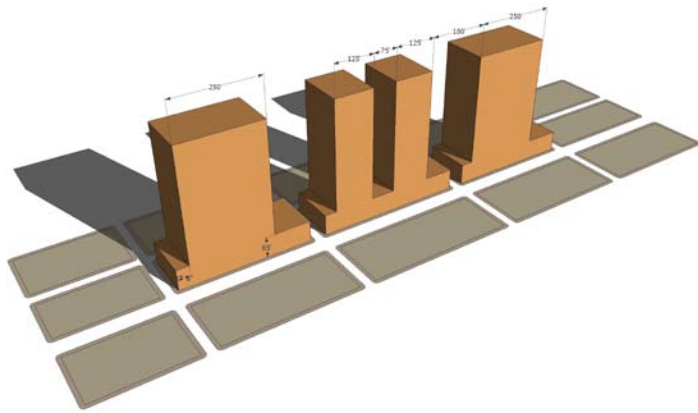


Allowable base FAR

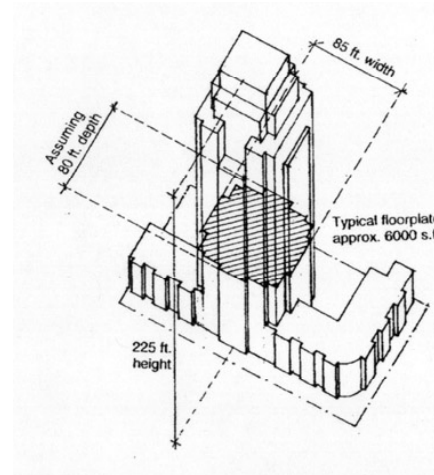


FAR bonuses available

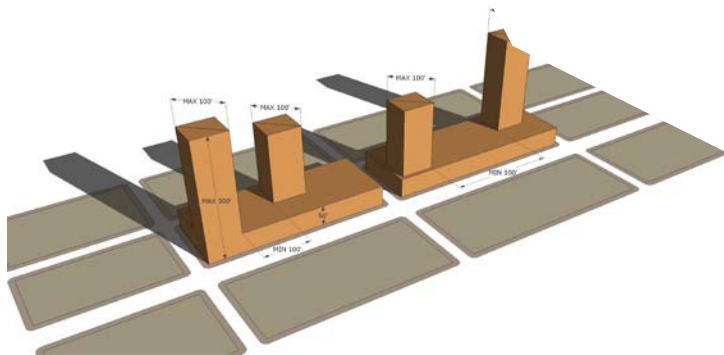
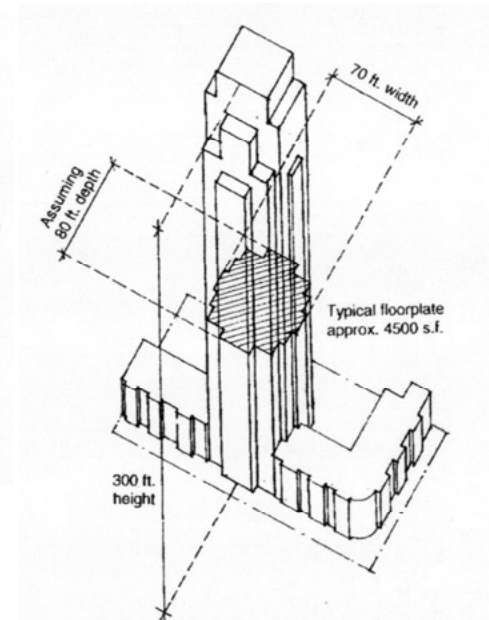




▲ **Figure 17. Existing Bulk Controls:** south side of JFK Boulevard should be extended to north side.



▲ **Figure 19. Vancouver's bulk standards** require slimmer towers as height increases to allow more openness and daylight along the street.



▲ **Figure 18. Possible Building Bulk Controls, RC 4 zones** in Parkway North area.



▲ **Image 85. Massing of building** should maximize daylight access and views.



# Inappropriate bonuses for building setbacks in R15 zones.

## PERIMETER BUILDING

Base FAR = 3.5

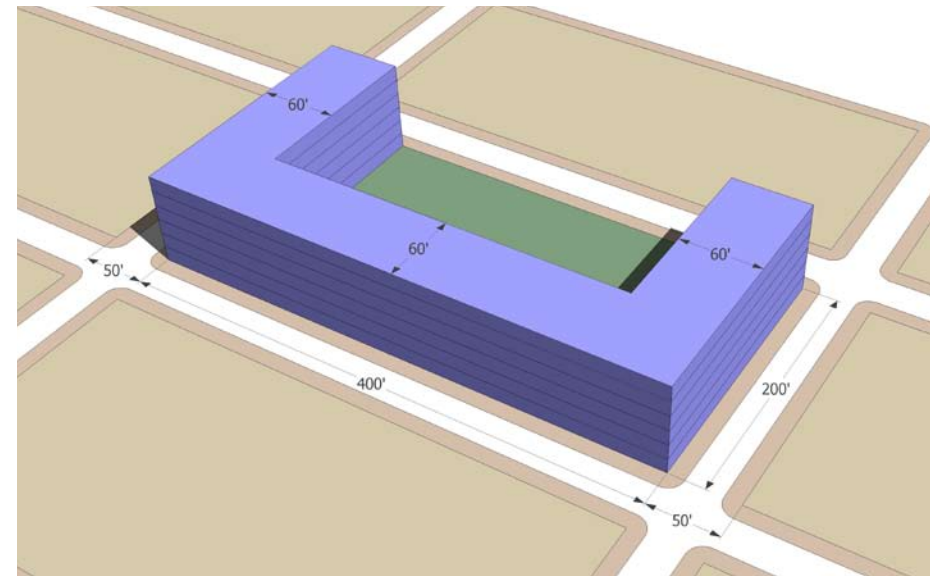
Lot Area = 80,000 SF

Buildable SF = **280,000 SF**

Building Footprint = 40,800 SF

Number of Floors = 6

=



## SLAB BUILDING

Base FAR = 3.5

Bonus FAR = 4 SF for every square foot between street line and the building

Lot Area = 80,000 SF

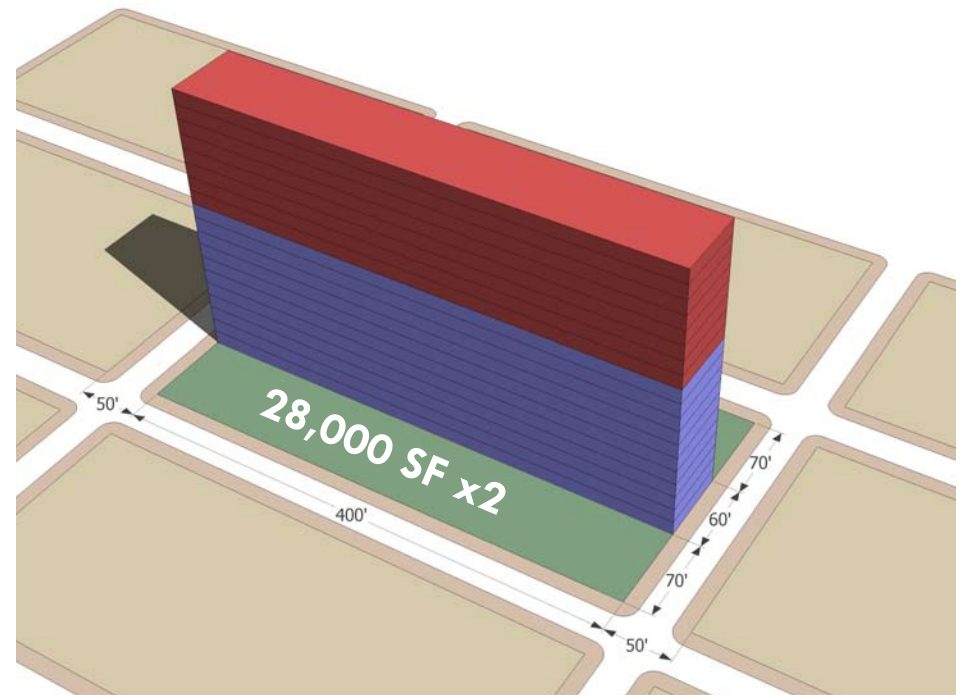
Buildable SF = **456,000 SF**

Building Footprint = 24,000 SF

Number of Floors = 19\*

\* Height limited to 2 times the distance from windowed wall to center line of street

=



R15 zoning currently permits much higher density for a pedestrian unfriendly high rise building setback from the street



#### 4. Accessory Parking Garages

To eliminate the adverse visual impacts of exposed parking podiums, above-ground parking garages as an accessory use should be prohibited within all zoning classifications in the Logan Square Neighborhood - Parkway, except under all the following conditions:

- The garage is completely wrapped in active uses permitted under the zoning classification for a minimum depth of 25' around the perimeter of the garage, for all facades that face or are visible from a public street, except alleys.
- Parking access should be limited to two openings, maximum 11' wide each.

▼ *Image 86. In appropriate above-ground parking garage at Murano condominium development*



To provide a further incentive for the provision of underground parking, an additional floor area of 350 sq.ft. per space should be provided for each underground parking space (as long as no above-ground parking is provided) in all medium- and high-density zoning classifications (C4/C5, RC4, R15, and R16). This additional floor area would only apply to spaces that do not exceed the minimum parking requirements. Total additional floor area could not exceed the new maximum additional floor area defined in on page 175.

#### 5. Loading

To eliminate the adverse streetscape impacts of loading bays, access openings to loading bays should be limited to one opening, maximum 15 feet wide. Where more than one loadings spaces is required by

▼ *Image 87. Comcast loading dock with excess width*



code, such spaces and access and circulation should be provided within the building.

#### 6. Non-Accessory Garages

Under RC4 and C4/C5 zoning, parking garages — both public and private — are permitted as a main use, with a special use permit. Retail commercial uses are required where a garage abuts a public sidewalk. Because of the adverse impacts of all above-ground parking garages on the urban streetscape, all above-ground parking garages, as a principal use, should be prohibited within any zoning classification in the study area, west of 16th Street, and in RC4 zoned parcels throughout the neighborhood.

▼ *Image 88. Unsightly exposed parking garage podium without active uses.*



#### 7. Open Space Requirements

The Logan Square Neighborhood - Parkway includes significant open space resources, such as the Parkway, Logan Square, and Franklin Town Park, as well as access to the Schuylkill River Trail. However, much of this space is not well suited to small-scale, family-oriented activity. Many municipalities, such as San Francisco and Chicago, have incorporated requirements for a minimum provision of open space per dwelling unit. This open space may be in the form of private usable open space, such as balconies and private roof terraces, common usable open space, such as roof terraces over parking, or grade level enclosed on open public space, or publicly accessible open space that is directly accessible to residential developments (*Image 89*).

▼ *Image 89. Active uses wrapping the perimeter of a garage.*



Zoning controls for the San Francisco downtown residential district of Rincon Hill require a minimum 75 square feet of open space per dwelling unit (private, common and public). A minimum of 40% of the open area is required to be common to all units in a development, and a minimum of 36 square feet of open space per dwelling unit must be provided on site. Chicago requires a minimum 36 square feet of open space per unit.

All high-density district zoning classifications (C4, C5, RC4, R14, R15, R16) should include a provision that accessible open space be provided at a minimum of 40 square feet per unit for all residential developments. This open space may be in the form of “green” accessible roofs, roof terraces, balconies, and yards, etc. Minimum requirements should be established for planting and landscaping of common and public access areas.

Controls should require a sufficient depth of soil above underground or above-ground, wrapped structures to provide the opportunity for landscaped open space where parking is not covered by development.

In the short term, these zoning requirements should be adopted as part of a zoning overlay for the Logan Square Neighborhood - Parkway

### 5.3.5 Design Review for Major Developments

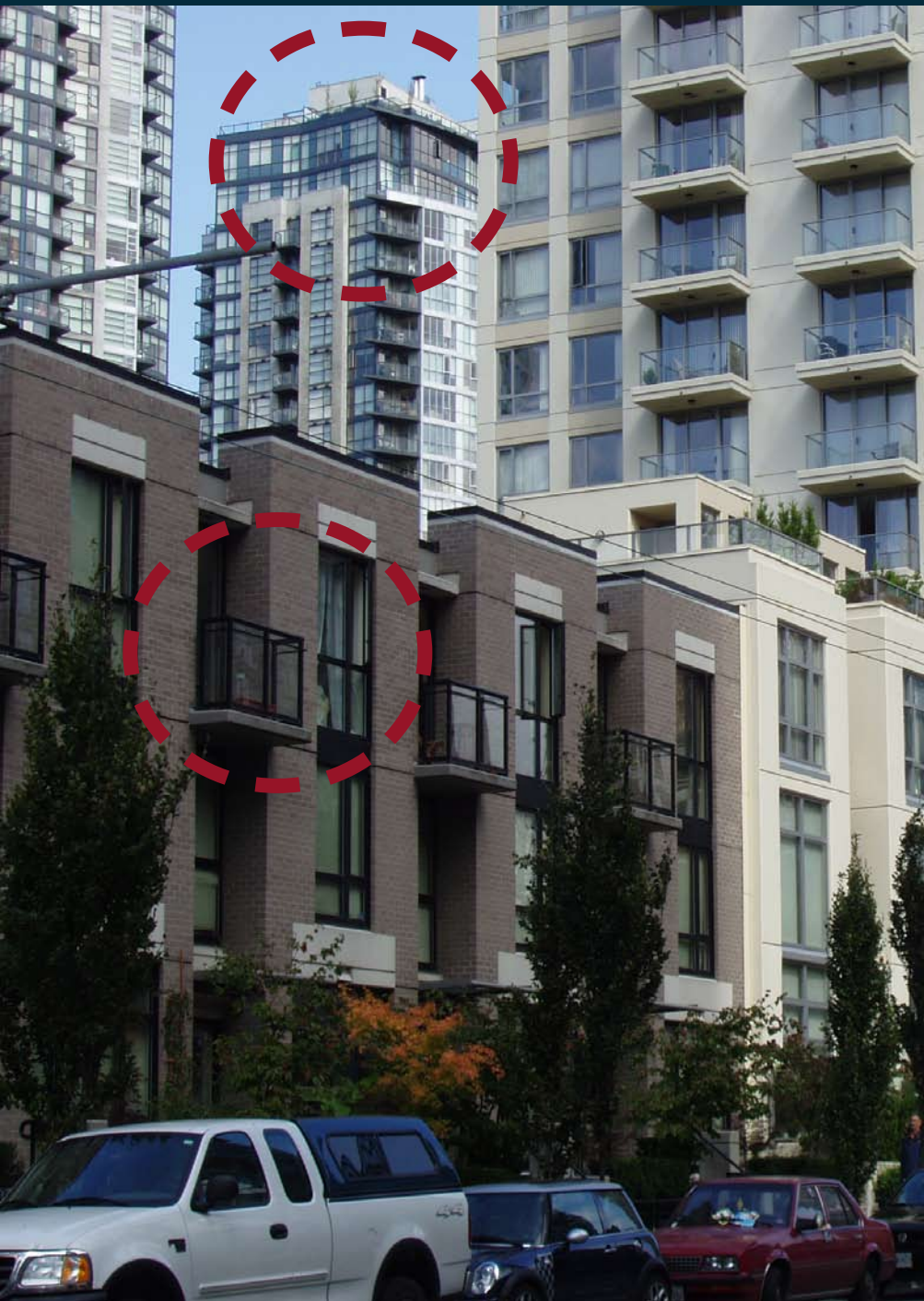
The current Philadelphia Zoning Code does not provide for design review, or community input, for major developments that could have a significant impact on the community. Currently, because of Philadelphia’s antiquated zoning code, most major projects require a zoning variance, and a zoning hearing, and thus an opportunity for the community to comment on, and have input into, the design of a development proposal. The goal of zoning reform in Philadelphia is to significantly reduce the need for zoning variances. However carefully a zoning code is drafted, it is necessarily generalized and cannot address the specific nuances of every site. It is likely that the new Philadelphia Zoning Code, currently under development, will incorporate provisions for design review.

Many cities such as Seattle, San Francisco and Boston have design review procedures in place, which provide the opportunity for community input into the design of a project. Seattle has one of the most sophisticated of these design review process. In all downtown mixed-use zones, development of over 20 residential units, or 20,000 square feet of non-residential use, are subject to mandatory design review, against established specific design guidelines

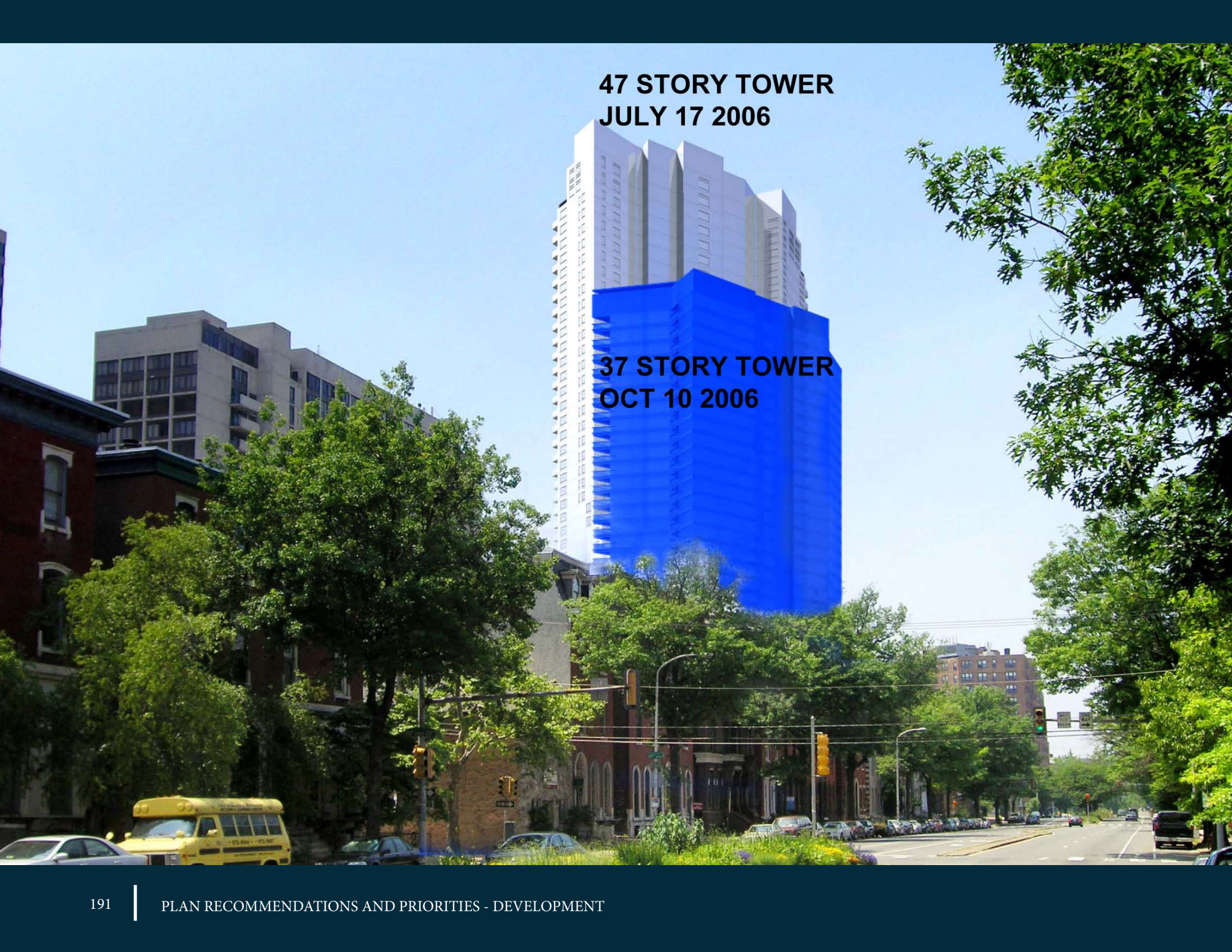
*Image 90. Vancouver’s balconies and roof terraces as open space.*











**47 STORY TOWER  
JULY 17 2006**

**37 STORY TOWER  
OCT 10 2006**



to provide predictability and consistency. The design guidelines address site planning, building massing, architectural elements and materials, public amenities, vehicle access and parking, and streetscape and landscaping. The goal of the design review procedure is to encourage better design that enhances the character of the city, ensure development sensitivity fits in the neighborhood, and provide some flexibility in application of development standards. In Philadelphia, the final requirements for a city-wide design review will be established by the City, but likely include a public hearing process that will include an opportunity for community input.

A design review procedure should be established for all major developments in the Logan Square Neighborhood - Parkway that include over 20 residential units, or 20,000 square feet of non-residential space. The applicant should be required to submit an analysis of the sunlight/shadow, daylight, and impacts on views, of the proposed development in addition to standard presentation materials. The design review procedure can provide an opportunity to grant departures from certain land use/zoning code standards, such as structure width and depth,

setbacks, parking, open space, and lot coverage, depending on the specific circumstances of the site, and community benefit of the proposed development.

To prepare for community testimony at design review hearings, LSNA should evaluate each property against the design guidelines standards in Section 5.3.6. Separate guidelines have been established for each character quadrant of the neighborhood, as follows:

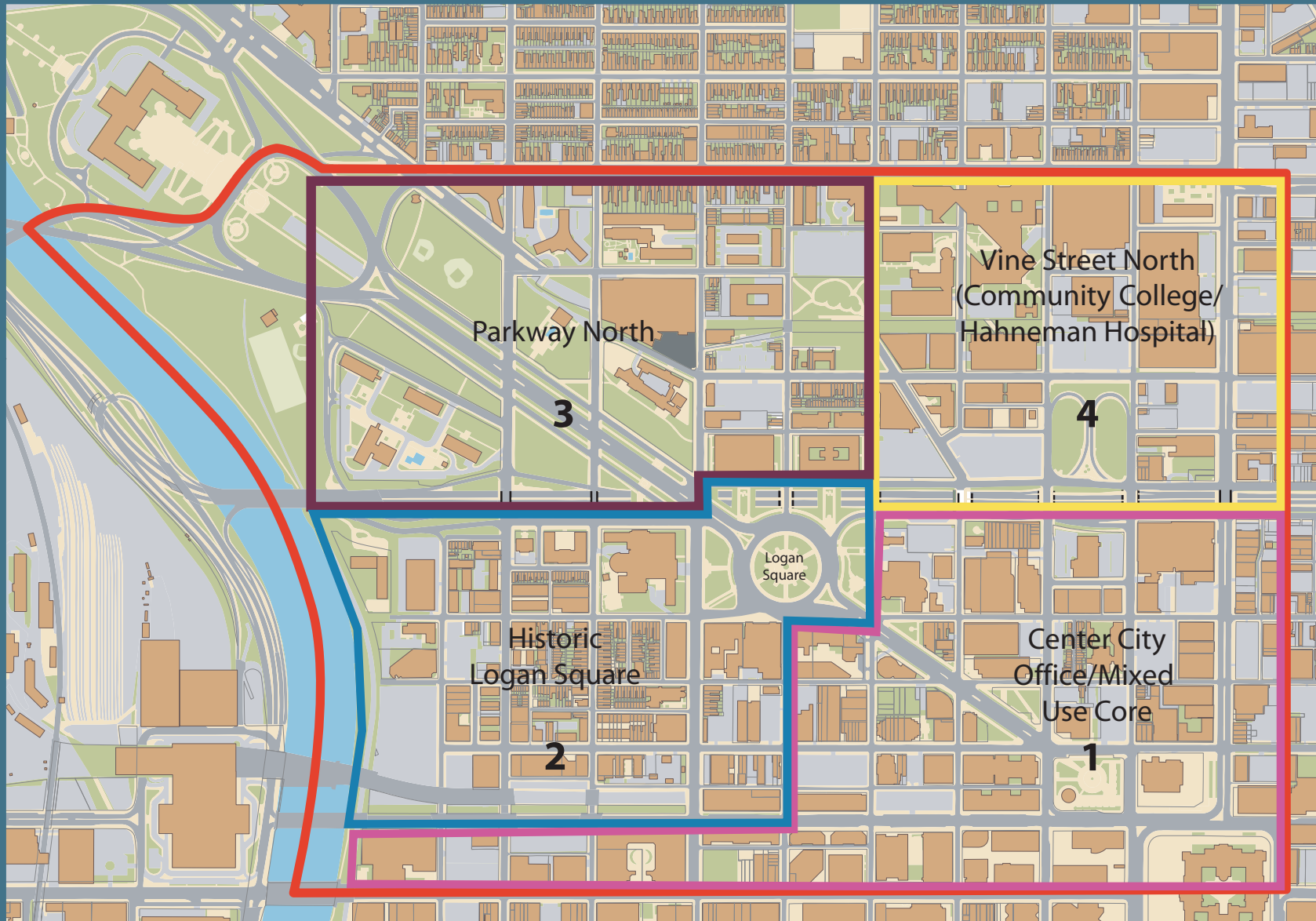
1. Center City Office Core
2. Historic Logan Square
3. Parkway North
4. Vine Street North Community College/Hahnemann Hospital

***The character quadrants are shown on Map 26.***

◀ ***Image 91. Reduction of building height as result of community meetings of proposed Parkway 22 Tower***



# CHARACTER QUADRANTS



### 5.3.6 Character Quadrant Design Guidelines

#### *Zone 1 – Center City Office/Mixed Use Core*

Zone 1 includes much of Philadelphia’s Center City Office/Mixed Use core, centered around Suburban Station and West Market Street. It includes parcels south of the SEPTA tracks west of 20th Street, and parcels east of 19th Street and south of Vine. It also includes the air rights parcels over the SEPTA tracks, although the air rights parcels west of 21st Street present significant constraints to development. Nineteenth Street is proposed as a line of demarcation between the Center City Office/Mixed Use Core and Logan Square residential neighborhood, creating a buffer of the 1900 block to transition from the large-scale office building to the low-scale historic core. However, 20th Street had also been suggested as the boundary since it is a wider street, and the edge of the square, but residents overwhelmingly preferred 19th Street as the boundary line. Over the years, the center of gravity of the office/mixed use core has moved northwest from Market Street. The

area is predominantly commercial, but with some new high-rise residential development along Market Street, such as the Murano. Many of the buildings are large footprint high-rise office buildings. The area has a high density of pedestrian traffic, mostly office workers, particularly around weekday lunch time. The zone includes some of the highest density high-rise buildings in the city. There is limited open space. Design guidelines are targeted to reinforcing the positive characteristics of the district, while encouraging more 24-hour activity. Major development opportunity sites include parcels on Market and Arch Streets, and the Archdiocese parcels adjacent the Cathedral between 17th and 18th Streets, fronting Logan Square.

#### A. Building Design Guidelines

- Tall buildings may be appropriate in this zone and should be designed to enhance the skyline, and contribute to the creating of a unique identity. Upper portions of tall buildings should promote visual interest.
- Locations of towers, tower configuration,



and spacing of towers should be adequate to preserve sunlight and daylight penetration. For the C5 zones south of Kennedy Boulevard and east of 19th Street bulk controls should be similar to those already in place for the south sides of Kennedy Boulevard and Market Street. A maximum podium height of 80-100 feet would be appropriate. Maximum tower dimensions should be adequate to permit a typical office building footprint of 25,000 sq.ft., with spacing between towers sufficient to assure daylight penetrations, and allow for views. A maximum building diagonal dimension of 250 feet, and minimum spacing between buildings of 75 feet would be appropriate. A threshold maximum height limit of 400-450' (30-40 floors) is proposed, with the approval for higher buildings at appropriate locations subject to appropriate public review. For air rights parcels over the SEPTA tracks, threshold maximum height limits should not exceed 300', to help create a transition to lower rise development to the north of Arch Street, and reflect the difference in height between JFK Boulevard and the SEPTA tracks, and the cross streets below.

- The base of tower buildings should be designed at the scale of the pedestrian, with materials and detailing that enhance the pedestrian experience.
- Mixed-use buildings and developments are encouraged.
- New structures should provide a transition in form and scale of adjacent existing buildings which are likely to remain.
- New structures on parcels abutting Zone 2 should carefully address the transition in scale and land use.

*Image 92. Comcast Center* 





## B. Street Level Amenities

- Because of the high level of daytime pedestrian traffic, sidewalks should be sufficiently wide to provide the opportunity for sidewalk cafes, seating areas, etc. Buildings on north-south streets with only a 12' sidewalk right-of-way should be setback from the property line a minimum of 5' to create a sufficiently wide sidewalk for sidewalk cafes and other amenities. Buildings should also be setback on other blocks with a narrow sidewalk right-of-way, such as the north side of JFK Boulevard between 20th and 23rd Streets. Here, a 20' sidewalk should be provided, consistent with the overall width of JFK Boulevard. A consistent build-to-line should be provided to create a consistent "street wall" at podium level.
- Provide usable public-access open spaces with public amenities such as retail uses or cafes at appropriate locations.
- Avoid large expanses of blank or opaque facades at all levels. Maintain a minimum 75% transparency at first floor level, and a minimum 30% transparency at upper levels. Articulate blank areas of wall with building materials, art work, landscaping, etc.
- Visually reinforce building entrances.
- Minimize impact of loading docks and parking access. Limit parking access to two (2) openings maximum 11' wide, and one 15' loading access.
- Locate loading and turn around underground with access from service streets or alleys, where feasible, to reduce street level impacts.
- Provide pedestrian amenities, such as public restrooms, seating and building ledges that encourage pedestrian interaction.
- The design of buildings fronting JFK Boulevard should actively address the enhancement of the 21st, 22nd, and 23rd Streets underpasses at JFK Boulevard and the SEPTA tracks, including establishing integral public access connections between JFK Boulevard and the cross streets below, and the street frontage at the cross street level.
- Incorporate street-level pedestrian spaces such as plazas or parcels parks with building setbacks, where appropriate, to support cafes, play areas, etc.



## *Zone 2 – Historic Logan Square*

The Historic Logan Square Zone includes Logan Square itself, the Franklin Institute, Moore College of Art and Academy of Natural Sciences, and the historic rowhouse neighborhood between Race and Arch, 20th and 22nd Streets. It also includes the transition areas of the 1900-2200 blocks of Arch Street, and the 23rd Street corridor. Residents are particularly concerned about the potential westward expansion of the Center City office core uses and the potential for high, bulky buildings along Arch Street, west of 19th Street, where they could have adverse impacts on residential townhouse blocks, as well as on views north from existing high-rise residential buildings, such as Kennedy House. Pedestrian activity is typically by local residents and Parkway museum visitors. Active open space is largely utilized by residents, except the Schuylkill River Trail.

### A. Building Design Guidelines

- New buildings within or adjacent to the historic rowhouse core should respond to the physical

context of the adjacent site, in terms of building massing, scale, fenestration, and materials.

- Large-footprint commercial office-core-type high-rise buildings should be prohibited in this zone.
- Development outside the historic core should consist of a mix of townhouses, mid-rise apartments with first floor commercial, and strategically located high-rise small-footprint residential towers and semi-public open space. Overall development density should be in the order of 100-150 units per acre.
- Density within the historic core should not exceed the existing R10 zoning density of approximately 60 units per acre.
- Developments on sites adjacent to Zone 1 should create a transition in bulk of scale,



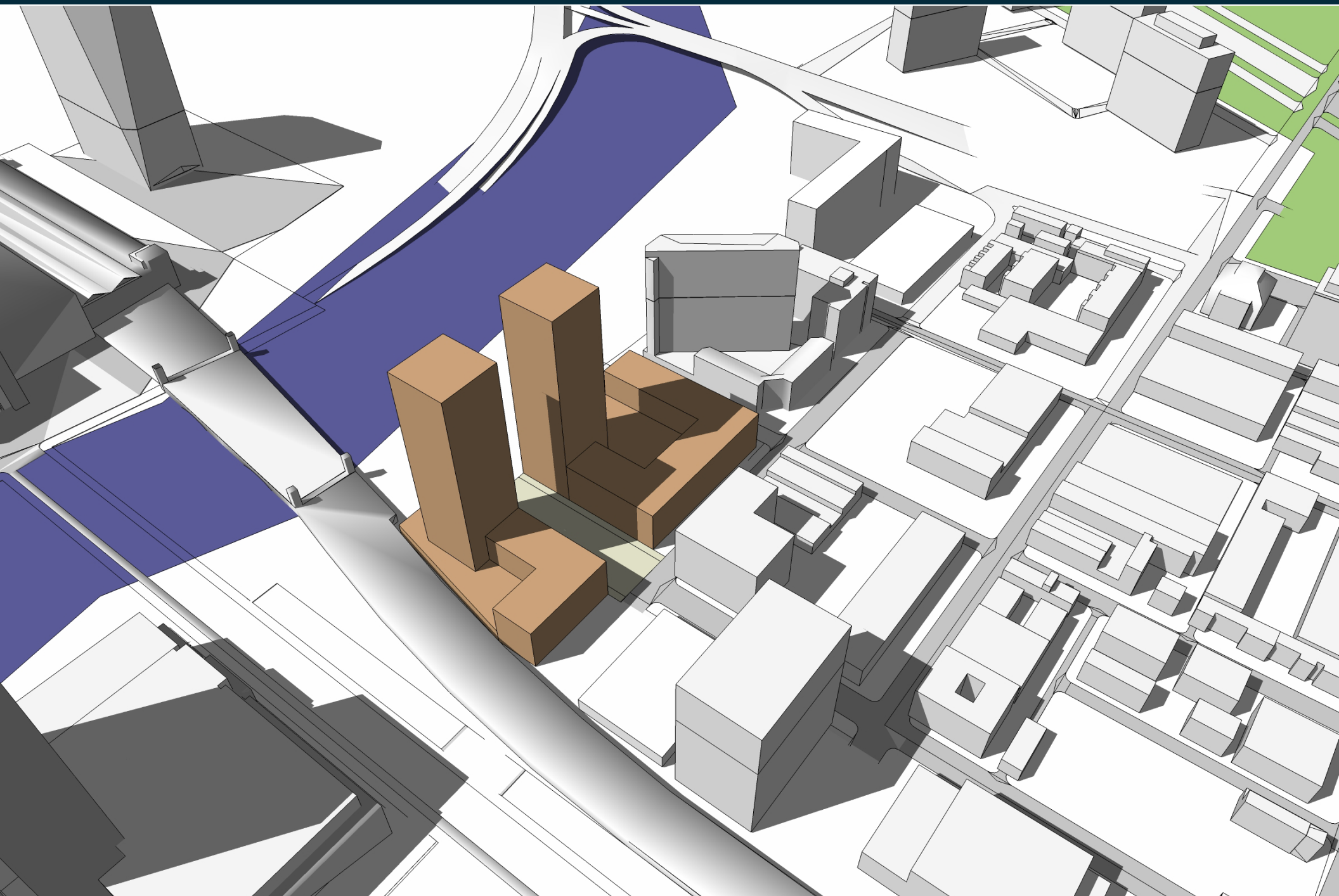
between the high-rise core and townhouse neighborhood, by incorporating elements of each.

- Additional historic resources within the area should be identified, listed on the Philadelphia Register of Historic Places, and integrated into new development proposals.
- The residential townhouse scale of the neighborhood should be reinforced with design elements, such as individual unit entrances, bays, patios, stoops, and balconies that respond to the 16-20' townhouse module, to create a pedestrian-friendly streetscape.
- Arch Street view corridor and potential future access corridor to the Schuylkill River, west of 23rd Street should be maintained in the development of 23rd Street frontage.
- The 23rd Street edge of development of 23rd Street parcels should respond to the townhouse residential scale of the historic townhouse core.
- Small footprint, wide spaced high-rise towers would be appropriate for waterfront sites west of 23rd Street, in conjunction with a low-rise residential/mixed-use podium, 65 to 70' in height. The first floor would be retail, or residential on a raised first floor, above floodplain level.
- Parcels rezoned to RC4 on the south side of Arch Street could appropriately be developed with mid-rise street wall buildings of 8 stories (100') that would not overwhelm this wide street, and would also minimize shadow impacts on low-rise residential blocks to the north, and maximize views. Residents of high-rise Kennedy House have requested that buildings on the south side of the 1900 block of Arch Street be limited to 5 stories to protect

*Image 93. Mid-rise Development in Washington, DC that could be appropriate on Arch Street, west of 19th Street*









their uninterrupted views to the north. A possible education campus has been proposed for this block. However, the allowable FAR for this block will likely result in a higher building that would have some impact on views.

- For the transition RC4 area west of 23rd Street adjacent the historic core of Logan Square, no bulk controls are proposed for buildings up to 65 feet in height. For buildings between 65' and 125' in height, the proposed maximum diagonal dimension of a building is 250', with a minimum building spacing of 60'. For buildings between 125' and 300' in height, the proposed maximum diagonal dimension of a building is 120', with a minimum building spacing of 100', to limit high-rise development to small footprint towers.
- Within the historic townhouse core, materials, pattern of solids and voids in the wall plane, and building scale should be consistent with the historic fabric.

B. Street Level Amenities

- Development should create a stimulating pedestrian environment along public streets

through multiple building entrances, for both residential and commercial uses. Residential buildings, including apartment buildings, should be designed with first floor units with direct street access.

- Buildings should be enhanced through landscaping, including planted strips between stoops.
- Developments should include pedestrian-oriented streetscape elements, such as pedestrian-scaled streetlights, porch lights at individual entrances, pedestrian-scaled signage for retail/commercial uses.
- Large developments should incorporate street-level pedestrian spaces such as plazas, or mini parks within building setbacks, to support outdoor seating areas, etc.
- Minimize impact of loading docks and parking access. Limit parking access to two (2) openings maximum 11' wide, and one 15' loading access.

◀ *Figure 23. Possible Development Scenario for 2300 Arch Street with 300' small footprint residential towers above a 65' high podium.*

### *Zone 3 – Parkway North*

Parkway North includes many high-rise residential apartment buildings, such as Park Towne Place, City View Condominiums and Museum Towers, as well as the historic rowhouses along Spring Garden Street. Residential densities of new development are typically in the order of 100 units per acre. Parkway North also includes the Benjamin Franklin Parkway and associated open spaces, as well as the Rodin Museum, the Free Library, Family Court Building, and future Barnes Museum. The area is predominantly residential except for Callowhill Street and the 2000 block of Hamilton Street. Many of the existing high-rise developments are setback far from the street line, and do not create a pedestrian-engaging environment. Residents of City View Condominiums are very concerned about protecting their uninterrupted views of the Center City skyline to the south, and have expressed preference for a mid-rise street wall building of 7-8 stories, as compared to a high-rise development on the abutting Whole Foods site.

The Children's Crisis Center on the 1800 block of Callowhill Street should be relocated over the long term, to open up the entire north side of the 1800 block of Callowhill Street for mixed-use development with first floor neighborhood retail.

The area has a relatively light density of pedestrian traffic, except for Parkway events. Design guidelines are targeted to reinforce the positive characteristics of the district, including its openness, while encouraging development that is built to the street line, is more pedestrian-friendly, and encourages more active use of existing open space.

#### A. Building Design Guidelines

- Development can accommodate a variety of forms, within the zoning density of FAR 5, from 7-8 story street wall apartment/mixed-use buildings, to developments that include both 3-4 story townhouses, and a limited number of carefully sited mid- to high-rise small footprint apartment buildings, to mid-to-high-rise small footprint apartment buildings

over a residential/mixed-use podium around public and semi-public protected courtyards, located for sun access. Small footprint high-rise buildings in conjunction with family-oriented townhouses may be most appropriate for parcels abutting Franklinton Park, to the north and south, while high density 7-8 story street wall buildings on the Whole Foods site may more appropriately address abutting residents concerns about protecting views. Overall density should be in the order of 100 units per acre.

- Small footprint, tall buildings should be designed to enhance the skyline, particularly when viewed from the Art Museum steps. Carefully sited tall buildings should define the northern edge of the Parkway, while minimizing any impacts on historic Spring Garden Street, and the Parkway museum buildings.
- Existing high-rise residential apartment buildings have typically been isolated structures

in a park-like setting. New buildings should be oriented to the street and contribute to the streetscape.

- Buildings should be sited to minimize impacts on views from, and sunlight, and daylight access on existing residential buildings.
- Podium buildings should be designed to respond to the traditional rowhouse scale, with individual unit entrances, small landscaped front yards, etc. The Tivoli Condominiums are a good example of this treatment.
- Common open space should be provided in the form of roof decks, terraces, and open space above parking.
- Buildings fronting the 2000 block of Hamilton Street, and 1800 and 1900 blocks of Callowhill Street, should include first floor retail uses.



- Low/mid-rise residential developments, with individual townhouse entrances, with carefully located small footprint high-rise towers, should front Franklin Town Park, to encourage more active use of this underutilized recreation resource.
- North-south streets at the Parkway crossings should be developed with freestanding small scale, single-story restaurant/café commercial uses as pavilions in a park setting, to encourage pedestrian activity, particularly in evening hours.
- High-rise/mid-rise residential buildings should have a residential character, with bays, balconies, setbacks and other features to create a residential scale.
- Developments should include supporting community amenities such as schools, and day care centers.

#### B. Street Level Amenities

- Buildings should be setback from the street right-of-way a small distance (5-15'), to create an opportunity for landscaping, patios, etc. and thus contribute to the open, green, park-like setting of this area.
- Impact of parking and loading access to apartment buildings should be minimized. Openings for parking access should be limited to one 22' wide opening, or two 11' wide openings for each development. Openings for loading access should be limited to one 15' wide opening, and preferable integrated with parking access.
- Developments such as the Tivoli incorporate modest landscaped setbacks that help

*Image 94. Mixed-Use Development in Seattle, WA that could be appropriate in Parkway North*









create an attractive residential scale. Small, approximately 5-foot deep, building setbacks at sidewalks should include landscaping treatments to reinforce the “green” character of the area. Landscaped areas should be protected by low railings.

- First floors of all developments should include pedestrian-oriented treatments, such as stoops, individual unit entries, and windows, to create an enjoyable pedestrian experience.



▲ *Image 95b. Daycare center in Vancouver, BC*

◀ *Image 95a. Townhouse and Tower Development in Vancouver, BC*



#### *Zone 4 – Vine Street North (Community College/ Hahnemann Hospital)*

Zone 4 north of Vine and east of 18th Street includes the large institutional developments of Community College and Hahnemann University. It also includes the Philadelphia School District offices in the former Philadelphia Inquirer Building and the office development of the former Smith Kline Building. There are a large number of structured parking facilities and surface parking lots. The zone includes large blocks, large footprint buildings with large areas of blank walls and few pedestrian amenities. The streetscape is generally bleak and traffic is heavy. The depressed Vine Expressway and 16th and Callowhill interchange ramps create a barrier to pedestrian activity between the north and south of Vine Street.

##### A. Building Design Guidelines

- Franklin Town parcels fronting Vine Street should be developed with high-rise, high-intensity mixed-use buildings to begin to draw pedestrian activity north of Vine Street and create a visual link to the high-intensity office core of Zone 1.

- The Vine Street Expressway air-rights parcels between 18th and Broad Streets should be developed for high-rise, high-intensity mixed-use buildings to further enhance the linkages north and south of the expressway.
- The air-rights parcels available at the Vine Street interchange should be developed for high-rise, high-density mixed-use.
- Encourage development of student housing for Pennsylvania Academy of Fine Arts students, possibly as a joint development with Drexel Hahnemann Hospital.
- New development should establish a “street wall” along both Vine Street and Franklin Town Boulevard, without creating the “canyon” effect of the Watermark high-rise.
- Expansion of Community College at appropriate locations should be encouraged. Potential sites would include the 400 block

of N. 18th Street fronting Franklin Town Park, and Vine Expressway air-rights parcels. Such a development on the 18th Street parcel could create an attractive “front door” for the campus, and screen the unsightly college parking structure fronting 18th Street.

#### B. Pedestrian Amenities

- Improvement of the streetscape is a key concern. Large areas of blank wall at ground level should be avoided. Any areas of blank wall should be articulated with differing building materials, artwork, landscaping, trellises, etc.
- Sidewalks on Vine Street should be a minimum 16' wide, consistent with those at the Free Library and Family Court. Sidewalks on Franklin Town Boulevard should be 20' wide to match the width of existing sidewalks north of Wood Street.
- All above-ground parking structures must be wrapped with active uses to create a more

inviting pedestrian environment.

- Encourage first floor retail/restaurants and other service uses to serve institutional employees and students.
- Minimize the impact of loading docks and parking access to 2 openings, maximum 11' wide, or one 22' wide opening. Provide one maximum 15' wide loading access.
- The area lacks landscaping and trees. An aggressive program of street tree planting should be initiated throughout the area.
- Locate loading underground, or entirely enclosed with the building, to reduce street level impacts.
- Support the narrowing of the cartway and widening of the sidewalks on 1400 block of Cherry Street to create pedestrian-oriented street and opportunities for a sidewalk restaurant/café.

### 5.3.7 Possible Development Scenarios

The following scenarios were developed to illustrate how certain key parcels could be developed in conformance with the proposed zoning overlays, and design guidelines. The scenarios are not meant to provide a prescriptive design for the sites, but only to provide an illustrative example.

#### *23rd and Arch Parcels*

- Maximum FAR: 5.0
- Maintain Arch Street public access/view corridor to Schuylkill River, minimum 72' wide.
- Maximum Building Podium Height: 65'.
- Maximum Tower Height Above 65': 300'.
- Maximum Building Dimension, 65'-125' high: 250' diagonal.
- Maximum Tower Dimensions, 125'-300': 120' diagonal.
- Minimum building spacing, 65'-125' high: 60'.

- Minimum Tower spacing 125'-300' high: 100'.
- Residential units with raised stoops and individual unit street front access, or retail fronting 23rd Street
- Residential units with raised stoops and individual unit street front access, or retail fronting Arch and Cherry Street.

#### *20th and Hamilton Parcels*

- Maximum FAR: 5.0.
- Maximum Residential Density: 130 units acre.
- Through building public access at railroad right-of-way.
- Retail uses at first floor fronting Hamilton Street.
- Retail uses at first floor on Pennsylvania Avenue.
- Residential uses with individual unit street front access fronting 20th Street.
- Alternate A: Street wall buildings, maximum



height at street frontage of 7-8 stories (90') fronting Pennsylvania Avenue and 20th Street (preferred by adjacent residents).

- Alternate B: Combination of low-rise 4-story buildings (maximum height of 50') fronting Pennsylvania Avenue and 20th Street. Small footprint high-rise residential apartment building at corner of 20th and Pennsylvania Avenue. (Maximum height, 300', maximum tower dimensions, 100' diagonal).

- Minimum Building Spacing, 50'-125' high: 100'.
- Minimum Tower Spacing: 100'.
- Maintain 1800 block of Hamilton Street as public access street between Franklin Town Park and the development.
- Residential units with individual street front access, fronting Hamilton and 19th Street.

#### *18th and Hamilton Streets*

- Maximum FAR: 5.0.
- Maximum Residential Density: 130 units acre.
- Maximum Podium Building Height: 50' (4 stories).
- Maximum Tower Height Above 50': 300'.
- Maximum Building Dimension, 50'-125' high: 250' diagonal
- Maximum Tower Dimensions, 125'-300' high: 120' diagonal



▲ *Image 96. Mixed Use Development in Vancouver, BC*





# 6

## PRIORITIES AND NEXT STEPS



## 6.1 Priorities

Implementation of the plan will require time, coordination with a variety of private and public entities, and funding from a variety of sources. Because of this, it is important to consider a prioritization of the recommendations, in order to ensure that the most important recommendations are enacted.

At the final public meeting for the Logan Square Neighborhood - Parkway Plan, local stakeholders and residents were asked to prioritize the recommendations presented as part of the final plan. In general, the priorities selected by these citizens focused on creating a lively, safe, and engaging public realm for residents and visitors alike.

### 6.1.1 Quality of Life

The highest priority issue within the Quality of Life section is addressing the chronic street homeless population. The recommendations are listed in full in Section 5.1.3 above, but immediate priorities are to remove all homeless encampments from the neighborhood; cease outdoor food and other distributions to the homeless on the Parkway; and adequately enforce all laws and regulations in the neighborhood, especially along the Parkway and in all parks and plazas. (Residents are concerned, however, that encampments displaced from the Parkway not be permitted to simply relocate into the more residential areas of the neighborhood.) Any and all policies that contribute to the disproportionately large chronic street homeless population in this and adjacent downtown areas should be critically reevaluated and overhauled. These action items will not only enhance the feeling of safety within the neighborhood, but help to preserve the vitality of this downtown hub for culture, tourism, and business.

Other high priorities within Quality of Life include increasing the number of small cafes and providing additional pedestrian enhancements, in the form of benches and upgraded sidewalks along the Parkway. Both of these items will serve residents and visitors. Similarly, the Schuylkill River Trail will benefit from restrooms/water fountains for the active users of the area. The potential dock should also be explored as a priority for the trail.

In terms of streetscape priorities, stakeholders felt that improvements should be made to the JFK/ SEPTA track underpasses at 21<sup>st</sup>, 22<sup>nd</sup> and 23<sup>rd</sup> Streets, and that new or additional pedestrian lighting throughout the neighborhood is also important; this simple change can make the area a more attractive evening destination.

Additional pocket parks (similar to the Coxe Park model) should be installed in the Logan Square area as an amenity for families; stakeholders also prioritized requiring passive open space development from developers building within the neighborhood. Sustainable development practices (such as LEED certification) should be encouraged throughout the neighborhood, and additional tree canopy should

be added. Additional trees will not only provide additional shading to streets, but also help the carbon footprint of the community. The goal is to provide 500 trees within 3 years.

Also in terms of development, a review process should be established for new projects within the neighborhood's boundary; the team should include developers, public agencies, and residents. An LSNA member should also be appointed to the Parkway Foundation Board, to increase communication between organizations and ensure decision making consistency.

### 6.1.2 Transportation

The highest priority identified within circulation issues was to improve the intersections at 20th and Parkway/15th and Vine, and Broad and Vine. These are crucial to improving pedestrian and vehicular mobility through the neighborhood.

New parking facility north of Vine between 18th and 21st Streets to serve Parkway institutions should be established, while making temporary visitor permits available in other garages. Business parking issues

were not identified as a high priority by residents of the neighborhood, but new underground parking at Barnes Museum or Free Library should be established while those buildings are undergoing their planned construction and renovations. An easy parking fix would be to allow permit-parking holders to bypass roadblocks.

The top priority for pedestrian enhancements is creating 4-way stop at 22nd and Cherry Street; similarly, 22<sup>nd</sup> Street was targeted for bicycle enhancements, including the creation of a dedicated bike lane.

Coordination with SEPTA will be necessary, but a priority should be to target a 10 minute service target for peak times, and 15 minutes for off-peak. This will encourage public transit use, and possibly reduce auto traffic through the neighborhood. Another transit improvement that could help encourage transit ridership is to create a more welcoming transit gateway at the City Hall Station/Dilworth Plaza area. Bus information should be made available at all bus stops.

### 6.1.3 Development

The highest priority recommendations overall fall within the development section. These recommendation suggestions are focused on maintaining the existing character and feel of the neighborhood, while allowing for future development. First, the zoning classification should be changed from C4 (Commercial) zoning to RC4 (Residential/Commercial) on Arch Street west of 19th Street. In general terms, the excessive additional FAR provisions in C4 and RC4 zoning classifications should be reduced. Provisions should be put in place to provide building bulk controls (height, spacing, and footprint dimensions) to assure daylight, sunlight penetration and protect views.



## 6.2 Implementation Matrix


The following tables summarize the top priority recommendations separated into three separate timelines; short term for within zero to two years, mid-term for within two to five years, and long-term for projects taking five or more years to implement. Based on feedback from the public workshop conducted in October, and the plan's Advisory Committee. The full results of the public workshop are included in the Appendix.

The Implementation Matrix takes the top 20 short term, 10 mid-term, and 5 long-term recommendations and provides guidance on responsible parties and possible funding sources. The list is not comprehensive and is intended to be a guide to the Logan Square Neighborhood - Parkway.

LSNA will continue to work with the Planning Commission to implement recommendations where there are currently differences of opinion between the Planning Commission and LSNA.

## Short-Term (0-2 Years) Recommendations

|     |   | Responsible Parties | Potential Funding Sources |
|-----|---|---------------------|---------------------------|
| 1.  | Reduce excessive additional FAR provisions in C4 and RC4 zoning classifications. <i>(DEV)</i>   | A                   | n/a                       |
| 2.  | Remove all homeless encampments, and enforce all laws and regulations. <i>(QoL)</i>   | A, C, D, E, F, H, I | A, E, G, I, J, K          |
| 3.  | Establish design review procedure, evaluated against design guidelines that incorporate community input, for all major developments. <i>(DEV)</i>   | A, D                | n/a                       |
| 4.  | Change C4 (Commercial) zoning to RC4 (Residential/Commercial) on Arch Street west of 19th Street. <i>(DEV)</i>                                      | A                   | n/a                       |
| 5.  | Install pedestrian lighting throughout neighborhood. <i>(QoL)</i>   | A, C, D, E          | A, D, E, F, I, J, K       |
| 6.  | Provide building bulk controls (height, spacing, and footprint dimensions) to assure daylight, sunlight penetration and protect views. <i>(DEV)</i> | A                   | n/a                       |
| 7.  | Cease outdoor food distribution. <i>(QoL)</i>   | A, D, E, H, I       | A, E, I, J, K             |
| 8.  | Add LSNA member to Parkway Foundation Board. <i>(QoL)</i>   | D, G, H             | n/a                       |
| 9.  | Establish review process for projects that includes developers, public agencies, and residents. <i>(QoL)</i>  | A, D                | n/a                       |
| 10. | 4-way stop at 22nd and Cherry Street. <i>(TSP)</i>  | A, B                | A, B                      |

 Recommendations where the Planning Commission has a different opinion from LSNA.

### Implementation Key

A = City of Philadelphia  
B = PennDOT  
C = Private Developers  
D = LSNA  
E = Center City District  
F = Private Land Owners

G = Institutions  
H = Parkway Council  
I = Non-Profits  
J = State of Pennsylvania  
K = Federal Government  
L = SEPTA

## Short-Term (0-2 Years) Recommendations

|     |   | Responsible Parties | Potential Funding Sources |
|-----|---|---------------------|---------------------------|
| 11. | Improve JFK/SEPTA track underpasses at 21st, 22nd and 23rd Streets. <i>(QoL)</i>  | A, B                | A, I, J, K,               |
| 12. | Modify existing 125' height limit to permit taller, small footprint buildings, in appropriate locations, to protect views and light. <i>(DEV)</i> | A                   | n/a                       |
| 13. | Make temporary visitor parking permits available. <i>(TSP)</i>  | A, D, J             | A, J                      |
| 14. | Additional cafés along Parkway. <i>(QoL)</i>  | A, C                | C                         |
| 15. | New sidewalks and benches on Parkway. <i>(QoL)</i>  | A, B, D, E, H       | A, B, E, I, J, K          |
| 16. | Off-peak service target of 15 minutes. <i>(TSP)</i>   | L                   | L                         |
| 17. | Peak service target of 10 minutes on bus routes. <i>(TSP)</i>   | L                   | J, K, L                   |
| 18. | Prohibit aboveground parking garages for accessory parking unless they are wrapped in active uses. <i>(DEV)</i>                                   | A                   | n/a                       |
| 19. | Advocate for use of City Branch Right-of-Way as high frequency transit corridor. <i>(TSP)</i>   | A, D, H, L          | J, K, L                   |
| 20. | Require car share and bicycle facilities. <i>(QoL)</i>  | A, C, D, H          | C, I                      |

### Implementation Key

A = City of Philadelphia  
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 J = State of Pennsylvania  
 K = Federal Government  
 L = SEPTA



## Mid-Term (2-5 Years) Recommendations

|     |  | Responsible Parties | Potential Funding Sources |
|-----|--|---------------------|---------------------------|
| 1.  | New underground parking garage at Barnes Museum or Free Library. <i>(TSP)</i>                                | C, G                | C, G                      |
| 2.  | Limit events to signature events (Thanksgiving Park, July 4th, etc.) <i>(QoL)</i>                            | A, G, H, I          | n/a                       |
| 3.  | Increase tree canopy (500 trees in 3 years) <i>(QoL)</i>   | A, C, D, F, G, I    | A, C, D, F, G, I, J       |
| 4.  | New parking facility north of Vine between 18th and 21st Streets to serve Parkway institutions. <i>(TSP)</i> | C, G                | C, G                      |
| 5.  | More pocket parks (like Coxe Park) <i>(QoL)</i>  | A, D, I             | A, D                      |
| 6.  | Bus status information system at all bus stop signs. <i>(TSP)</i>  | E, L                | J, K, L                   |
| 7.  | Make City Hall Station/Dilworth Plaza a welcoming transit gateway. <i>(TSP)</i>                              | A, B, E, J, K, L    | A, J, K, L                |
| 8.  | Minimize noise impacts of building mechanical equipment. <i>(QoL)</i>  | A, C, F, G          | F, G                      |
| 9.  | Add ramp/stair connections to/from JFK at 21st, 22nd, and 23rd Streets. <i>(TSP)</i>                         | A, B                | A, B, J, K                |
| 10. | Consider adoption of Neighborhood Conservation District, (20th/23rd, Winter and Arch) <i>(DEV)</i>           | A, D                | n/a                       |

### Implementation Key

A = City of Philadelphia  
 B = PennDOT  
 C = Private Developers  
 D = LSNA  
 E = Center City District  
 F = Private Land Owners

G = Institutions  
 H = Parkway Council  
 I = Non-Profits  
 J = State of Pennsylvania  
 K = Federal Government  
 L = SEPTA

## Long-Term (5+ Years) Recommendations

|    |  | Responsible Parties | Potential Funding Sources |
|----|--|---------------------|---------------------------|
| 1. | Improve intersections at 20th and Parkway/15th and Vine, and Broad and Vine. <i>(TSP)</i>        | A, B                | A, B, J, K                |
| 2. | Eliminate unneeded outer lanes on Parkway, to reduce pedestrian crossing distances. <i>(TSP)</i> | A, B                | A, B, J, K                |
| 3. | Infill openings over Vine Expressway at Free Library. <i>(QoL)</i>                               | A, B                | A, B, J, K                |
| 4. | Landscaped/widened replacement bridge crossings over the Vine Expressway. <i>(QoL)</i>           | A, B                | A, B, J, K                |
| 5. | Redesign Eakins Oval. <i>(TSP)</i>   | A, B,               | A, B, G, J, K             |

### Implementation Key

A = City of Philadelphia  
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## APPENDIX



# A.1 Resident Survey

## LSN-PP Community Survey

| 1. What age are you?   |  |                  |                |
|------------------------|--|------------------|----------------|
|                        |  | Response Percent | Response Count |
| Less than 20 years old |  | 1.7%             | 8              |
| 21 – 40 years old      |  | 34.9%            | 161            |
| 41 – 60 years old      |  | 29.7%            | 137            |
| More than 60 years old |  | 33.6%            | 155            |
| answered question      |  |                  | 461            |
| skipped question       |  |                  | 7              |

| 2. Are you a resident of the Logan Square neighborhood? |  |                  |                |
|---|--|------------------|----------------|
|   |  | Response Percent | Response Count |
| Yes   |  | 82.1%            | 384            |
| No  |  | 17.9%            | 84             |
| answered question                                       |  |                  | 468            |
| skipped question  |  |                  | 0              |

| 3. In what quadrant of the Logan Square neighborhood do you live?      |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Northwest, between Spring Garden, the Parkway, 19th and the Schuylkill |  | 15.9%            | 60             |
| Northeast, between Spring Garden, the Parkway, 19th and Broad          |  | 7.9%             | 30             |
| Southwest, between the Parkway, Market, 19th and the Schuylkill        |  | 69.3%            | 262            |
| Southeast, between the Parkway, Market, 19th and Broad                 |  | 7.1%             | 27             |
| answered question  |  |                  | 378            |
| skipped question   |  |                  | 90             |






| 4. In what type of building do you live?                                   |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Rowhouse/townhouse   |  | 35.1%            | 128            |
| Conversion of office or other building                                     |  | 7.1%             | 26             |
| Purpose-built condominium/co-op/apartment                                  |  | 58.1%            | 212            |
| If you live in a multi-family building, what is the building name/address? |  |                  | 154            |
| answered question  |  |                  | 365            |
| skipped question   |  |                  | 103            |



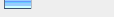
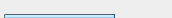
| 5. Are you an owner or a renter in Logan Square neighborhood? |  |                  |                |
|---|--|------------------|----------------|
|   |  | Response Percent | Response Count |
| Owner   |  | 88.0%            | 330            |
| Renter  |  | 12.3%            | 46             |
| answered question   |  |                  | 375            |
| skipped question  |  |                  | 93             |


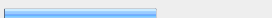
| 6. How long have you lived here? |  |                  |                |
|----------------------------------|--|------------------|----------------|
|                                  |  | Response Percent | Response Count |
| 0-2 years                        |  | 23.9%            | 90             |
| 3-5 years                        |  | 22.0%            | 83             |
| 6-10 years                       |  | 21.5%            | 81             |
| 11+ years                        |  | 32.9%            | 124            |
| answered question                |  |                  | 377            |
| skipped question                 |  |                  | 91             |



| 7. How would you characterize your household?                      |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Household with children  |  | 14.3%            | 54             |
| Single, no children  |  | 38.5%            | 145            |
| Married or living with significant other, no children in household |  | 43.5%            | 164            |
| Other (please specify)   |  | 4.0%             | 15             |
| answered question  |  |                  | 377            |
| skipped question   |  |                  | 91             |

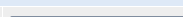

| 8. What is your household size? |  |                  |                |
|---------------------------------|--|------------------|----------------|
|                                 |  | Response Percent | Response Count |
| 1 person                        |  | 35.0%            | 132            |
| 2 people                        |  | 49.3%            | 186            |
| 3 people                        |  | 9.5%             | 36             |
| 4 people                        |  | 5.3%             | 20             |
| 5 or more people                |  | 1.1%             | 4              |
| answered question               |  |                  | 377            |
| skipped question                |  |                  | 91             |

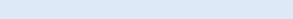

| 8. What is your household size? |   |                  |                |
|---------------------------------|---|------------------|----------------|
|                                 |   | Response Percent | Response Count |
| 1 person                        |  | 35.0%            | 132            |
| 2 people                        |  | 49.3%            | 186            |
| 3 people                        |  | 9.5%             | 36             |
| 4 people                        |  | 5.3%             | 20             |
| 5 or more people                |  | 1.1%             | 4              |
| answered question               |   |                  | 377            |
| skipped question                |   |                  | 91             |


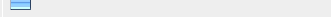
| 10. In what quadrant of the Logan Square neighborhood do you work?    |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Northwest, between Spring Garden, the Parkway, 19th and the Schuykill |  | 10.7%            | 13             |
| Northeast, between Spring Garden, the Parkway, 19th and Broad         |  | 7.4%             | 9              |
| Southwest, between the Parkway, Market, 19th and the Schuykill        |  | 32.2%            | 39             |
| Southeast, between the Parkway, Market, 19th and Broad                |  | 49.6%            | 60             |
| answered question   |   |                  | 121            |
| skipped question  |   |                  | 347            |



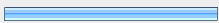

| 11. If you choose to stay in the Logan Square neighborhood after work on occasion, for what reasons do you most often stay? (Check all that apply.) |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| outdoor events, such as parades   |    | 40.0%            | 46             |
| recreation, such as gym, bike riding, etc   |  | 55.7%            | 64             |
| museum exhibits   |  | 67.8%            | 78             |
| library   |  | 44.3%            | 51             |
| dining  |  | 79.1%            | 91             |
| shopping for food   |  | 57.4%            | 66             |
| shopping for other retail items   |  | 40.9%            | 47             |
| answered question   |   |                  | 115            |
| skipped question  |   |                  | 353            |

| 12. Are you aware of the Logan Square Neighborhood Association? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Yes   |  | 90.5%            | 417            |
| No  |  | 9.5%             | 44             |
| answered question   |   |                  | 461            |
| skipped question  |   |                  | 7              |

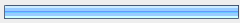
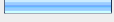
| 13. Are you a member of the Logan Square Neighborhood Association? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Yes  |  | 51.7%            | 209            |
| No   |  | 48.3%            | 195            |
| answered question  |   |                  | 404            |
| skipped question   |   |                  | 64             |

| 14. Did you know that it has monthly meetings, a website, and newsletter? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Yes   |  | 83.3%            | 345            |
| No  |  | 16.7%            | 69             |
| answered question   |   |                  | 414            |
| skipped question  |   |                  | 54             |

| 15. Are you a business owner in the Logan Square neighborhood? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Yes  |  | 5.5%             | 25             |
| No   |  | 94.5%            | 433            |
| answered question  |   |                  | 458            |
| skipped question   |   |                  | 10             |

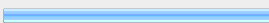
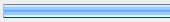
| 16. In what quadrant is your business located?   |                  |                |
|--|------------------|----------------|
|  | Response Percent | Response Count |
| Northwest, between Spring Garden, the Parkway, 19th and the Schuylkill  | 6.3%             | 1              |
| Northeast, between Spring Garden, the Parkway, 19th and Broad           | 6.3%             | 1              |
| Southwest, between the Parkway, JFK Boulevard, 19th and the Schuylkill  | 62.5%            | 10             |
| Southeast, between the Parkway, JFK Boulevard, 19th and Broad           | 25.0%            | 4              |
| answered question  |                  | 16             |
| skipped question   |                  | 452            |

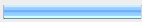
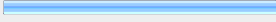
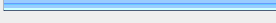

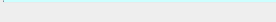

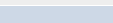
| 17. How long (in years) have you owned a business in the Logan Square neighborhood? |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
|   |                  | 17             |
| answered question   |                  | 17             |
| skipped question  |                  | 451            |

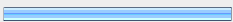
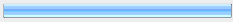

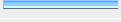
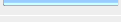
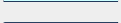
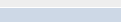
| 18. Is this your only location?   |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
| Yes (Skip to Question 6.)  | 68.8%            | 11             |
| No                        | 31.3%            | 5              |
| answered question   |                  | 16             |
| skipped question  |                  | 452            |

| 19. If No, how many other locations do you have? |                  |                |
|--|------------------|----------------|
|  | Response Percent | Response Count |
|  |                  | 5              |
| answered question                                |                  | 5              |
| skipped question                                 |                  | 463            |

| 20. And where are they located? |                  |                |
|---------------------------------|------------------|----------------|
|                                 | Response Percent | Response Count |
|                                 |                  | 5              |
| answered question               |                  | 5              |
| skipped question                |                  | 463            |

| 21. How many employees do you have?   |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
| Full-time  | 78.6%            | 11             |
| Part-time  | 50.0%            | 7              |
| answered question   |                  | 14             |
| skipped question  |                  | 454            |

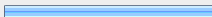
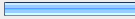
| 22. What are your hours of operation?   |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
| Sunday     | 40.0%            | 4              |
| Monday     | 80.0%            | 8              |
| Tuesday    | 80.0%            | 8              |
| Wednesday  | 80.0%            | 8              |
| Thursday   | 80.0%            | 8              |
| Friday     | 80.0%            | 8              |
| Saturday   | 30.0%            | 3              |
| answered question   |                  | 10             |
| skipped question  |                  | 458            |

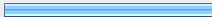
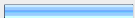
| 23. What are your busiest times of operation? Please specify hours                              |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
| Sunday     | 66.7%            | 4              |
| Monday     | 66.7%            | 4              |
| Tuesday    | 33.3%            | 2              |
| Wednesday  | 33.3%            | 2              |
| Thursday   | 33.3%            | 2              |
| Friday     | 33.3%            | 2              |
| Saturday   | 33.3%            | 2              |
| answered question   |                  | 6              |
| skipped question  |                  | 462            |

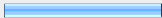




| 24. Please describe any unique characteristics of your customers. |                |
|---|----------------|
|   | Response Count |
|   | 10             |
| answered question   | 10             |
| skipped question  | 458            |

| 25. Please describe the age and gender characteristics of your customers. |                |
|---|----------------|
|   | Response Count |
|   | 10             |
| answered question   | 10             |
| skipped question  | 458            |

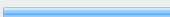
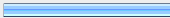
| 26. Do you own or lease your space? |   |                  |                |
|-------------------------------------|---|------------------|----------------|
|                                     |   | Response Percent | Response Count |
| Own                                 |  | 61.5%            | 8              |
| Lease                               |  | 38.5%            | 5              |
| answered question                   |   |                  | 13             |
| skipped question                    |   |                  | 455            |



| 27. What have been your sales trends over the past year?       |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Keeping with inflation   |  | 62.5%            | 5              |
| Declining  |   | 0.0%             | 0              |
| Growing  |  | 37.5%            | 3              |
| If growing or declining, to what do you attribute this change? |   |                  | 3              |
| answered question  |   |                  | 8              |
| skipped question   |   |                  | 460            |

| 28. Within the next 24 months, what do you plan to do? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Expand   |  | 46.2%            | 6              |
| Stay about the same                                    |  | 38.5%            | 5              |
| Downsize or close business                             |  | 15.4%            | 2              |
| If you are planning any changes, why?                  |   |                  | 6              |
| answered question                                      |   |                  | 13             |
| skipped question                                       |   |                  | 455            |

| 29. Have there been any recent changes in the area that are beneficial to your business? |                |
|--|----------------|
|  | Response Count |
|  | 11             |
| answered question  | 11             |
| skipped question   | 457            |

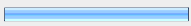
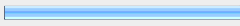

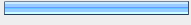

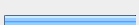
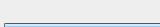
| 30. Are there any issues in the surrounding area that have a negative impact on your business? |                |
|--|----------------|
|  | Response Count |
|  | 11             |
| answered question  | 11             |
| skipped question   | 457            |




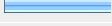






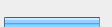


| 31. Is parking an issue for your business? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Yes  |  | 50.0%            | 7              |
| No   |  | 50.0%            | 7              |
| answered question                          |   |                  | 14             |
| skipped question                           |   |                  | 454            |

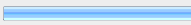

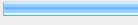
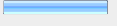




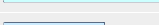




| 32. Is parking adequate for your customers? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Yes   |  | 41.7%            | 5              |
| No  |  | 58.3%            | 7              |
| answered question                           |   |                  | 12             |
| skipped question                            |   |                  | 456            |

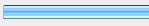

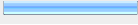

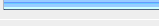




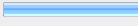

| 33. Where do your customers park? |                |
|-----------------------------------|----------------|
|                                   | Response Count |
|                                   | 10             |
| answered question                 | 10             |
| skipped question                  | 458            |

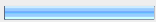

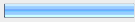

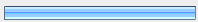


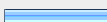

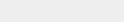

| 34. Where do your employees park? |                |
|-----------------------------------|----------------|
|                                   | Response Count |
|                                   | 9              |
| answered question                 | 9              |
| skipped question                  | 459            |

| 35. What types of activities do you think would help improve your business?      |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Physical improvements to neighborhood streets                                    |  | 53.8%            | 7              |
| Improved maintenance/cleanliness   |  | 69.2%            | 9              |
| Coordinated promotional activities by businesses                                 |  | 15.4%            | 2              |
| More parking   |  | 53.8%            | 7              |
| Accessibility to financing for your business                                     |  | 7.7%             | 1              |
| Addition of businesses that would complement the services or products you supply |  | 38.5%            | 5              |
| Other (please specify)   |  | 46.2%            | 6              |
| answered question  |   |                  | 13             |
| skipped question   |   |                  | 455            |

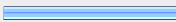
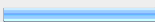
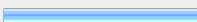










| 36. What are the best features of your immediate neighborhood? (Choose the 3 best features.) |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Walkability  |    | 73.6%            | 306            |
| Historic character   |    | 12.7%            | 53             |
| Open space along the Parkway   |    | 25.2%            | 105            |
| Access to Schuylkill River Trail   |    | 31.0%            | 129            |
| Close-knit community   |  | 10.6%            | 44             |
| Schools  |  | 1.9%             | 8              |
| Playgrounds  |  | 1.9%             | 8              |
| Proximity to work  |  | 26.2%            | 109            |
| Proximity to museums   |  | 27.9%            | 116            |
| Proximity to cultural events   |  | 20.9%            | 87             |
| Convenience of public transportation   |  | 30.8%            | 128            |
| Convenience of Center City retail  |  | 27.6%            | 115            |
| Other (please specify)   |  | 6.0%             | 25             |
| answered question  |   |                  | 416            |
| skipped question   |   |                  | 52             |

| 37. What are the best features of the larger Logan Square community? (Check the 3 best features.) |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Walkability   |  | 57.2%            | 237            |
| Historic character  |  | 17.6%            | 73             |
| Open space along the Parkway  |  | 39.6%            | 164            |
| Access to Schuylkill River Trail  |  | 30.2%            | 125            |
| Close-knit community  |  | 5.1%             | 21             |
| Schools   |  | 1.0%             | 4              |
| Playgrounds   |  | 1.2%             | 5              |
| Proximity to work   |  | 14.5%            | 60             |
| Proximity to museums  |  | 47.8%            | 198            |
| Proximity to cultural events  |  | 29.2%            | 121            |
| Convenience of public transportation  |  | 18.6%            | 77             |
| Convenience of Center City retail   |  | 29.5%            | 122            |
| Other (please specify)  |  | 3.4%             | 14             |
| answered question   |   |                  | 414            |
| skipped question  |   |                  | 54             |

| 38. What are your primary concerns for your immediate neighborhood? (Choose 3 primary concerns.) |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| New development pressures (e.g. River City, Barnes Tower, etc.)                                  |    | 44.2%            | 182            |
| Inadequacy of open space   |    | 13.6%            | 56             |
| Inadequacy of retail   |    | 38.8%            | 160            |
| Parkway event management   |  | 11.7%            | 48             |
| Number of homeless people  |  | 47.3%            | 195            |
| Parking  |  | 27.7%            | 114            |
| Traffic congestion   |  | 22.6%            | 93             |
| Difficult pedestrian crossings   |  | 26.5%            | 109            |
| Lack of easy access to river trails  |  | 6.1%             | 25             |
| Streetscape issues, such as street trees, sidewalks, and pedestrian lighting                     |  | 39.8%            | 164            |
| Other (please specify)   |  | 13.1%            | 54             |
| answered question  |   |                  | 412            |
| skipped question   |   |                  | 56             |

| 39. What are your primary concerns for the larger Logan Square community (Choose 3 primary concerns.) |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| New development pressures (e.g. River City, Barnes Tower, etc.)                                       |  | 43.7%            | 179            |
| Inadequacy of open space  |  | 11.2%            | 46             |
| Inadequacy of retail  |  | 37.8%            | 155            |
| Parkway event management  |  | 14.1%            | 58             |
| Number of homeless people   |  | 56.1%            | 230            |
| Parking   |  | 22.4%            | 92             |
| Traffic congestion  |  | 25.9%            | 106            |
| Difficult pedestrian crossings  |  | 30.5%            | 125            |
| Lack of easy access to river trails   |  | 3.9%             | 16             |
| Streetscape issues, such as street trees, sidewalks, and pedestrian lighting                          |  | 35.4%            | 145            |
| Other (please specify)  |  | 8.5%             | 35             |
| answered question   |   |                  | 410            |
| skipped question  |   |                  | 58             |

| 40. What issues on your block concern you most? Briefly list items in less than 20 words. |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| a.  |  | 100.0%           | 323            |
| b.  |  | 81.4%            | 263            |
| c.  |  | 64.1%            | 207            |
| answered question   |   |                  | 323            |
| skipped question  |   |                  | 145            |

| 41. What types of new development would you like to see in your immediate neighborhood? (Choose 3 characteristics.) |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Retail  |    | 51.1%            | 204            |
| Grocery or food store   |    | 44.4%            | 177            |
| Restaurants   |    | 58.4%            | 233            |
| New office, residential, or commercial space (10 stories or less)   |    | 9.3%             | 37             |
| New office, residential, or commercial space (11 stories or more)   |    | 9.8%             | 39             |
| New office space mid rise (10 stories or less)  |    | 1.8%             | 7              |
| New office space high rise (11 stories or more)   |    | 6.3%             | 25             |
| New residential condominiums mid rise (10 stories or less)  |    | 7.3%             | 29             |
| New residential condominiums high rise (11 stories or more)   |    | 8.5%             | 34             |
| New townhouses  |    | 26.6%            | 106            |
| Parking garages   |   | 16.0%            | 64             |
| Community center  |  | 22.6%            | 90             |
| Other (please specify)  |  | 12.0%            | 48             |
| answered question   |   |                  | 399            |
| skipped question  |   |                  | 69             |


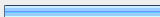



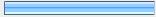

| 42. What types of new development would you like to see in the larger Logan Square community? (Choose 3 characteristics.) |  |                  |                |
|---|--|------------------|----------------|
|   |  | Response Percent | Response Count |
| Retail  |  | 54.1%            | 216            |
| Grocery or food store   |  | 43.9%            | 175            |
| Restaurants   |  | 56.6%            | 226            |
| New office, residential, or commercial space (10 stories or less)   |  | 8.8%             | 35             |
| New office, residential, or commercial space (11 stories or more)   |  | 14.5%            | 58             |
| New office space mid rise (10 stories or less)  |  | 2.0%             | 8              |
| New office space high rise (11 stories or more)   |  | 8.5%             | 34             |
| New residential condominiums mid rise (10 stories or less)  |  | 8.8%             | 35             |
| New residential condominiums high rise (11 stories or more)   |  | 11.0%            | 44             |
| New townhouses  |  | 19.0%            | 76             |
| Parking garages   |  | 19.3%            | 77             |
| Community center  |  | 24.6%            | 98             |
| Other (please specify)  |  | 10.0%            | 40             |
| answered question   |  |                  | 399            |
| skipped question  |  |                  | 69             |

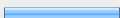





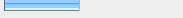
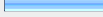
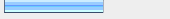


| 43. Do you think the recent development in Logan Square is |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| too much   |  | 24.7%            | 96             |
| about right  |  | 49.1%            | 191            |
| not enough?  |  | 26.2%            | 102            |
| answered question  |  |                  | 389            |
| skipped question   |  |                  | 79             |

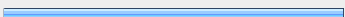
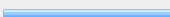

| 44. If you have concerns over recent developments in the neighborhood, what are they? (Choose 3 primary concerns.) |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Design of new construction unfriendly to pedestrian environment  |  | 30.1%            | 116            |
| Unsympathetic alterations to historic buildings  |  | 12.2%            | 47             |
| Lack of parking  |  | 24.7%            | 95             |
| Design of parking structure  |  | 18.7%            | 72             |
| Inadequate sidewalks   |  | 9.9%             | 38             |
| Inadequate lighting  |  | 23.4%            | 90             |
| Height or size of the development incompatible with neighborhood   |  | 44.4%            | 171            |
| Lack of landscaping  |  | 22.6%            | 87             |
| Blocking of views  |  | 30.6%            | 118            |
| Blocking of sunlight   |  | 27.0%            | 104            |
| Traffic congestion   |  | 24.4%            | 94             |
| Other (please specify)   |  | 9.1%             | 35             |
| answered question  |  |                  | 385            |
| skipped question   |  |                  | 83             |


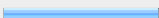
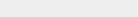
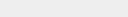




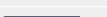

| 45. Which recent developments do you like? |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| a.   |  | 100.0%           | 238            |
| b.   |  | 61.3%            | 146            |
| c.   |  | 44.1%            | 105            |
| answered question                          |  |                  | 238            |
| skipped question                           |  |                  | 230            |


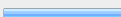
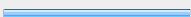
| 46. Which recent developments do you not like? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| a.   |  | 100.0%           | 201            |
| b.   |  | 45.8%            | 92             |
| c.   |  | 20.4%            | 41             |
| answered question                              |   |                  | 201            |
| skipped question                               |   |                  | 267            |

| 47. Do you believe there is enough park space in the neighborhood? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Yes (Skip to Question 4.)  |  | 43.8%            | 172            |
| No   |  | 56.2%            | 221            |
| answered question  |   |                  | 393            |
| skipped question   |   |                  | 75             |

| 48. If No, what type of open space is needed? (Choose 3 types.) |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Playgrounds/tot lots  |    | 33.6%            | 76             |
| Lawns for informal games  |    | 20.8%            | 47             |
| Playing fields  |    | 10.6%            | 24             |
| Private parklets incorporated into new development              |    | 27.0%            | 61             |
| Basketball courts   |    | 8.0%             | 18             |
| Tennis courts   |  | 21.7%            | 49             |
| Small parks with benches  |  | 52.2%            | 118            |
| Dog parks   |  | 28.8%            | 65             |
| Landscaped garden   |  | 48.7%            | 110            |
| Skate parks   |  | 10.2%            | 23             |
| Other (please specify)  |  | 9.3%             | 21             |
| answered question   |   |                  | 226            |
| skipped question  |   |                  | 242            |

| 49. Where would you locate new neighborhood open space? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| a.  |  | 100.0%           | 141            |
| b.  |  | 48.9%            | 69             |
| c.  |  | 22.0%            | 31             |
| answered question                                       |   |                  | 141            |
| skipped question  |   |                  | 327            |

| 50. Which parks do you think should be improved? (Choose up to 3.) |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Franklinton Park   |  | 21.1%            | 75             |
| Dilworth Plaza at City Hall  |  | 45.1%            | 160            |
| JFK Plaza/Tourist Hospitality Center ("Love Park")                 |  | 38.3%            | 136            |
| Logan Circle in front of Library and Family Court                  |  | 36.1%            | 128            |
| Science Park at 21st and Race Streets                              |  | 17.7%            | 63             |
| Calder Sculpture Garden at 22nd Street & the Parkway               |  | 24.8%            | 88             |
| Eakin's Oval   |  | 22.0%            | 78             |
| Schuykill River Park   |  | 22.3%            | 79             |
| Three Sisters Park in front of the Cathedral                       |  | 29.6%            | 105            |
| Other (please specify)   |  | 6.2%             | 22             |
| answered question  |   |                  | 355            |
| skipped question   |   |                  | 113            |

| 51. Do you believe there are too few, too many, or the right amount of events on the Parkway? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Too few   |  | 10.5%            | 40             |
| Too many  |  | 35.1%            | 134            |
| Just right  |  | 54.5%            | 208            |
| answered question   |   |                  | 382            |
| skipped question  |   |                  | 86             |

| 52. What event on the Parkway impact the Logan Square neighborhood the most? |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| July 4th   |  | 61.8%            | 217            |
| Unity Day  |  | 20.2%            | 71             |
| Thanksgiving Day Parade  |  | 10.8%            | 38             |
| Other (please specify)   |  | 7.1%             | 25             |
| answered question  |  |                  | 351            |
| skipped question   |  |                  | 117            |

| 53. Do you believe that your nearest commercial business and/or cultural institution is a "good neighbor?" |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Yes (Skip to Question 9.)  |  | 80.6%            | 300            |
| No   |  | 19.4%            | 72             |
| answered question  |  |                  | 372            |
| skipped question   |  |                  | 96             |

| 54. If No, what is your biggest concern? |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Parking                                  |  | 29.6%            | 24             |
| Traffic                                  |  | 9.9%             | 8              |
| Noise                                    |  | 6.2%             | 5              |
| Trash                                    |  | 27.2%            | 22             |
| Other (please specify)                   |  | 27.2%            | 22             |
| answered question                        |  |                  | 81             |
| skipped question                         |  |                  | 387            |

| 55. Are you a member of a neighborhood museum? |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Yes  |  | 60.3%            | 229            |
| No   |  | 39.7%            | 151            |
| answered question                              |  |                  | 380            |
| skipped question                               |  |                  | 88             |

| 56. Do you think pedestrian lighting is adequate in the neighborhood? |  |                  |                |
|---|--|------------------|----------------|
|   |  | Response Percent | Response Count |
| Yes (Skip to Question 12.)  |  | 43.3%            | 164            |
| No  |  | 56.7%            | 215            |
| answered question   |  |                  | 379            |
| skipped question  |  |                  | 89             |

| 57. If No, where do you recommend lighting be improved? |  |                  |                |
|---|--|------------------|----------------|
|   |  | Response Percent | Response Count |
| a.  |  | 100.0%           | 185            |
| b.  |  | 53.0%            | 98             |
| c.  |  | 27.6%            | 51             |
| answered question                                       |  |                  | 185            |
| skipped question  |  |                  | 283            |

| 58. Do you feel safe walking at night? |  |                  |                |
|--|--|------------------|----------------|
|  |  | Response Percent | Response Count |
| Yes (Skip to Question 14.)             |  | 58.6%            | 222            |
| No                                     |  | 41.4%            | 157            |
| answered question                      |  |                  | 379            |
| skipped question                       |  |                  | 89             |

| 59. If No, please list the places where you feel unsafe at night. |  |                  |                |
|---|--|------------------|----------------|
|   |  | Response Percent | Response Count |
| a.  |  | 100.0%           | 147            |
| b.  |  | 45.6%            | 67             |
| c.  |  | 25.2%            | 37             |
| answered question   |  |                  | 147            |
| skipped question  |  |                  | 321            |

| 60. Do you have school-aged children? |  |                  |                |
|---------------------------------------|--|------------------|----------------|
|                                       |  | Response Percent | Response Count |
| Yes                                   |  | 10.9%            | 42             |
| No                                    |  | 89.1%            | 342            |
| answered question                     |  |                  | 384            |
| skipped question                      |  |                  | 84             |



| 61. If you have children who attend school in the neighborhood, which school do they attend? |                  |                |
|--|------------------|----------------|
|  | Response Percent | Response Count |
|  |                  | 20             |
| <b>answered question</b>   |                  | <b>20</b>      |
| <b>skipped question</b>  |                  | <b>448</b>     |

| 62. If you have children who do not attend school in the neighborhood, which school do they attend? |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
|   |                  | 19             |
| <b>answered question</b>  |                  | <b>19</b>      |
| <b>skipped question</b>   |                  | <b>449</b>     |

| 63. Do you feel safe having your children walk to school? |                  |                |  |
|---|------------------|----------------|--|
|   | Response Percent | Response Count |  |
| Yes (Skip to Question 5.)                                 | 38.7%            | 12             |  |
| No  | 61.3%            | 19             |  |
| <b>answered question</b>                                  |                  | <b>31</b>      |  |
| <b>skipped question</b>                                   |                  | <b>437</b>     |  |

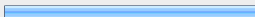




| 64. If No, why not?      |                  |                |  |
|--------------------------|------------------|----------------|--|
|                          | Response Percent | Response Count |  |
| Speed of traffic         | 31.8%            | 7              |  |
| Strangers                | 40.9%            | 9              |  |
| School too far from home | 18.2%            | 4              |  |
| Child too young          | 54.5%            | 12             |  |
| Other (please specify)   | 36.4%            | 8              |  |
| <b>answered question</b> |                  | <b>22</b>      |  |
| <b>skipped question</b>  |                  | <b>446</b>     |  |



| 65. Do you have daycare-aged children? |                  |                |  |
|--|------------------|----------------|--|
|  | Response Percent | Response Count |  |
| Yes                                    | 40.0%            | 16             |  |
| No (Continue to next page.)            | 60.0%            | 24             |  |
| <b>answered question</b>               |                  | <b>40</b>      |  |
| <b>skipped question</b>                |                  | <b>428</b>     |  |

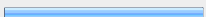
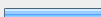

| 66. If Yes, do you feel daycare is adequate in this neighborhood? |                  |                |  |
|---|------------------|----------------|--|
|   | Response Percent | Response Count |  |
| Yes   | 29.4%            | 5              |  |
| No  | 70.6%            | 12             |  |
| <b>answered question</b>  |                  | <b>17</b>      |  |
| <b>skipped question</b>   |                  | <b>451</b>     |  |

| 67. What form of sidewalk behavior, if any, do you believe present the greatest problem for the greater Logan Square? |                  |                |  |
|---|------------------|----------------|--|
|   | Response Percent | Response Count |  |
| Encampments on public property  | 32.9%            | 123            |  |
| Encampments on private property   | 3.7%             | 14             |  |
| Anti-social behavior (threatening behavior, aggressive panhandling, etc.)   | 28.6%            | 107            |  |
| Sleeping on sidewalks   | 23.0%            | 86             |  |
| None  | 3.7%             | 14             |  |
| Other (please specify)  | 8.0%             | 30             |  |
| <b>answered question</b>  |                  | <b>374</b>     |  |
| <b>skipped question</b>   |                  | <b>94</b>      |  |



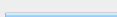


| 68. Do you believe that the number of homeless people/encampments in the larger Logan Square neighborhood (versus your quadrant thereof) presents a problem beyond humanitarian concerns? |                  |                |  |
|---|------------------|----------------|--|
|   | Response Percent | Response Count |  |
| Yes   | 72.1%            | 271            |  |
| No (Skip to Question 4.)  | 11.4%            | 43             |  |
| Somewhat  | 16.5%            | 62             |  |
| <b>answered question</b>  |                  | <b>376</b>     |  |
| <b>skipped question</b>   |                  | <b>92</b>      |  |

|   |   |                  |                |
|---|---|------------------|----------------|
| 69. If Yes or Somewhat, in which locations do you think the presence of homeless people/encampments presents the biggest problem?   |   |                  |                |
|   |   | Response Percent | Response Count |
| The Parkway (including the Youth Study Center, Sister Cities Park in front of St. Peter's Basilica, the park around Swann Fountain, the area in front of the public library, Aviator Park in front of the Franklin Institute, the park in front of the ExcelleRx building at 3 Parkway) |  | 73.9%            | 243            |
| Love Park   |  | 8.5%             | 28             |
| Near the Vine Street Expressway   |  | 4.3%             | 14             |
| Schuykill River Trail in the vicinity of Paine's Park (future skatepark)  |  | 3.6%             | 12             |
| Other (please specify)  |  | 9.7%             | 32             |
| answered question   |   |                  | 329            |
| skipped question  |   |                  | 139            |








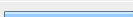


|   |   |                  |                |
|---|---|------------------|----------------|
| 70. Do you believe that the Parkway is an appropriate venue for feeding programs? |   |                  |                |
|   |   | Response Percent | Response Count |
| Yes   |  | 14.1%            | 52             |
| No  |  | 85.9%            | 316            |
| answered question   |   |                  | 368            |
| skipped question  |   |                  | 100            |


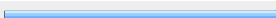
|   |   |                  |                |
|---|---|------------------|----------------|
| 71. Do you believe that the number of homeless people/encampments in your quadrant of the neighborhood (versus the larger Logan Square neighborhood) presents a problem beyond humanitarian concerns? |   |                  |                |
|   |   | Response Percent | Response Count |
| Yes   |  | 58.1%            | 215            |
| No (Skip to Question 7.)  |  | 28.1%            | 104            |
| Somewhat  |  | 13.8%            | 51             |
| answered question   |   |                  | 370            |
| skipped question  |   |                  | 98             |

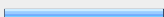
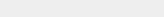


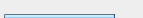
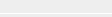

|  |   |                  |                |
|--|---|------------------|----------------|
| 72. If Yes or Somewhat, in which location do you think the presence of homeless people/encampments presents the biggest problem? |   |                  |                |
|  |   | Response Percent | Response Count |
| 22nd & Summer Streets  |  | 18.8%            | 49             |
| Cuthbert Street (between JFK and Arch)   |  | 30.3%            | 79             |
| Wawa at 21st and Hamilton Streets  |  | 27.2%            | 71             |
| Other (please specify)   |  | 23.8%            | 62             |
| answered question  |   |                  | 261            |
| skipped question   |   |                  | 207            |

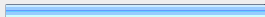

|  |   |                  |                |
|--|---|------------------|----------------|
| 73. If you believe that the number of homeless people/encampments presents a problem beyond humanitarian concerns, what are the major issues you associate with the problem? |   |                  |                |
|  |   | Response Percent | Response Count |
| Safety/Crime   |  | 29.5%            | 102            |
| Trash  |  | 10.4%            | 36             |
| Visual impact on visitors and residents  |  | 33.8%            | 117            |
| Monopolization of public spaces and amenities  |  | 16.2%            | 56             |
| Other (please specify)   |  | 10.1%            | 35             |
| answered question  |   |                  | 346            |
| skipped question   |   |                  | 122            |










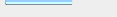
|  |  |                |  |
|--|--|----------------|--|
| 74. Are there any other quality of life issues that impact you? Please briefly list in 20 words or less. |  |                |  |
|  |  | Response Count |  |
|  |  | 119            |  |
| answered question  |  | 119            |  |
| skipped question   |  | 349            |  |






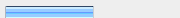
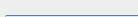
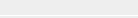

| 75. What form of transportation do you most often use to commute to work or school? (Choose 1 that applies most. If you routinely combine modes other than walking, explain your commute pattern on the 'Other' line.) |                  |                          |            |
|--|------------------|--------------------------|------------|
|  | Response Percent | Response Count           |            |
| Bus   | 13.9%            | 51                       |            |
| Regional Rail   | 3.5%             | 13                       |            |
| Subway    | 5.4%             | 20                       |            |
| Trolley   | 1.6%             | 6                        |            |
| Car – Drive alone   | 19.9%            | 73                       |            |
| Car - Carpool   | 0.5%             | 2                        |            |
| Bike    | 3.3%             | 12                       |            |
| <b>Walk</b>   | <b>37.6%</b>     | 138                      |            |
| None, work at home    | 3.0%             | 11                       |            |
| Other (please specify)    | 11.2%            | 41                       |            |
|  |                  | <b>answered question</b> | <b>367</b> |
|  |                  | <b>skipped question</b>  | <b>101</b> |

| 76. Are you a member of Philly CarShare or FlexCar?                                   |                  |                          |            |
|---|------------------|--------------------------|------------|
|   | Response Percent | Response Count           |            |
| Yes  | 20.1%            | 75                       |            |
| No   | 79.9%            | 299                      |            |
|   |                  | <b>answered question</b> | <b>374</b> |
|   |                  | <b>skipped question</b>  | <b>94</b>  |

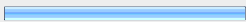
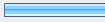
| 77. Regarding public transit, what are your highest priorities for improvements? (Choose 3 highest priorities.)   |                  |                          |            |
|---|------------------|--------------------------|------------|
|   | Response Percent | Response Count           |            |
| Improved, cleaner, more attractive shelters at major bus stops             | 46.5%            | 154                      |            |
| <b>Improved information at stops</b>                                       | <b>47.4%</b>     | 157                      |            |
| Improved frequency of service at rush hour                                 | 21.1%            | 70                       |            |
| Improved frequency of service during the evening                           | 32.0%            | 106                      |            |
| Improved frequency of service on weekends                                  | 40.5%            | 134                      |            |
| Faster service (express) to other locations in Center City                 | 30.5%            | 101                      |            |
| New routes to specific destinations. If chosen, please list destinations:  | 13.9%            | 46                       |            |
|   |                  | <b>answered question</b> | <b>331</b> |
|   |                  | <b>skipped question</b>  | <b>137</b> |

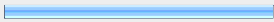


| 78. Do you use public transit?  |                  |                          |            |
|---|------------------|--------------------------|------------|
|   | Response Percent | Response Count           |            |
| Yes (Skip to Question 6.)  | 76.1%            | 284                      |            |
| No                         | 23.9%            | 89                       |            |
|   |                  | <b>answered question</b> | <b>373</b> |
|   |                  | <b>skipped question</b>  | <b>95</b>  |

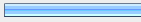





| 79. If No, why not?  |                  |                          |            |
|--|------------------|--------------------------|------------|
|  | Response Percent | Response Count           |            |
| Does not go where I need to go                              | 25.3%            | 24                       |            |
| Too slow    | 24.2%            | 23                       |            |
| Does not run when I need it                                 | 5.3%             | 5                        |            |
| Schedules are too infrequent                                | 10.5%            | 10                       |            |
| Too crowded   | 10.5%            | 10                       |            |
| Unclean vehicles    | 20.0%            | 19                       |            |
| Discourteous service                                        | 18.9%            | 18                       |            |
| Too expensive   | 7.4%             | 7                        |            |
| Information for schedules and routes not readily available  | 18.9%            | 18                       |            |
| <b>Other (please specify)</b>                               | <b>34.7%</b>     | 33                       |            |
|  |                  | <b>answered question</b> | <b>95</b>  |
|  |                  | <b>skipped question</b>  | <b>373</b> |

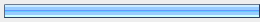



| 80. Are there any particular intersections that are difficult to cross? (Choose 3 most difficult crossings.)                                    |                  |                          |            |
|---|------------------|--------------------------|------------|
|   | Response Percent | Response Count           |            |
| Spring Garden Street/Pennsylvania Avenue/23rd Street         | 18.9%            | 63                       |            |
| 15th/16th Streets at I-676 Ramps                           | 12.9%            | 43                       |            |
| Pennsylvania Avenue between 20th and 23rd Streets          | 12.9%            | 43                       |            |
| 22nd Street at I-676 ramps                                 | 25.8%            | 86                       |            |
| The Parkway between 15th and 17th                          | 25.2%            | 84                       |            |
| <b>Logan Circle and the Parkway between 18th and 20th</b>  | <b>52.6%</b>     | 175                      |            |
| The Parkway between 21st and 22nd                          | 38.7%            | 129                      |            |
| Eakins Oval and the Parkway                                | 40.5%            | 135                      |            |
| Other (please specify)                                     | 18.0%            | 60                       |            |
|   |                  | <b>answered question</b> | <b>333</b> |
|   |                  | <b>skipped question</b>  | <b>135</b> |



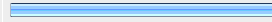

| 81. Do you own a car? |   |                  |                |
|-----------------------|---|------------------|----------------|
|                       |   | Response Percent | Response Count |
| Yes                   |  | 70.7%            | 265            |
| No                    |  | 29.3%            | 110            |
| answered question     |   |                  | 375            |
| skipped question      |   |                  | 93             |

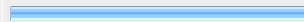
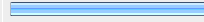
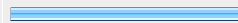


| 82. How many cars are in your household? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| 0  |   | 0.0%             | 0              |
| 1  |  | 78.9%            | 210            |
| 2  |  | 20.3%            | 54             |
| More than 2                              |  | 0.8%             | 2              |
| answered question                        |   |                  | 266            |
| skipped question                         |   |                  | 202            |

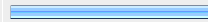
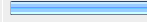
| 83. Where do you park it?             |  |                  |                |
|---------------------------------------|--|------------------|----------------|
|                                       |  | Response Percent | Response Count |
| In your own garage on your property   |   | 40.4%            | 107            |
| In a garage off of your property      |   | 6.4%             | 17             |
| In your driveway or parking pad       |   | 12.1%            | 32             |
| In a parking lot off of your property |   | 8.3%             | 22             |
| On street                             |   | 23.0%            | 61             |
| Other (please specify)                |  | 9.8%             | 26             |
| answered question                     |  |                  | 265            |
| skipped question                      |  |                  | 203            |


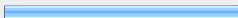

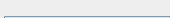
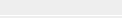

| 84. If you generally park on-street, how far from your home/destination do you usually park? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| 0 to 2 blocks  |  | 75.0%            | 84             |
| 2 to 4 blocks  |  | 23.2%            | 26             |
| 4 to 6 blocks  |  | 0.9%             | 1              |
| More than 6 blocks   |  | 0.9%             | 1              |
| answered question  |   |                  | 112            |
| skipped question   |   |                  | 356            |

| 85. If you park a car at a private facility, how much do you spend per month to park it? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Under \$50   |  | 8.9%             | 10             |
| \$51 - \$100   |  | 10.7%            | 12             |
| \$101 - \$150  |  | 50.9%            | 57             |
| \$151 - \$200  |  | 17.9%            | 20             |
| \$201 - \$250  |  | 5.4%             | 6              |
| More than \$250  |  | 6.3%             | 7              |
| answered question  |   |                  | 112            |
| skipped question   |   |                  | 356            |

| 86. Do you consider the train underpasses between 21st and 23rd Streets north of Market Street barriers between the central business district and the larger Logan Square neighborhood? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Yes   |  | 77.4%            | 278            |
| No (Skip to Question 3.)  |  | 22.6%            | 81             |
| answered question   |   |                  | 359            |
| skipped question  |   |                  | 109            |

| 87. If Yes, what improvements would you recommend to encourage movement through the underpasses? |   |                  |                |
|--|---|------------------|----------------|
|  |   | Response Percent | Response Count |
| Better lighting  |    | 87.3%            | 247            |
| Landscaping  |    | 58.3%            | 165            |
| Cleaning and/or painting the structures  |    | 67.5%            | 191            |
| Commissioning murals or other types of art installations for the abutments                       |  | 24.7%            | 70             |
| Other (please specify)   |  | 17.7%            | 50             |
| answered question  |   |                  | 283            |
| skipped question   |   |                  | 185            |

| 88. Do you consider the Parkway to be a barrier between the north and south parts of the Logan Square neighborhood? |   |                  |                |
|---|---|------------------|----------------|
|   |   | Response Percent | Response Count |
| Yes   |  | 58.9%            | 218            |
| No (Continue to next page.)   |  | 41.1%            | 152            |
| answered question   |   |                  | 370            |
| skipped question  |   |                  | 98             |

| 89. If Yes, what improvements would you recommend to improve the connection?  |                  |                |
|---|------------------|----------------|
|   | Response Percent | Response Count |
| Improved signals                                   | 64.7%            | 141            |
| <b>Better crosswalks</b>                           | <b>68.8%</b>     | 150            |
| Better signage                                     | 25.2%            | 55             |
| Widening the median for pedestrians                | 48.6%            | 106            |
| Widen the sidewalk at the corners (e.g. bumpouts)  | 34.4%            | 75             |
| Other (please specify)                             | 21.1%            | 46             |
| <i>answered question</i>  |                  | <b>218</b>     |
| <i>skipped question</i>   |                  | <b>250</b>     |

## A.2 Public Workshop Priority Results

Table 5a. Quality of Life Community Priorities

|                                  |                       | 1st<br>Priority | 2nd<br>Priority | 3rd<br>Priority |  |
|----------------------------------|-----------------------|-----------------|-----------------|-----------------|--|
| Passive Open Space & Streetscape | Logan Square Parkway  | 4               | 1               | 1               | Additional cafés along Parkway.  |
|                                  |                       | 3               | 2               |                 | New sidewalks and benches on Parkway.  |
|                                  |                       | 3               |                 | 1               | Infill openings over Vine Expressway at Free Library.  |
|                                  |                       | 2               |                 | 1               | Better landscape on north-south streets across Parkway.  |
|                                  |                       | 1               |                 |                 | Landscaped/widened replacement bridge crossings over the Vine Expressway.                                |
|                                  | Schuylkill River Park | 2               | 1               | 1               | Restroom/water fountain on River trail.  |
|                                  |                       | 2               |                 |                 | Public dock on River between Arch and Vine.  |
|                                  |                       | 1               |                 | 1               | Bicycle/pedestrian link between Rivers Edge and Cherry Street east of rail tracks.                       |
|                                  |                       | 1               |                 |                 | Wayfinding signage from Art Museum to River trail.   |
|                                  |                       |                 |                 |                 | Bridge over rail tracks to River trail from 24 <sup>th</sup> Street near Parktowne Place.                |
|                                  | Dilworth Plaza        | 1               |                 |                 | Rehabilitate plaza/improve connections to subway concourse.  |
|                                  |                       |                 |                 |                 | Reduce dense tree cover.   |
|                                  | Streetscape           | 5               | 7               | 2               | Install pedestrian lighting throughout neighborhood.   |
|                                  |                       | 5               | 3               |                 | Improve JFK/SEPTA track underpasses at 21 <sup>st</sup> , 22 <sup>nd</sup> and 23 <sup>rd</sup> Streets. |
|                                  |                       | 2               |                 |                 | Improved shelter/transit information at bus stops.   |
|                                  |                       | 1               | 1               |                 | Enhance historic small streets (lighting, paving, trees)   |
|                                  |                       | 1               |                 |                 | "Green" existing parking garages with "vertical gardens."  |
|                                  |                       |                 | 1               | 1               | Enhance neighborhood "gateways" (Vine Expressway to 22 <sup>nd</sup> and 23 <sup>rd</sup> Streets).      |
|                                  |                       |                 |                 | 1               | "Green" Race Street between Convention Center and Schuylkill River.                                      |
|                                  |                       |                 |                 |                 | "Green" Cherry Street between Convention Center and Parkway.   |
|                                  | Other Open Space      |                 |                 |                 | Add trash receptacles in commercial areas.   |
|                                  |                       | 4               |                 | 2               | More pocket parks (like Coxe Park)   |
|                                  |                       | 3               |                 |                 | Require new development to provide passive open space  |
|                                  |                       | 2               | 1               | 1               | Provide neighborhood dog parks   |
|                                  |                       | 1               |                 |                 | Open Science Park for neighborhood children.   |
| Active Open Space                |                       | 1               |                 |                 | Community use of rooftop tennis courts at Community College.   |
|                                  |                       |                 |                 | 2               | Construct tennis courts (under the Expressway at 24th Street?)   |
|                                  |                       |                 |                 |                 | Community use of Roman Catholic High playing fields on 24th Street.                                      |
|                                  |                       |                 |                 |                 | Complete Paines Park skateboard venue.   |
|                                  |                       |                 |                 |                 | Picnic grove on trail at Martin Luther King Drive.   |
|                                  |                       |                 |                 |                 | Reprogram existing playing fields/add new fields, for soccer.  |
| Homelessness                     |                       | 11              | 4               |                 | Remove all homeless encampments.   |
|                                  |                       | 10              |                 |                 | Cease outdoor food distribution.   |
|                                  |                       | 2               |                 | 1               | Install public restrooms on Parkway.   |
|                                  |                       | 2               |                 |                 | Encourage businesses to employ formerly homeless.  |
|                                  |                       |                 |                 |                 | Increase public support for addressing issue.  |
|                                  |                       |                 |                 |                 | Inform community on proposals for homeless/social service facilities in neighborhood.                    |



Table 5b. Quality of Life Community Priorities

|                           |                               | 1st<br>Priority | 2nd<br>Priority | 3rd<br>Priority |   |
|---------------------------|-------------------------------|-----------------|-----------------|-----------------|---|
| Education                 | title<br>received<br>one vote | 1               |                 |                 | Support appropriate expansion of charter and private schools.                                       |
|                           |                               | 1               |                 |                 | Support appropriate expansion of higher education facilities.                                       |
| Sustainability            |                               | 4               | 2               | 3               | Increase tree canopy (500 trees in 3 years)   |
|                           |                               | 3               |                 |                 | Promote LEED for new developments.  |
|                           |                               | 2               | 3               |                 | Require car share and bicycle facilities.   |
|                           |                               | 2               |                 |                 | Promote "green streets."  |
|                           |                               | 1               |                 | 1               | Discourage private auto ownership/encourage transportation alternatives.                            |
|                           |                               | 1               |                 |                 | Encourage alternate energy systems.   |
|                           |                               |                 | 2               | 1               | Community gardens on neighborhood side of CSX tracks.   |
|                           |                               |                 |                 |                 | Encourage "green roofs."  |
|                           |                               |                 |                 |                 | Encourage rainwater capture (rain barrels).   |
|                           |                               |                 |                 |                 | Promote recycling/composting.   |
|                           |                               |                 |                 |                 | Require trash recycling for multi-family developments.  |
| Parkway<br>Events         |                               | 7               | 4               |                 | Limit events to signature events (Thanksgiving Park, July 4th, etc.)                                |
|                           |                               | 2               |                 |                 | Encourage alternatives to driving to events.  |
|                           |                               | 2               |                 |                 | Extend standard weekly parking time limits in residential areas to weekends/holidays during events. |
|                           |                               | 1               |                 |                 | Create committee to evaluate number and type of events.   |
|                           |                               | 1               |                 |                 | Provide shuttles to off-site parking areas.   |
| Light/Noise<br>Pollution  |                               | 4               |                 |                 | Minimize noise impacts of building mechanical equipment.  |
|                           |                               | 2               |                 | 1               | Minimize impacts of outside light sources on interior residential spaces.                           |
| Community<br>Organization |                               | 8               | 1               | 1               | Establish review process for projects that includes developers, public agencies, and residents.     |
|                           |                               | 7               | 4               |                 | Add LSNA member to Parkway Foundation Board.  |
|                           |                               |                 |                 | 1               | Build coalition of support for initiatives.   |

Table 6a. Transportation Community Priorities

|             |                             | 1st<br>Priority  | 2nd<br>Priority | 3rd<br>Priority  |   |
|-------------|-----------------------------|--|-----------------|--|---|
| Circulation |                             | 7  | 1               |  | Improve intersections at 20th and Parkway/15th and Vine, and Broad and Vine.                          |
|             |                             | 2  |                 |  | Adjust signal timing at 22nd and Parkway, 16th and Spring Garden, and Broad and Callowhill.           |
|             |                             | 2  |                 |  | Traffic signal on 22nd Street at Vine Street ramps.   |
|             |                             | 1  |                 |  | Reestablish 18th Street as through street at Callowhill.  |
|             |                             |  | 1               | 1  | Redesign traffic/pedestrian patterns at Eakins Oval.  |
|             |                             |  |                 |  | Convert Callowhill Street between Franklin Boulevard and 15th Street to 2-way.                        |
|             |                             |  |                 |  | Convert Market Street and JFK Boulevard to 2-way traffic.   |
| Parking     | General                     | 4  | 2               | 1  | New parking facility north of Vine between 18th and 21st Streets to serve Parkway institutions.       |
|             |                             | 2  |                 | 1  | Replace unneeded traffic lanes with parking lanes.  |
|             |                             | 1  |                 |  | Remove unnecessary parking restrictions.  |
|             |                             | 1  |                 |  | Rehabilitate JFK Plaza garage.  |
|             |                             |  |                 |  | Encourage weekend/evening use of parking resources near Broad Street.                                 |
|             | Residential Parking         | 4  | 2               |  | Make temporary visitor parking permits available.   |
|             |                             | 2  |                 |  | Prohibit garage front townhouses.   |
|             |                             | 1  | 1               |  | Maintain existing minimum 0.7 space per unit parking ratio. Maximum 1.0 space per unit.               |
|             |                             |  | 4               | 1  | Require 1 car share space per 50 units.   |
|             |                             |  |                 |  | Reduction of 4 required parking spaces for every 1 car share space.                                   |
|             |                             |  |                 |  | Reconfigure Pennsylvania Avenue to accommodate additional parking.                                    |
|             |                             | Added: 24 Hour Parking for Visitors; 3 votes at 1st priority |                 |  |   |
|             | Business Parking            |  |                 |  | Increase on-street meter rates, and reduce short-term garage rates, (2005 Center City Parking Study). |
|             |                             |  |                 |  | 2-hr. meter parking on Callowhill Street for retail.  |
|             |                             |  |                 |  | Provide parking for Fed Ex, UPS, to minimize double parking.  |
|             |                             | 9  | 10              | 1  | New underground parking at Barnes Museum or Free Library  |
|             |                             | 3  |                 |  | Eliminate surface parking at Eakins Oval.   |
|             |                             | 1  |                 |  | Parking lanes on Parkway for institutional uses.  |
|             |                             |  |                 | Better signage to parking facilities.                    |   |
|             | Special<br>Event<br>Parking | 1  |                 | 1  | Allow permit-parking holders to bypass roadblocks.  |
| 1           |                             |  |                 | Permit residential parking on event days.                |   |
|             |                             |  |                 | Extend permit-parking restrictions to weekends/holidays. |   |
|             |                             |  |                 | Make use of Community College parking.                   |   |

Table 6b. Transportation Community Priorities

|                               | 1st<br>Priority | 2nd<br>Priority | 3rd<br>Priority |   |
|-------------------------------|-----------------|-----------------|-----------------|---|
| Pedestrians                   | 7               | 2               |                 | 4-way stop at 22nd and Cherry Street.   |
|                               | 2               | 1               | 3               | Add ramp/stair connections to/from JFK at 21st, 22nd, and 23rd Streets.                                       |
|                               | 2               | 1               | 2               | Eliminate unneeded outer lanes on Parkway, to reduce pedestrian crossing distances.                           |
|                               | 1               |                 |                 | Calm traffic on Callowhill Street.  |
|                               | 1               |                 |                 | Install pedestrian count down timers at all crossings with more than 3 traffic lanes.                         |
|                               | 1               |                 |                 | Redesign Eakins Oval.   |
|                               | 1               |                 |                 | Tighten curb radii and add bulb outs at 21st and 22nd Streets and Parkway, to calm traffic.                   |
|                               |                 |                 |                 | Eliminate free turn at 23rd Street and Vine Expressway.   |
|                               |                 |                 |                 | Eliminate free turn at 24th Street and Eakins Oval.   |
|                               |                 |                 |                 | Eliminate right angle parking at WaWa and restore sidewalk.   |
| Bicycles                      | 2               |                 | 1               | Complete bicycle lanes on 22nd Street.  |
|                               | 1               |                 | 1               | Relocate bike lanes to outer lanes of Parkway.  |
|                               |                 | 1               |                 | Install bicycle lanes on 20th Street south of Parkway.  |
|                               |                 | 1               |                 | Install bicycle rental/parking facility on Race Street at Schuylkill River Park, Free Library, and JFK Plaza. |
|                               |                 |                 |                 | Establish Pennsylvania Avenue/Callowhill Street as bike route.  |
|                               |                 |                 |                 | Provide bike parking on Callowhill Street, Van Colln Field, and Spring Garden station.                        |
| Transit                       | 3               | 2               |                 | Peak service target of 10 minutes on bus routes.  |
|                               | 3               | 2               |                 | Off-peak service target of 15 minutes.  |
|                               |                 | 1               |                 | Encourage coordination of private shuttle routes and schedules.   |
|                               |                 |                 |                 | Replace 40-foot buses with 60-foot articulated buses.   |
| Infrastructure<br>Investment  | 4               |                 |                 | Make City Hall Station/Dilworth Plaza a welcoming transit gateway.  |
|                               | 3               | 1               | 1               | Advocate for use of City Branch Right-of-Way as high frequency transit corridor.                              |
|                               |                 | 1               |                 | Frequent, year round service linking tourist attractions.   |
| Informational<br>Improvements | 2               | 1               | 5               | Bus status information system at all bus stop signs.  |
|                               |                 |                 |                 | Install permanent signs showing Parkway event detour routes.  |



**Table 7. Development Community Priorities**

|  | 1st<br>Priority | 2nd<br>Priority | 3rd<br>Priority |  |
|--|-----------------|-----------------|-----------------|--|
| Neighborhood<br>Conservation               | 2               | 2               | 1               | Consider adoption of Neighborhood Conservation District, (20th/23rd, Winter and Arch)  |
| Historic<br>Resources                      | 2               |                 | 1               | Complete survey of historic resources for Local Register.  |
|  | 1               |                 |                 | Encourage use of Transfer of Development Rights for historic preservation  |
| Zoning Map                                 | 9               | 4               | 1               | Change C4 (Commercial) zoning to RC4 (Residential/Commercial) on Arch Street west of 19th Street.                                      |
|  | 1               |                 |                 | Update zoning map to eliminate obsolete industrial.  |
|  |                 |                 | 2               | Rezone parcels along Broad Street corridor for C5 higher intensity commercial zoning   |
|  |                 |                 | 1               | Rezone 1900 and 2000 blocks of Spring Garden Street from R15 to R10, to reflect historic district.                                     |
|  |                 |                 |                 | Rezone parks (Franklinton Park, JFK Plaza) for recreation.   |
| Specific Zoning<br>District Controls       | 17              | 4               | 2               | Reduce excessive additional FAR provisions in C4 and RC4 zoning classifications.   |
|  | 6               | 6               | 1               | Provide building bulk controls (height, spacing, and footprint dimensions) to assure daylight, sunlight penetration and protect views. |
|  | 4               | 4               |                 | Modify existing 125' height limit to permit taller, small footprint buildings, in appropriate locations, to protect views and light.   |
|  | 2               | 2               | 3               | Prohibit aboveground parking garages for accessory parking unless they are wrapped in active uses.                                     |
|  | 1               |                 | 1               | Require that all new residential developments provide open space at minimum 40 sq.ft. per unit.  |
|  |                 |                 | 1               | Prohibit parking garages as a main use west of 16th Street.  |
|  |                 |                 |                 | Eliminate inappropriate FAR bonuses for building setbacks in R15 zone.   |
| Design Review<br>for Major<br>Developments | 11              | 1               | 3               | Establish design review procedure, evaluated against design guidelines that incorporate community input, for all major developments.   |