

# Dauphin Street Senior Residences West Dauphin St. & North Carlisle St.



Submission To: Philadelphia City Planning Commission

Purpose: Civic Design Review

Project: Dauphin Street Senior Residences  
1412-1426 West Dauphin Street  
Philadelphia, PA 19132

Applicant/Owner: 1400 Dauphin Associates, LLC  
1341 N. Delaware Ave, Suite 204  
Philadelphia, PA 19125

Architect: WRT, LLC

Engineer: Amberic Technology Corporation

# **PHILADELPHIA CITY PLANNING COMMISSION**

## **Civic Design Review Submission**

1412-1426 W. Dauphin Street  
“Dauphin Street Senior Residences”

**Tuesday, September 2, 2014**

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1. Civic Design Review Application
2. Project Summary Including Sustainable Design Elements and Building Materials
3. Photographs of the Proposed Building Site
4. Aerial Photographs in Plan and Oblique Views
5. Existing Conditions & Consolidation Plan
6. Site Plan of Proposed Project with Landscaping Elements
7. Complete Streets Handbook Checklist
8. Supplemental Streets Department Zoning Plan
9. PWD Limit of Disturbance/Erosion & Sediment Control Plan
10. Proposed Ground Floor Plan with Landscaping Elements
11. Typical Upper Floor Plans
12. Elevations with Exterior Building Materials
13. Site Sections
14. Perspective Renderings
15. Physical Massing Model

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## CDR PROJECT APPLICATION FORM

**L&I APPLICATION NUMBER:** 550079

**What is the trigger causing the project to require CDR Review? Explain briefly.**

The property is located in a commercial district (CMX-2). It affects several RM-1 properties as it shares a rear property line with an RM-1 property and is within 200 ft on the opposite blockface of an RM-1 property, separated by a street less than 100 ft wide. Furthermore, the project contemplates more than 50 new residential units (54 total) and includes buildings that are more than 20 ft. taller than the maximum permitted height of an affected RM-1 zoned lot (38 ft. maximum height permitted in RM-1, 61 ft. proposed).

### PROJECT LOCATION

<b>Planning District:</b> <u>Lower North</u>	<b>Council District:</b> <u>5th</u>
<b>Address:</b> <u>1412-1426 W. Dauphin Street</u> <u></u>	
<b>Is this parcel within a Master Plan District?</b> Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>	

### CONTACT INFORMATION

<b>Applicant Name:</b> <u>1400 Dauphin Associates, LLC.</u>	<b>Primary Phone:</b> <u>215-751-0205</u> <u>267-765-7377 (mferleger)</u> <u>267-765-9653 (dbeauvais)</u>
<b>Email:</b> <u>hjreid@mazegroup.com</u> <u>cc: drbeauvais@zarwin.com</u> <u>mferleger@zarwin.com</u>	<b>Address:</b> <u>1341 N. Delaware Ave., Suite 204</u> <u>Philadelphia, PA 19125</u>
<b>Property Owner:</b> <u>1400 Dauphin Associates, LLC.</u>	<b>Developer:</b> <u>1400 Dauphin Associates, LLC.</u>
<b>Architect:</b> <u>Ambric Technology Corporation (Engineering), Wallace, Roberts &amp; Todd (Architects)</u>	

**CONTINUED ON NEXT PAGE**

## SITE CONDITIONS

Site Area: 10,967 sq. ft. (post-consolidation)

Existing Zoning: CMX-2 Are Zoning Variances required? Yes X No     

## SITE USES

1412 W. Dauphin St. - Vacant ground

Present Use: 1414-26 W. Dauphin St. - Vacant two (2) story brick building (27' high)

1414-26 W. Dauphin St. - Vacant two (2) story brick building (27' high)

1414-26 W. Dauphin St. - Vacant three (3) story brick building (35' high)

Proposed Use:

*Area of Proposed Uses, Broken Out by Program (Include Square Footage and # of Units):*

For the demolition of all existing structures on both lots. For the erection of a new five (5) story structure for the use as a multi-family dwelling for senior housing with fifty-four (54) dwelling units.

*Proposed # of Parking Units:*

None

## COMMUNITY MEETING

Community meeting held: Yes X No      Additional community meeting scheduled by the coordinating RCO, Uptown Entertainment and Development Corporation

If yes, please provide written documentation as proof.

If no, indicate the date and time the community meeting will be held:

Date: August 21, 2014 Time: 6:00p.m. at 2227 N Broad Street

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**Overall Project Summary**

Dauphin Street Senior Residences is intended to help address very under-served needs for secure and affordable senior housing in its North Philadelphia neighborhood. The proposed structure will provide 52-54 one-bedroom apartments with full kitchens. The building will also include common facilities, 2 protected outdoor areas, and space for social / supportive services aimed at allowing its residents to safely “age in place” without having to leave the neighborhood. Additionally, the developer is seeking relationships with nearby institutions that would allow some of the ground floor common / support facilities to be accessible on a limited basis to the broader community surrounding this new facility.

**The Site**

The site for the proposed Dauphin Street Residences has been entirely assembled from parcels that had been previously cleared or contain buildings that are un-occupied and boarded up. As similar under-utilization of land resources can be seen in blocks to the north, south and west of the site, it is hoped that the substantial investment in the Dauphin Street Senior Residences will serve as a vote of confidence in this section of the City and spawn further improvements.

The project’s close proximity to the City’s North Broad Street Corridor make it an ideal candidate for densified transit-oriented development. The building will be within about 200’ of an entrance to the Broad Street Subway (Susquehanna-Dauphin Station). It will also be very close to stops for several SEPTA bus routes and within walking range of retail and service uses along this part of Broad Street. Given the importance of walkable services and mass-transit options for low income seniors, the proposed Dauphin Street Senior Residences is a very appropriate fit for its site.

**Project Goals**

Some of the development team’s more significant goals include:

1. Return a significant un-utilized parcel of land in an important location to the City’s tax base by redeveloping it for an active and appropriate use.
2. Reinforce the City’s goals to promote transit-oriented development, reduce reliance on automobiles, and create walkable activity zones around major transportation hubs.
3. Help to extend a sense of safe walkable streets out from Broad Street to the neighborhood to the west.
4. Address minimum “threshold” and “selection” requirements established by the Pennsylvania Housing Finance Authority (PHFA), so as to create a socially and economically viable development of at least 50 apartment units that can be awarded the Tax Credits necessary to fund the construction of affordable housing projects.
5. Develop architectural treatments that transition between larger-scale buildings one block away along Broad Street and the row house communities to the south, east and west of the site.
6. Create the potential for the broader surrounding neighborhood to avail itself in limited ways of resident services and other facilities within the new building.

### **Organization & Overall Massing**

In order to be economically viable/sustainable from an operating perspective, PHFA-funded affordable senior housing projects typically need to have at least 50 apartment units. The program for the Dauphin Street Senior Residences has been kept as close to this 50-unit threshold as possible, so as to minimize the building's bulk and allow its form to mediate between neighboring townhouses and larger structures along the Broad Street Corridor.

Floors 2-4 of the building will each have 12 one-bedroom apartments. The upper-most (5<sup>th</sup>) floor will have one less unit (11 total). The area normally occupied by the twelfth unit on that top floor will instead be re-assigned to provide an additional satellite common area for all of the building's residents and an adjoining roof terrace. Both of these will feature un-obstructed views back to Center City. The interior common space at this location faces south and will be treated as a solarium.

At the ground floor level, apartments units will be limited to no more than 5-6 one bedroom units, so as to allow ample room for commons, office and service/support type functions. Key common spaces, including a large community room, will flank the main entrance and will be provided with large windows to help activate that area the building. The main entrance will be located just off the intersection of Dauphin and Carlisle Streets. It, along with the stack of specially-configured one-bedroom units directly above it, is intended to recall the turret features of many of the neighborhood's older corner buildings, and to serve as a welcoming beacon when approaching the site from Broad Street.

### **Project Exterior Treatment, Details and Materials**

The design of the project uses projecting bays of differing sizes and configurations to accentuate its main entrance, help break down its overall mass, and allow its form to more comfortably relate to nearby 2-3 story townhouses. Typical windows exceed code-minimums and have been given a vertical proportion to more closely replicate the size and orientation of openings found in the neighborhood's older row homes. Most units will have sliding-door assemblies in their living areas with metal "French balcony rails" outboard of them. Aside from their safety function, these rails add a layer of detail to the building. By allowing glazing in apartment living areas to extend close to the floor level, they also create a more "eyes on the street" feel that can increase comfort levels for people walking through the neighborhood.

The main body of the building will be surfaced in a combination of face-brick and integrally-colored synthetic stucco, with the stucco limited to the upper portion of the building.

The project will rely on a vocabulary of exterior materials typically found in Philadelphia row-house neighborhoods, including;

1. Face Brick: Modular extruded brick, mid-range red color, with limestone-colored precast copings at key transitions with other materials.
2. Synthetic Stucco: Buff or light warm grey range, with added control joints to help break down scale of these areas.
3. Metal Panels: A combination of flat horizontal-joint panels and vertical ribbed panels. Vertical ribbed panels would be lighter (similar to Atas #13-Dove Grey). Horizontal panels would be more mid-range (similar to Atas #20-Slate Grey).
4. Windows; Factory-finished aluminum casement or slider type. Color to be clear anodized.
5. Storefronts & Other Ground Floor Openings: Factory-finished aluminum. Clear anodized.
6. Entry Canopy/Trellis system: Color to match adjoining storefront materials.

### **Intended Sustainable Design Elements**

The project incorporates sustainable thinking at many different levels including:

- **Siting & Density:** Dauphin Street Senior Residences is a model of appropriate transit-oriented development. Its location is within 200' of a major subway station. It is also within similar range of stops for at least 4 SEPTA bus routes. And, it is within walking range to a wide variety of services clustered around the Broad St subway stop.
- **Parking:** Because this will be an affordable senior housing project with excellent access to transit and services, the developer plan to provide no additional on-site parking. This will allow the street level of the building to feel more urban and pedestrian friendly.
- **Site Details:** The project will include all items required by Philadelphia's Complete Streets Regulations. Additionally, the developer intends to provide outdoor recreational areas for residents in the form of an at-grade protected garden and an additional 400-square foot terrace at the top floor of the building. Both are south-facing. At least one of these areas will be fitted out with raised beds for community gardening. Any ornamental vegetation will be limited to drought-tolerant native species.
- **Appliances:** All appliances installed within apartments will meet Energy Star 3.0 rating thresholds (unless the appliance type is exempt from the Energy Star rating program).
- **Electrical:** High efficiency light fixtures will be used throughout the facility.
- **Plumbing:** The developer has committed to providing at least 5% of domestic hot water needs for this building using a solar-assist system to pre-heat water. A battery of solar panels will be provided on the roof of the building for this purpose. In addition, low flow fixtures will be used throughout.
- **Mechanical:** Recent changes to PHFA regulations require that mechanical systems for individual apartment units meet certification requirements established under the Energy Star 3.0 rating system. In practice, the kinds of low-budget through-wall mechanical units typically used in senior housing will no longer be allowed in PHFA-funded projects. The developer of the Dauphin Street Senior Residences has committed to providing high efficiency mechanical units throughout the building that fully comply with the new Energy Star 3.0 standards. Although a final determination has not yet been made, mechanical systems used in this project will most likely be one of the following:
  - a. Individual high efficiency split-system heat pumps with rooftop mounted condensers.
  - b. Grouped very high efficiency Variable Refrigerant Flow (VFR) systems, also with rooftop mounted condensers.

Note that the rendered images and massing model views that have been submitted for this project's Civic Design Review continue to show rectangular 2'w x 3'h through-wall louvers on building facades. Using either of the above-identified high-efficiency systems will eliminate the need for such mechanical louvers, which will produce a cleaner overall imagery for the completed project.

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Carlisle St, Looking South



Dauphin St, Looking East



N. 15th St, Looking South



Dauphin St, Looking West

1412-26 W. Dauphin Street



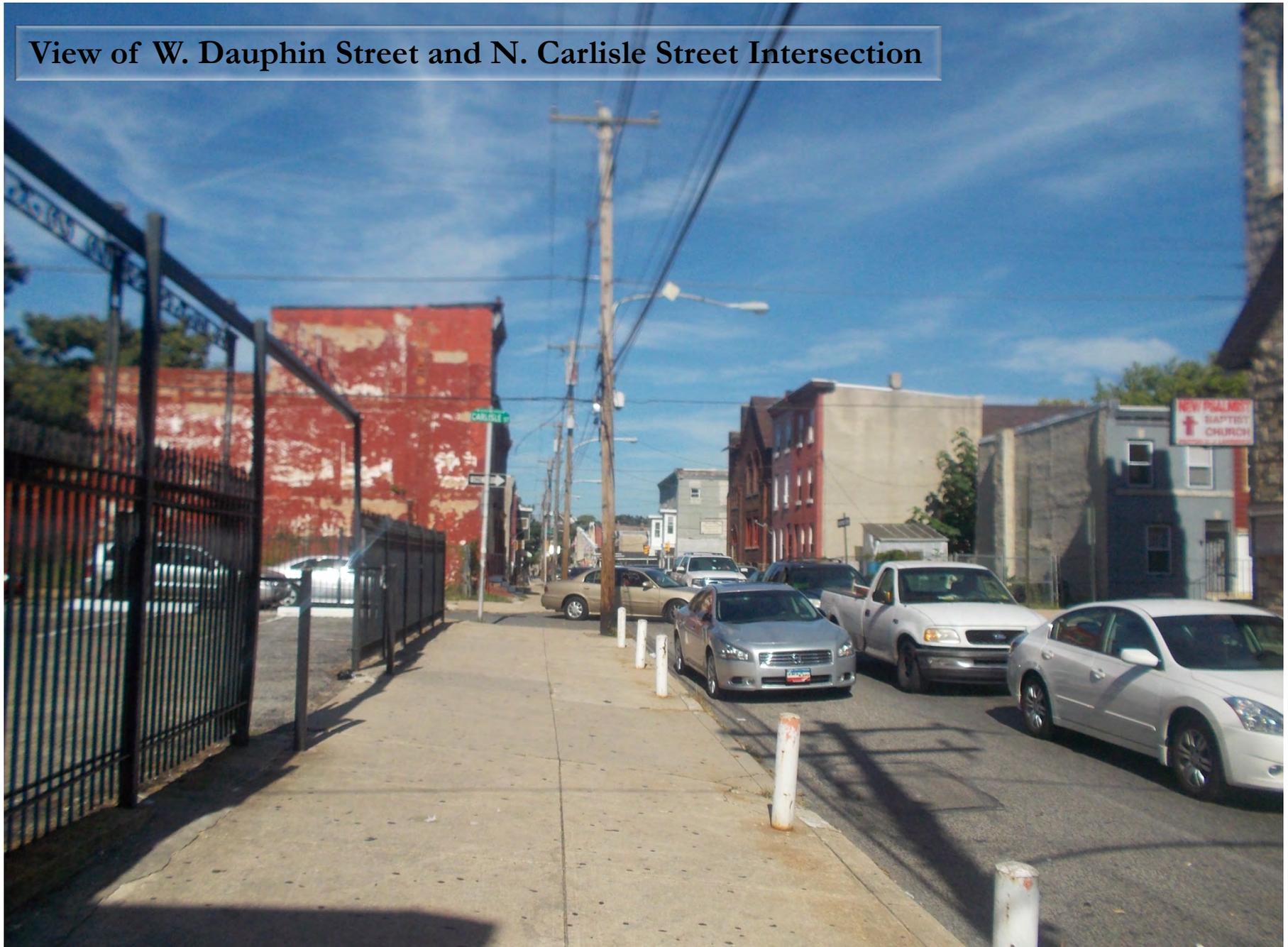
View of W. Dauphin Street Towards N. 15<sup>th</sup> Street



View Across from Property on W. Dauphin Street



View of W. Dauphin Street and N. Carlisle Street Intersection



View of Property and N. Carlisle Street



View of 1412-26 W. Dauphin Street Existing Frontage



View to Right of Property on W. Dauphin Street



View to Right of Property on W. Dauphin Street



View of W. Dauphin Street from N. 15<sup>th</sup> Street



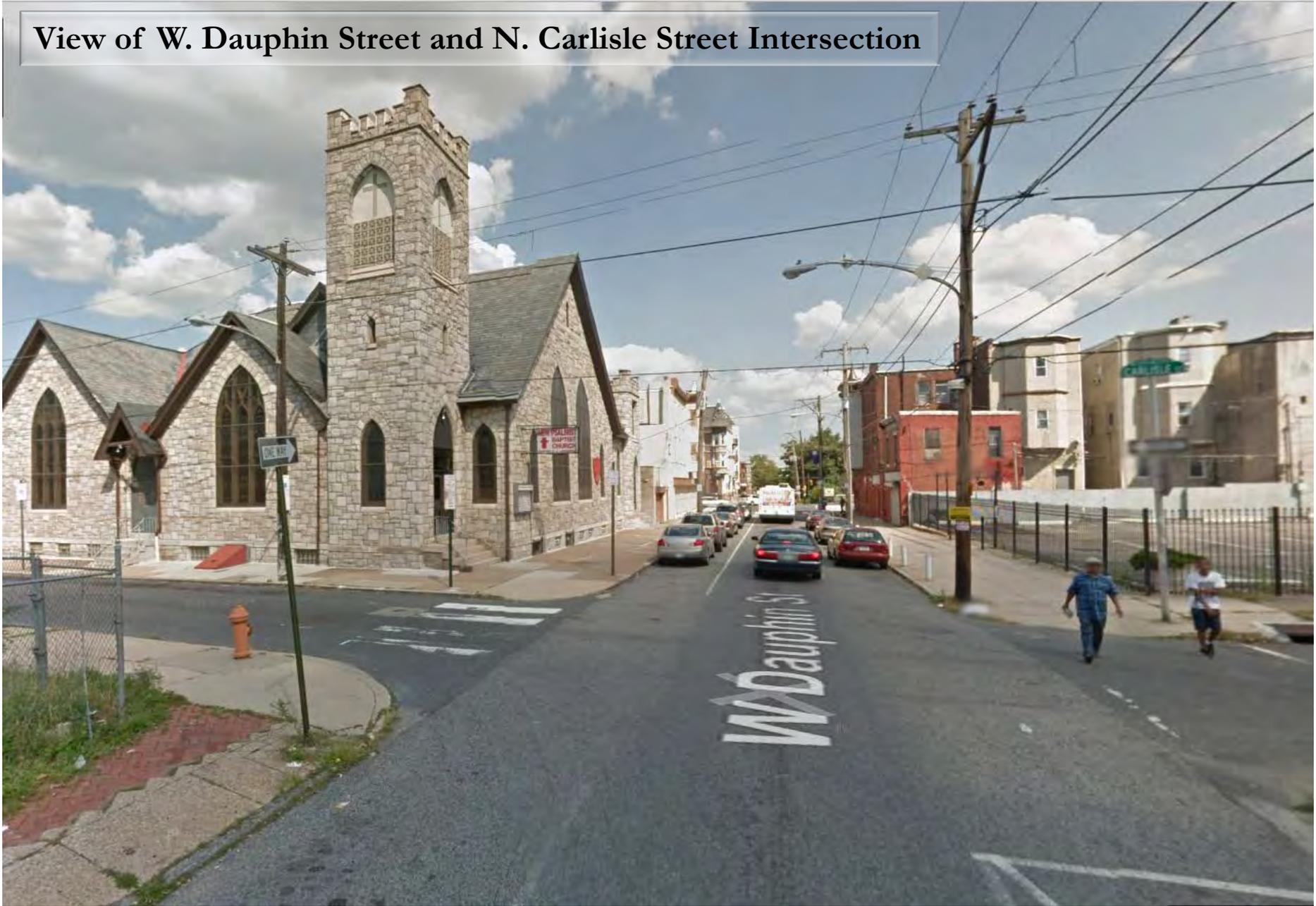
View Across from Property on W. Dauphin Street



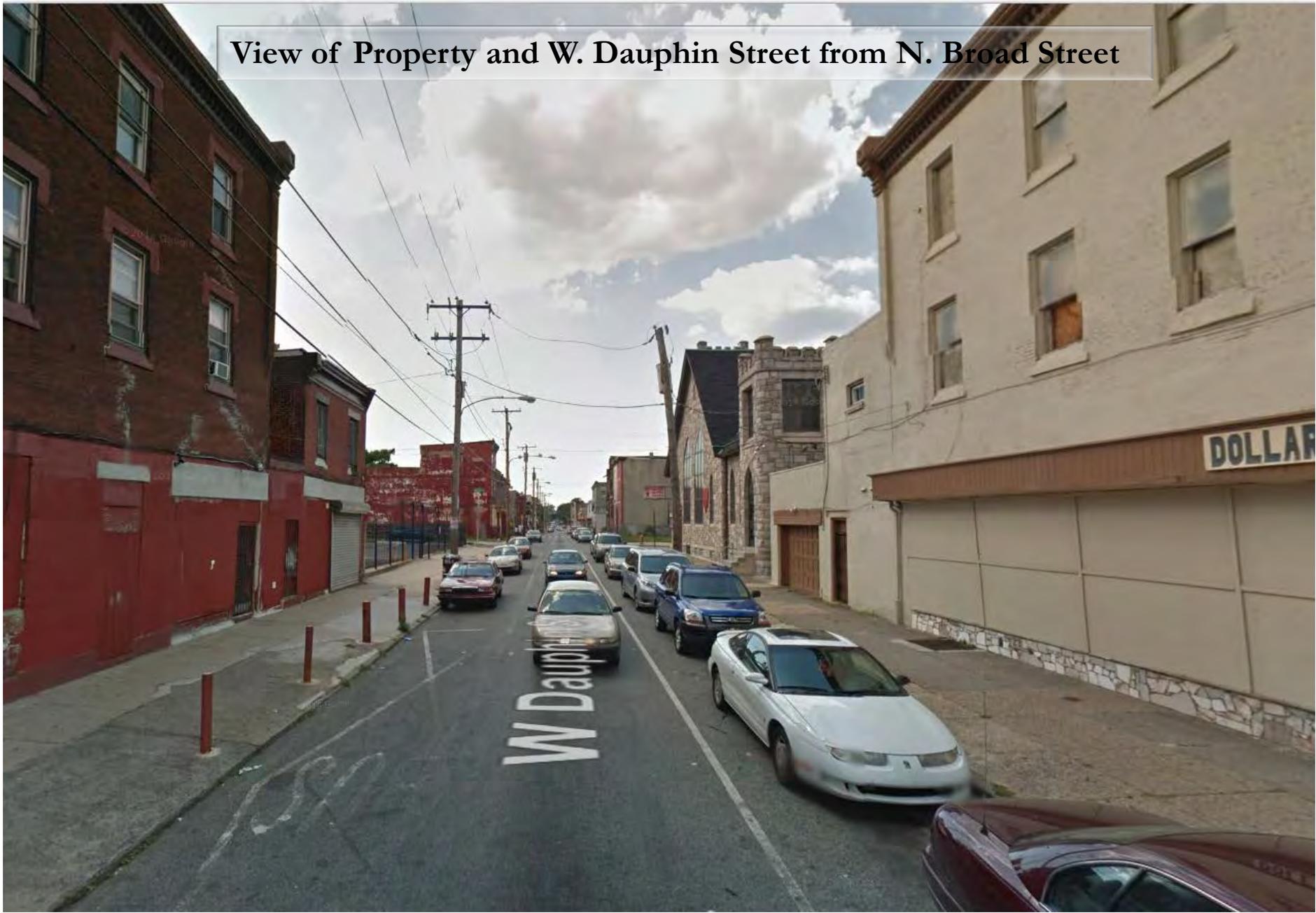
View Across from Property on W. Dauphin Street



View of W. Dauphin Street and N. Carlisle Street Intersection



View of Property and W. Dauphin Street from N. Broad Street



View Across from Property on N. Carlisle Street



View Across from Property on N. Carlisle Street



View of N. Carlisle Street from W. Dauphin Street



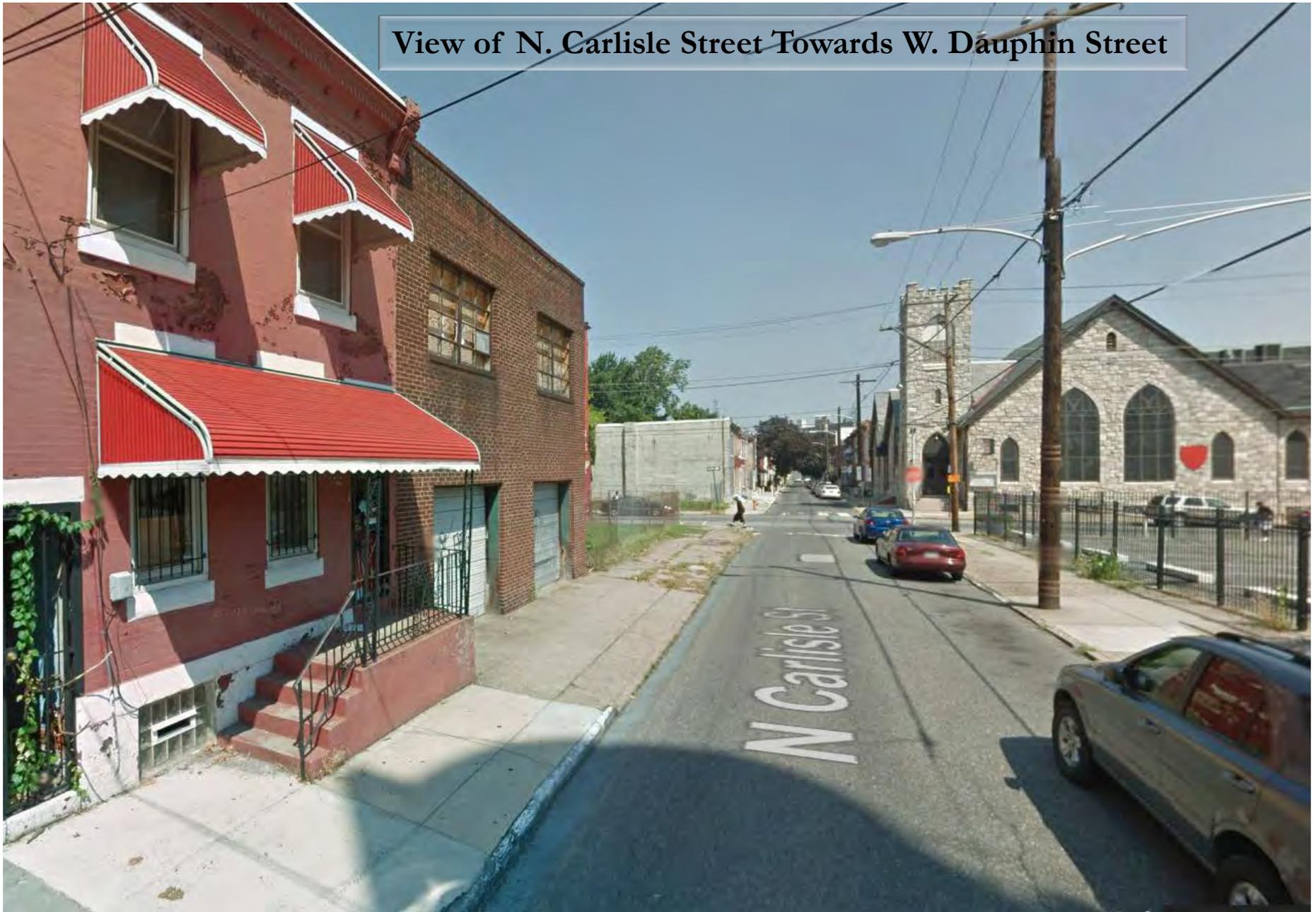
View of Property from N. Carlisle Street



View of Property from N. Carlisle Street



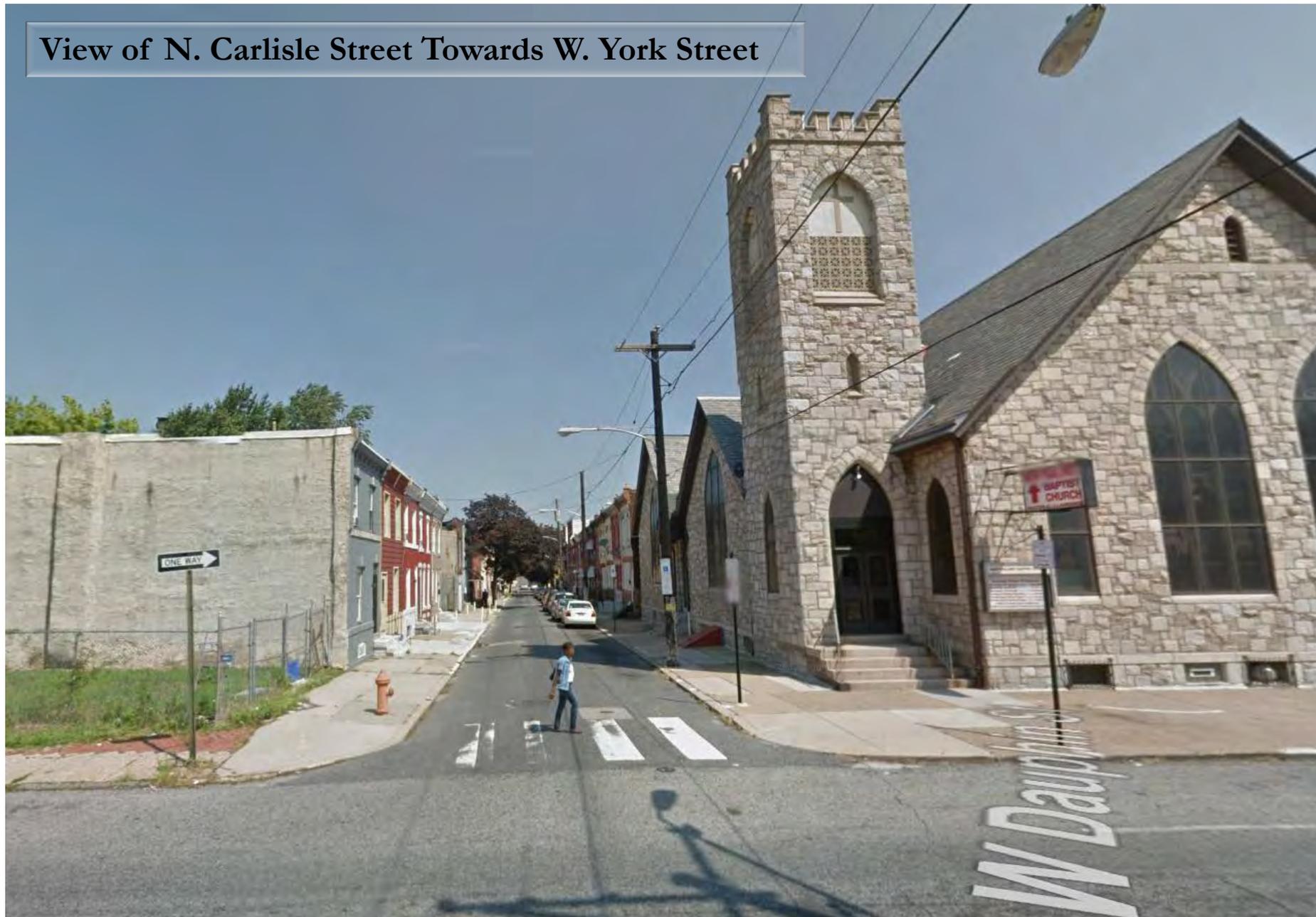
View of N. Carlisle Street Towards W. Dauphin Street



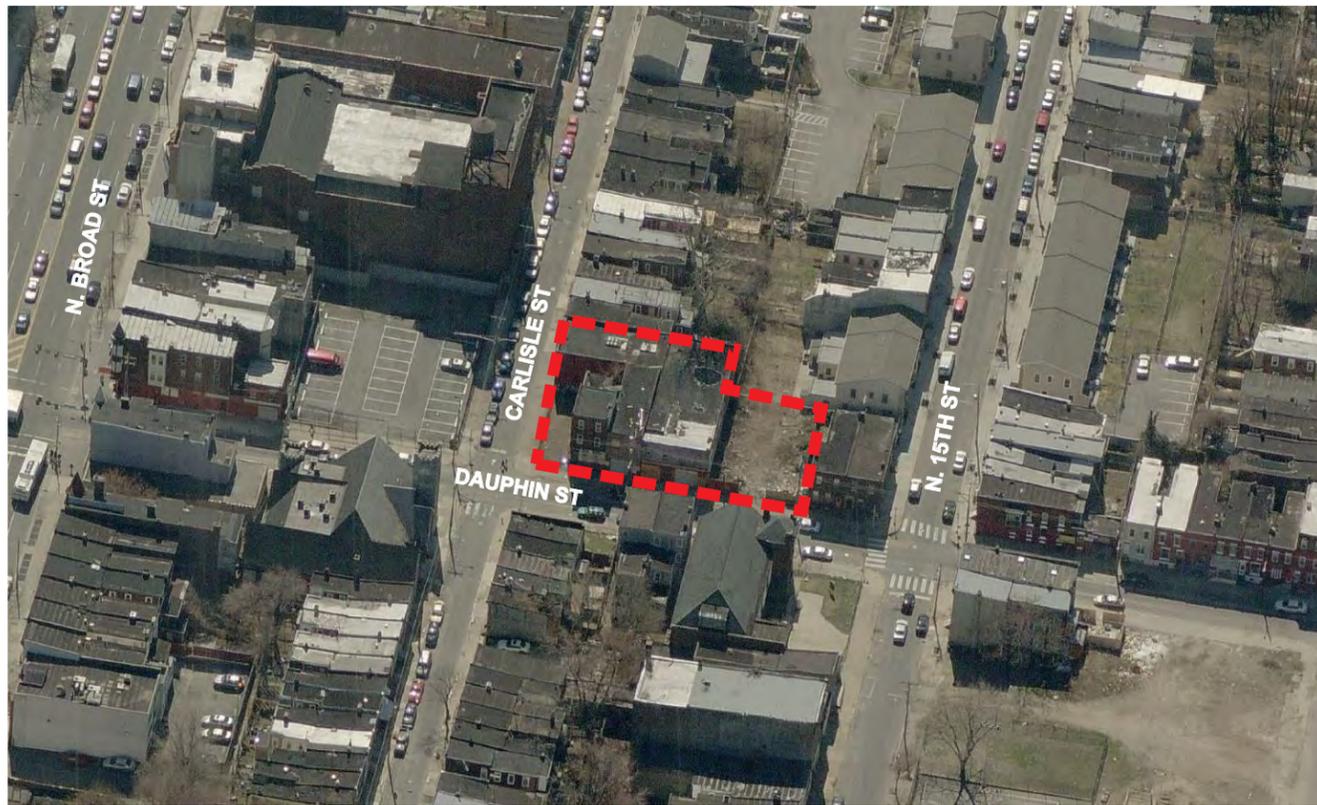
View of W. Dauphin Street from N. Carlisle Street

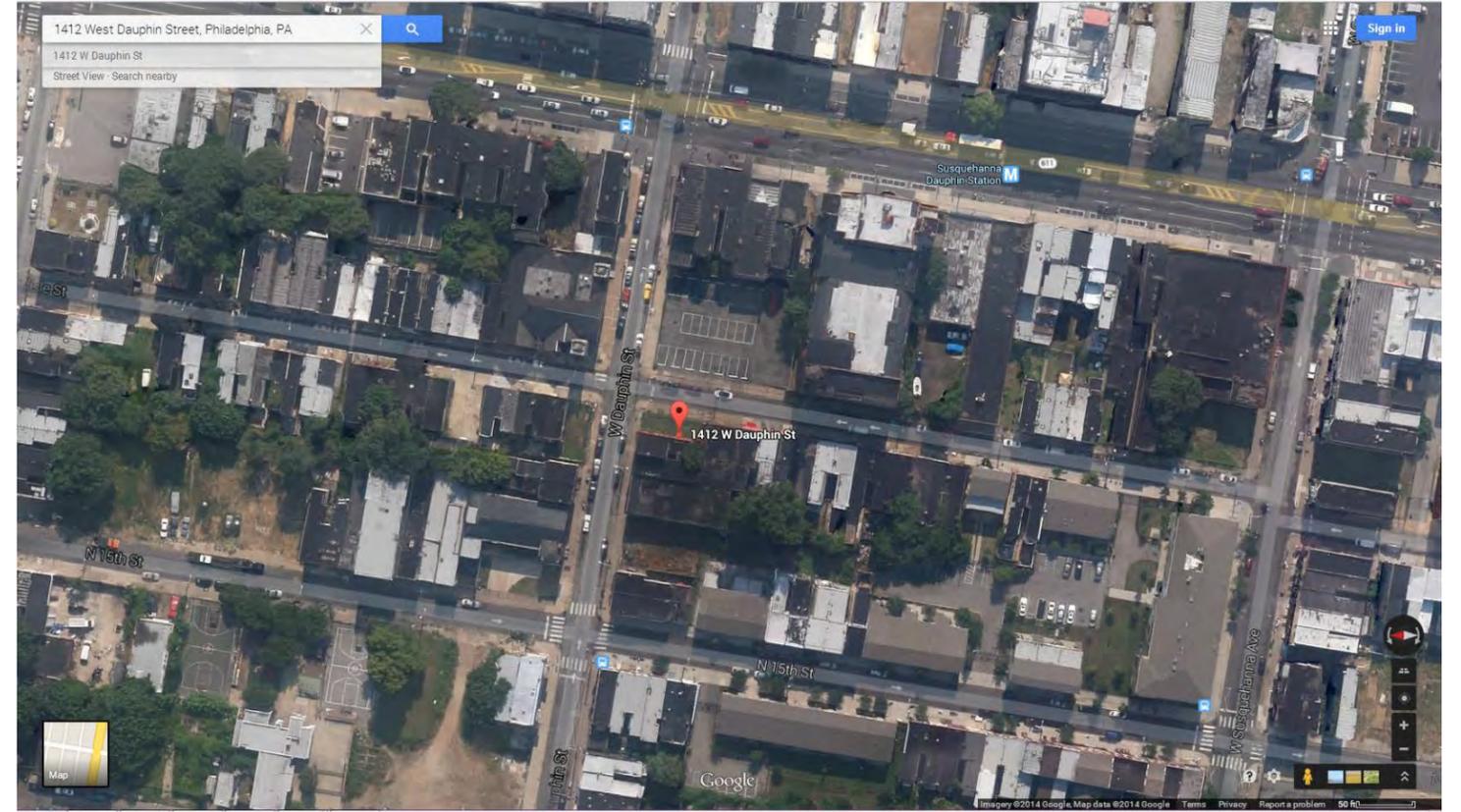
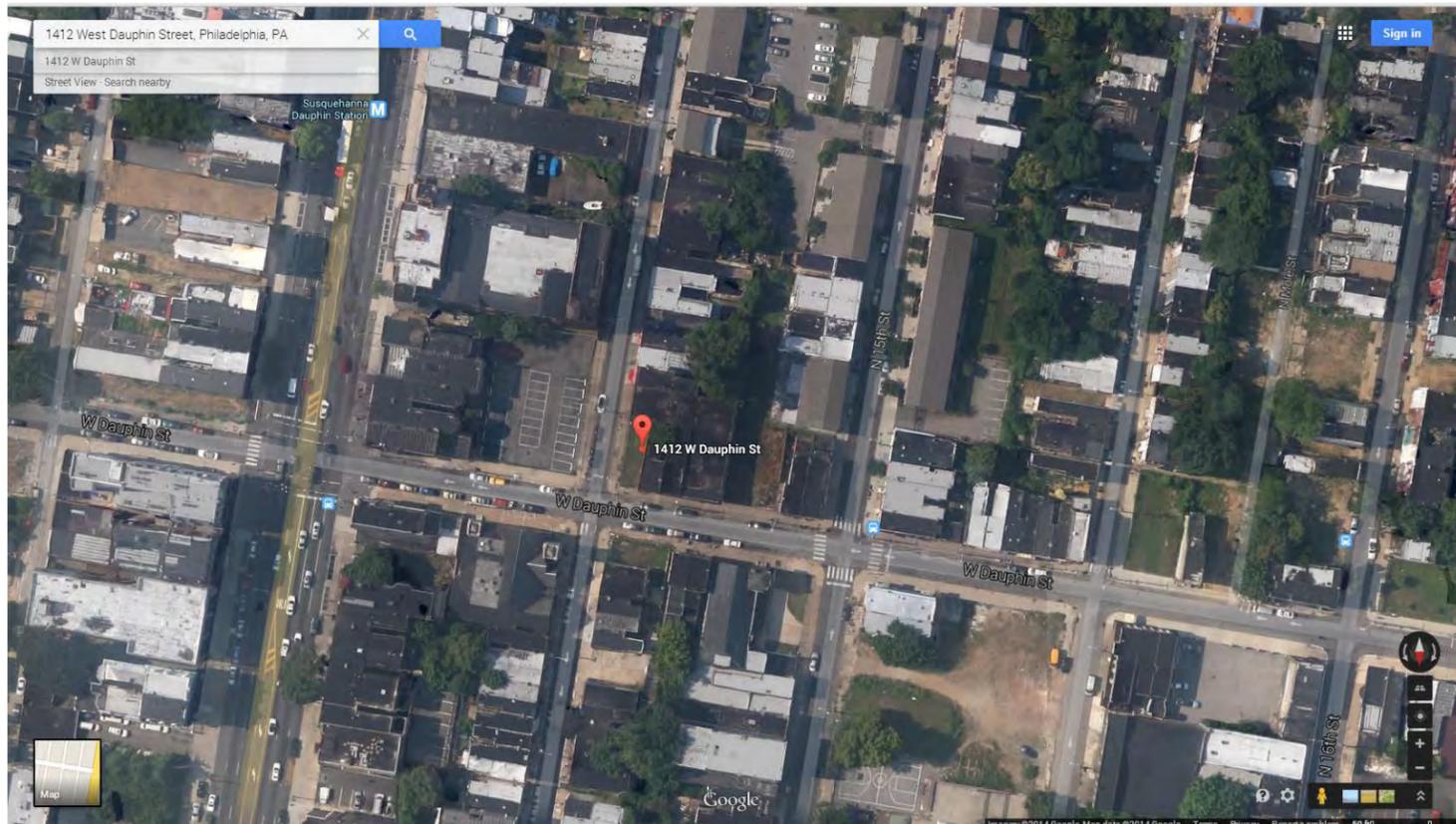
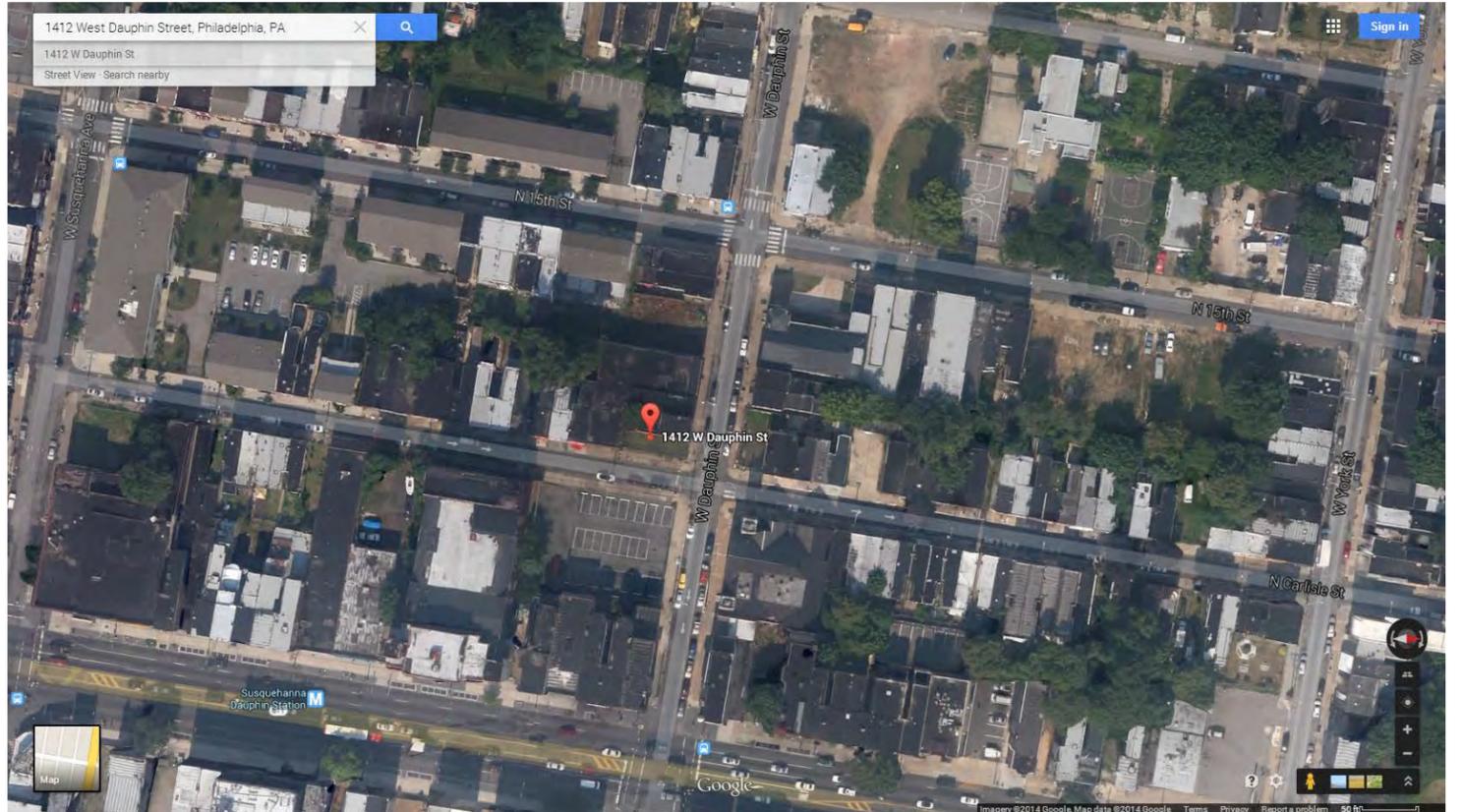
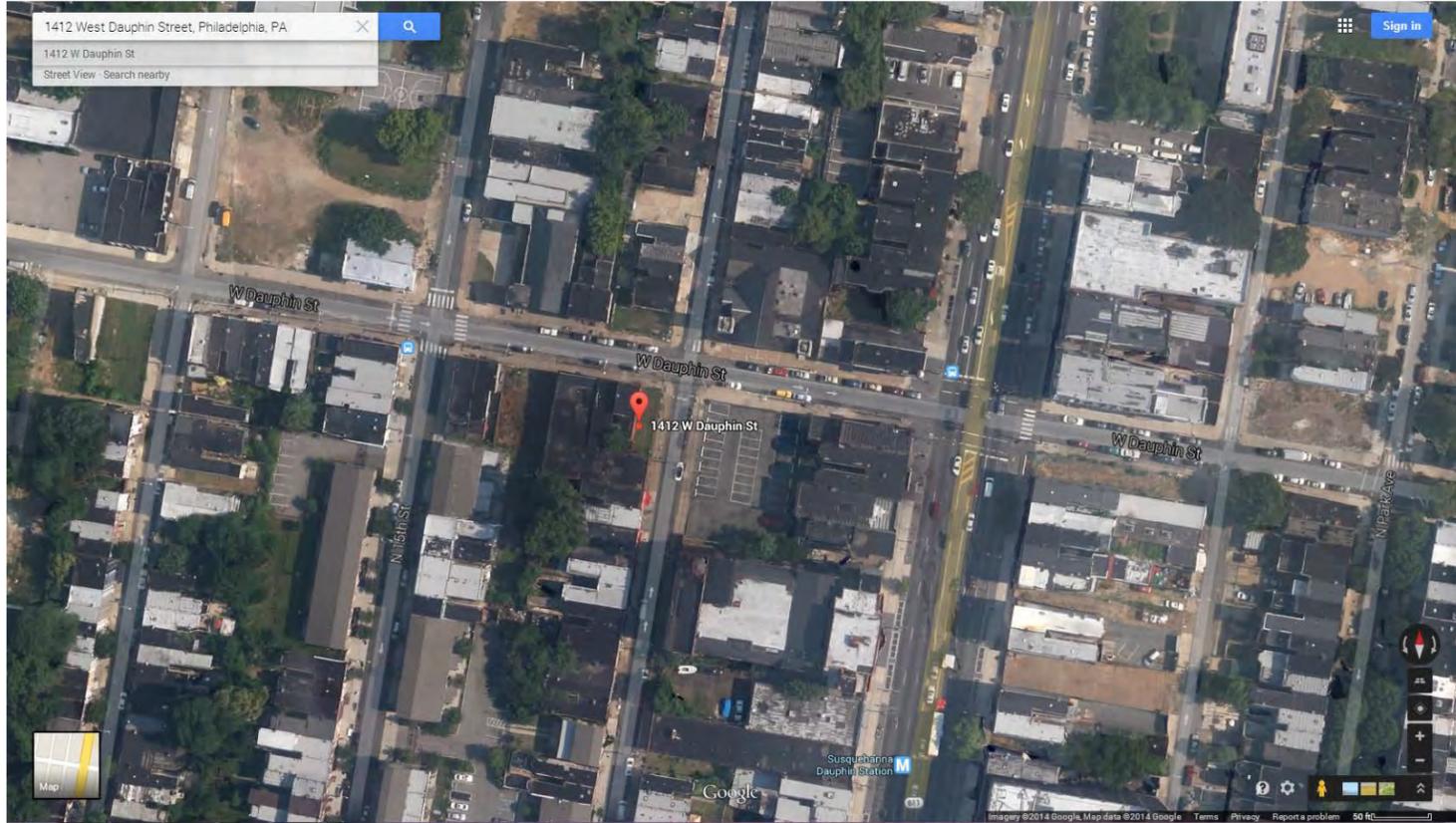


View of N. Carlisle Street Towards W. York Street

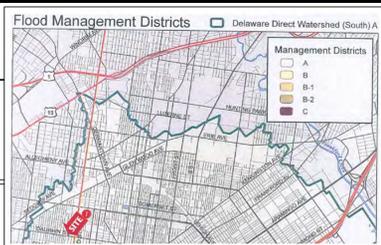
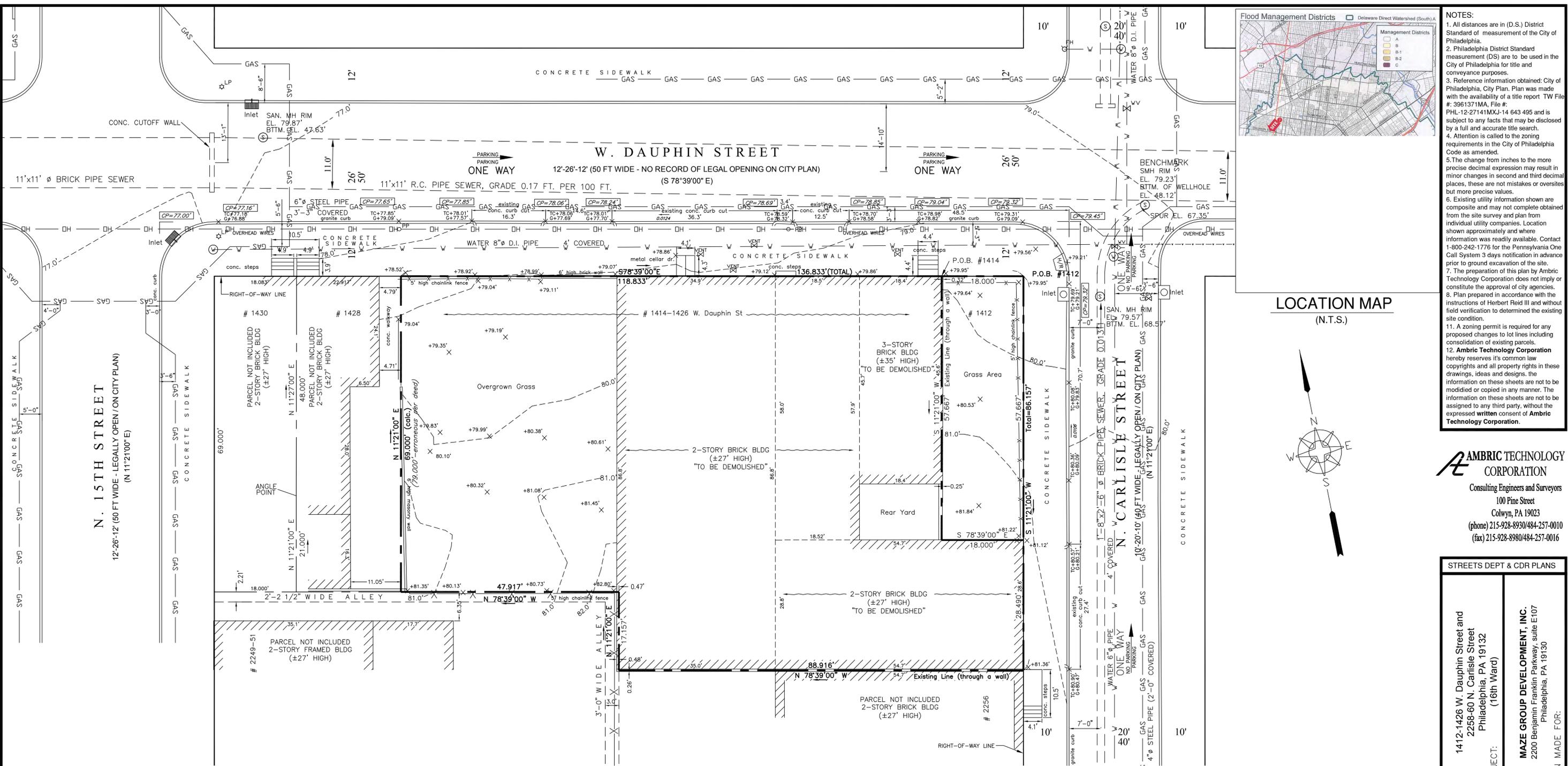


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**NOTES:**

- All distances are in (D.S.) District Standard of measurement of the City of Philadelphia.
- Philadelphia District Standard measurement (DS) are to be used in the City of Philadelphia for title and conveyance purposes.
- Reference information obtained: City of Philadelphia, City Plan. Plan was made with the availability of a title report TV File #: 3961371MA, File #: PHL-12-27141MXJ-14 643 495 and is subject to any facts that may be disclosed by a full and accurate title search.
- Attention is called to the zoning requirements in the City of Philadelphia Code as amended.
- The change from inches to the more precise decimal expression may result in minor changes in second and third decimal places, these are not mistakes or oversites but more precise values.
- Existing utility information shown are composite and may not be complete obtained from the site survey and plan from individual utility companies. Location shown approximately and where information was readily available. Contact 1-800-242-1776 for the Pennsylvania One Call System 3 days notification in advance prior to ground excavation of the site.
- The preparation of this plan by Ambric Technology Corporation does not imply or constitute the approval of city agencies.
- Plan prepared in accordance with the instructions of Herbert Reid III and without field verification to determine the existing site condition.
- A zoning permit is required for any proposed changes to lot lines including consolidation of existing parcels.
- Ambric Technology Corporation hereby reserves its common law copyrights and all property rights in these drawings, ideas and designs. The information on these sheets are not to be modified or copied in any manner. The information on these sheets are not to be assigned to any third party, without the expressed written consent of Ambric Technology Corporation.

**LOCATION MAP**  
(N.T.S.)



**AMBRIC TECHNOLOGY CORPORATION**  
Consulting Engineers and Surveyors  
100 Pine Street  
Colwyn, PA 19023  
(phone) 215-928-8930/484-257-0010  
(fax) 215-928-8980/484-257-0016

**STREETS DEPT & CDR PLANS**

1412-1426 W. Dauphin Street and  
2256-60 N. Carlisle Street  
Philadelphia, PA 19132  
(16th Ward)

**PROJECT:**  
**MAZE GROUP DEVELOPMENT, INC.**  
2200 Benjamin Franklin Parkway, Suite E107  
Philadelphia, PA 19130

PLAN MADE FOR:

**ZONING: CMX-2 (Commercial District):**

AREA REGULATIONS	REQUIRED / ALLOWED
Occupied Area	Intermediate Lots: < 75% of lot area; Corner Lots: < 80%.
Open Area	Intermediate Lots: > 25% of lot area; Corner Lots: > 20%.
Min. Front Yard Depth	No front yards shall be required
Min. Side Yard Width, Each	5 ft. if used
Min. Depth Rear Yard	the greater of 9 ft. or 10% of lot depth
Max. Height	38 ft.
D.U. (Site Density)	480 SF Min. Lot Area / D.U. (10,967-1919=9,048) 9,048 / 480 = 18.85 + 3 = 21 D.U.

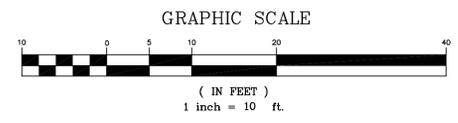
For information pertaining to bracketed numbers (e.g. "14") see Commercial District Dimensional Tables (b) notes for Table 14.701-3. For a definitive zoning decision, you must contact the Department of Licenses and Inspections.  
\* see Zoning Code, www.phila.gov for details and exceptions.

**EXISTING LOT:**

ADDRESS	# 1412 W. Dauphin St.	# 1414-26 W. Dauphin St.
Lot Area (sq. ft.)	1,038 sq. ft. (0.02383 Acre)	9,929 sq. ft. (0.22794 Acre)
Open Area (sq. ft.)	1,038 sq. ft.	3,221 sq. ft.
Occupied Area (sq. ft.)	0.0 sq. ft.	6,708 sq. ft.

**LEGEND:**

- (S) ST./SAN. SEWER MANHOLE
  - (W) WATER MANHOLE
  - (X) TRAFFIC LIGHT POLE
  - (X) UNKNOWN VALVE
  - (X) WATER VALVE
  - (X) POWER UTILITY POLE W/ LIGHT
  - (X) FIRE HYDRANT
  - (X) GAS VALVE
  - (---) LOT LINES
  - (---) CHAINLINK FENCE
  - (---) WOOD FENCE
  - (---) WATER MAINS
- 79.000' EXISTING LOT DIMENSION—ERRONEOUS
  - S 28°14'19" W 146.725' PROPOSED LOT BEARING AND DIMENSION
  - (T.B.R.) TO BE REMOVED
  - TC TOP CURB
  - G GUTTER
  - P.O.B. POINT OF BEGINNING
  - X 100.00+ EXISTING SPOT ELEVATION
  - X 102.02 CALCULATED CITY PLAN (TOP CURB EL.)
  - CONC. CONCRETE
  - EXIST. EXISTING
  - (C) CITY INLET
  - (H/R) EXISTING HANDICAPPED RAMP



PWD TRACKING NO. 2014-1412-2554-01

REV.	DATE	REVISION
1	06/13/14	PER CITY SURVEYORS REVIEW
2	08/11/14	SHEET NUMBERING / EAS PLAN

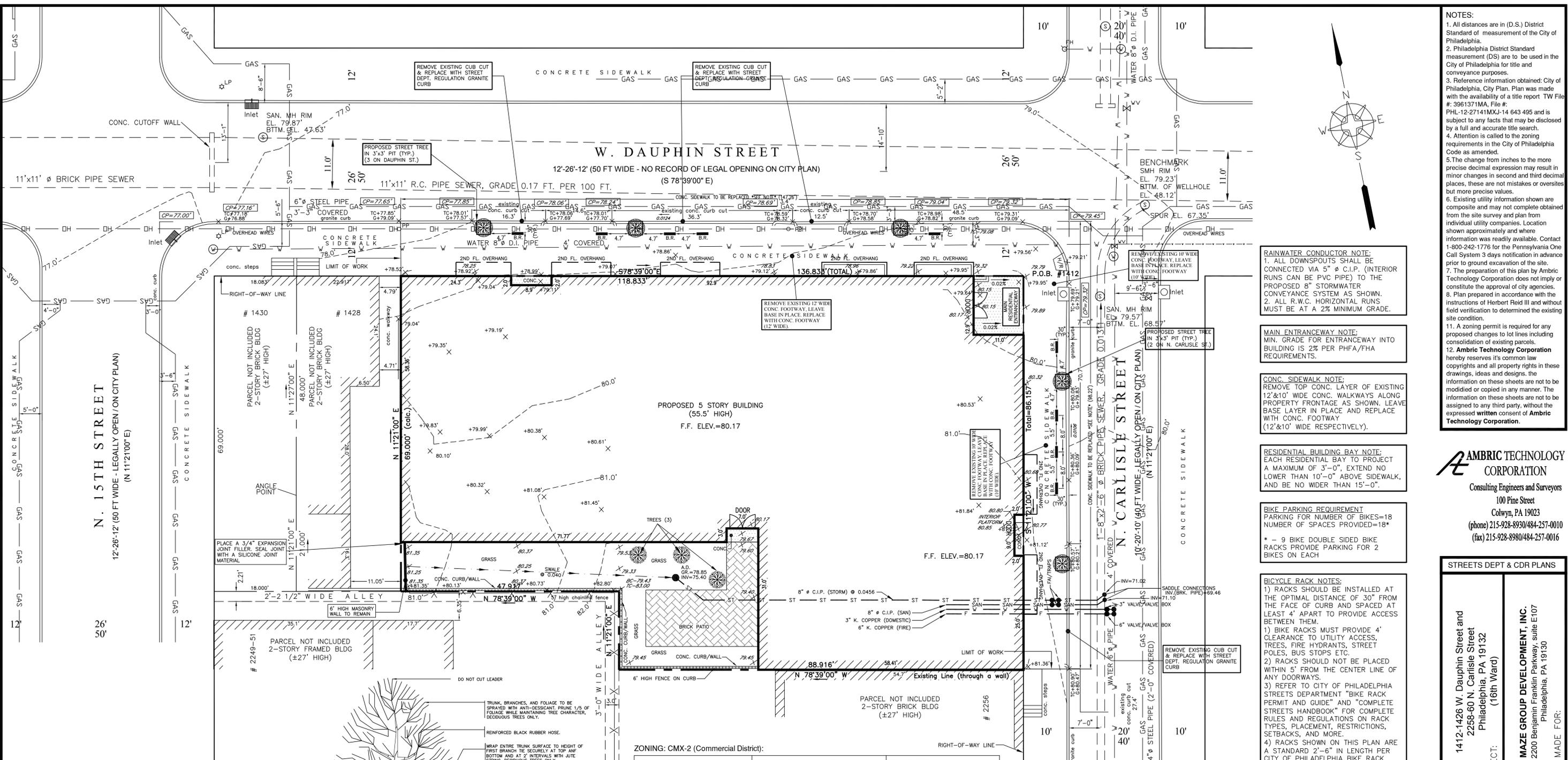
  

DESIGNED MK	PROPOSED TITLE: <b>TOPOGRAPHIC SURVEY AND EXISTING CONDITIONS PLAN</b>
DRAWN PC	
CHECKED MK	
APPROVED MK	
Marcus Kaplan, PLS 011417-E	

SCALE: 1" = 10'	SHEET NO.:
PROJECT #: S14-5388	1
DATE: 05/29/14	

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**RAINWATER CONDUCTOR NOTE:**  
 1. ALL DOWNSPOUTS SHALL BE CONNECTED VIA 5" C.I.P. (INTERIOR RUNS CAN BE PVC PIPE) TO THE PROPOSED 8" STORMWATER CONVEYANCE SYSTEM AS SHOWN.  
 2. ALL R.W.C. HORIZONTAL RUNS MUST BE AT A 2% MINIMUM GRADE.

**MAIN ENTRANCEWAY NOTE:**  
 MIN. GRADE FOR ENTRANCEWAY INTO BUILDING IS 2% PER PHFA/FHA REQUIREMENTS.

**CONC. SIDEWALK NOTE:**  
 REMOVE TOP CONC. LAYER OF EXISTING 12'x10' WIDE CONC. WALKWAYS ALONG PROPERTY FRONTAGE AS SHOWN. LEAVE BASE LAYER IN PLACE AND REPLACE WITH CONC. FOOTWAY (12'x10' WIDE RESPECTIVELY).

**RESIDENTIAL BUILDING BAY NOTE:**  
 EACH RESIDENTIAL BAY TO PROJECT A MAXIMUM OF 3'-0". EXTEND NO LOWER THAN 10'-0" ABOVE SIDEWALK, AND BE NO WIDER THAN 15'-0".

**BIKE PARKING REQUIREMENT:**  
 PARKING FOR NUMBER OF BIKES=18  
 NUMBER OF SPACES PROVIDED=18\*  
 \* - 9 BIKE DOUBLE SIDED BIKE RACKS PROVIDE PARKING FOR 2 BIKES ON EACH

**BIKE RACK NOTES:**  
 1) RACKS SHOULD BE INSTALLED AT THE OPTIMAL DISTANCE OF 30" FROM THE FACE OF CURB AND SPACED AT LEAST 4' APART TO PROVIDE ACCESS BETWEEN THEM.  
 2) RACKS SHOULD NOT BE PLACED WITHIN 5' FROM THE CENTER LINE OF ANY DOORWAYS.  
 3) REFER TO CITY OF PHILADELPHIA STREETS DEPARTMENT "BIKE RACK PERMIT AND GUIDE" AND "COMPLETE STREETS HANDBOOK" FOR COMPLETE RULES AND REGULATIONS ON RACK TYPES, PLACEMENT, RESTRICTIONS, SETBACKS, AND MORE.  
 4) RACKS SHOWN ON THIS PLAN ARE A STANDARD 2'-6" IN LENGTH PER CITY OF PHILADELPHIA BIKE RACK APPLICATION DIAGRAM, PG. 7.

**ZONING: CMX-2 (Commercial District):**

AREA REGULATIONS	REQUIRED / ALLOWED	PROPOSED
Occupied Area	Intermediate Lots: < 75% of lot area; Corner Lots: < 80%.	9,209 / 10,967 = 84.0%
Open Area	Intermediate Lots: > 25% of lot area; Corner Lots: > 20%.	1,758 / 10,967 = 16.0%
Min. Front Yard Depth	No front yards shall be required	No front yards
Min. Side Yard Width, Each	5 ft. if used	No side yards
Min. Depth Rear Yard	the greater of 9 ft. or 10% of lot depth	10 ft. or greater
Max. Height	38 ft.	55.5 ft. to top of main roof structural deck
D.U. (Site Density)	480 SF Min. Lot Area / D.U. (10,967-1919-9,048) 9,048 / 480 = 18.85 + 3 = 21 D.U.	54 D.U.

**EXISTING LOT:**

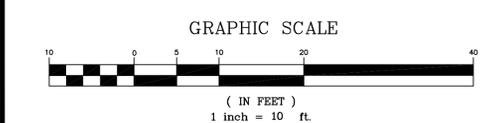
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Occupied Area (sq. ft.)	0.0 sq. ft.	6,708 sq. ft.	9,209 sq. ft.

**PROPOSED CONSOLIDATED LOT:**

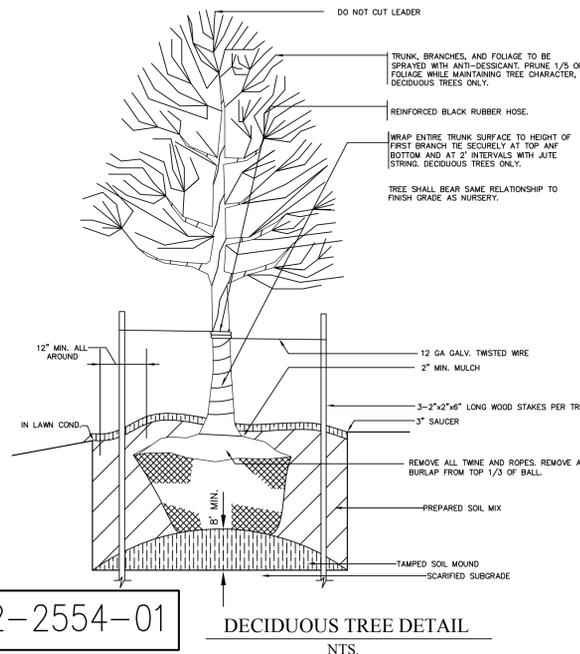
FLOOR	TOTAL GSF
1	9,209
2	9,516
3	9,516
4	9,516
5	9,516
TOTAL	47,273

**LEGEND:**

ST./SAN. SEWER MANHOLE	GR.	GRATE
WATER MANHOLE	INV.	INVERT
TRAFFIC LIGHT POLE	X 100.00+	EXISTING SPOT ELEVATION
UNKNOWN VALVE	X 100.00	CALCULATED SPOT ELEVATION (TOP CURB EL.)
WATER VALVE	X 100.00	PROPOSED SPOT ELEVATION
POWER UTILITY POLE W/ LIGHT	CONC.	CONCRETE
FIRE HYDRANT	F.F.	FINISHED FLOOR
GAS VALVE	EXIST.	EXISTING
LOT LINES	PROP.	PROPOSED
CHAINLINK FENCE	0.36	SLOPE DIRECTION
WOOD FENCE		
WATER MAINS		
TO BE REMOVED		
TC		
G		
P.O.B.		
C.I.P.		
A.D.		
B.R.		
		PROP. FRESH AIR INLET / TRAP CURB STOP / WET TAP
		CITY INLET
		PROP. SAN SEWER LATERAL
		PROP. STORM WATER LINE
		PROP. DOMESTIC WATER LATERAL
		PROP. FIRE WATER LATERAL
		PROP. STREET TREE W/ 3'x3' PIT (TYP.)
		PROP. REAR YARD TREE
		OVERHANG (2nd - 3rd Fl.)
		OVERHANG (2nd - 4th Fl.)



**NOTE:**  
 STREET TREES MUST BE PERMITTED BY THE PHILADELPHIA DEPARTMENT OF PARKS & RECREATION. CONTACT THE STREET TREE MANAGEMENT DIVISION AT (215) 685-4363.



PWD TRACKING NO. 2014-1412-2554-01

**AMBRIC TECHNOLOGY CORPORATION**  
 Consulting Engineers and Surveyors  
 100 Pine Street  
 Colwyn, PA 19023  
 (phone) 215-928-8930/484-257-0010  
 (fax) 215-928-8980/484-257-0016

**STREETS DEPT & CDR PLANS**

1412-1426 W. Dauphin Street and 2256-60 N. Carlisle Street Philadelphia, PA 19132 (16th Ward)

**PROJECT:** MAZE GROUP DEVELOPMENT, INC. 2200 Benjamin Franklin Parkway, suite E107 Philadelphia, PA 19130

PLAN MADE FOR:

REV.	DATE	REVISION
1	06/13/14	PER CITY SURVEYORS REVIEW
2	08/11/14	SHEET NUMBERING / EAS PLAN

DESIGNED: MK  
 DRAWN: PC  
 CHECKED: MK  
 APPROVED: MK

**SHEET TITLE:** SITE DEVELOPMENT & UTILITY PLAN

SCALE: 1" = 10'  
 PROJECT #: 9510-ENG-14  
 DATE: 05/29/14

SHEET NO.: 2

**7**

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## INSTRUCTIONS

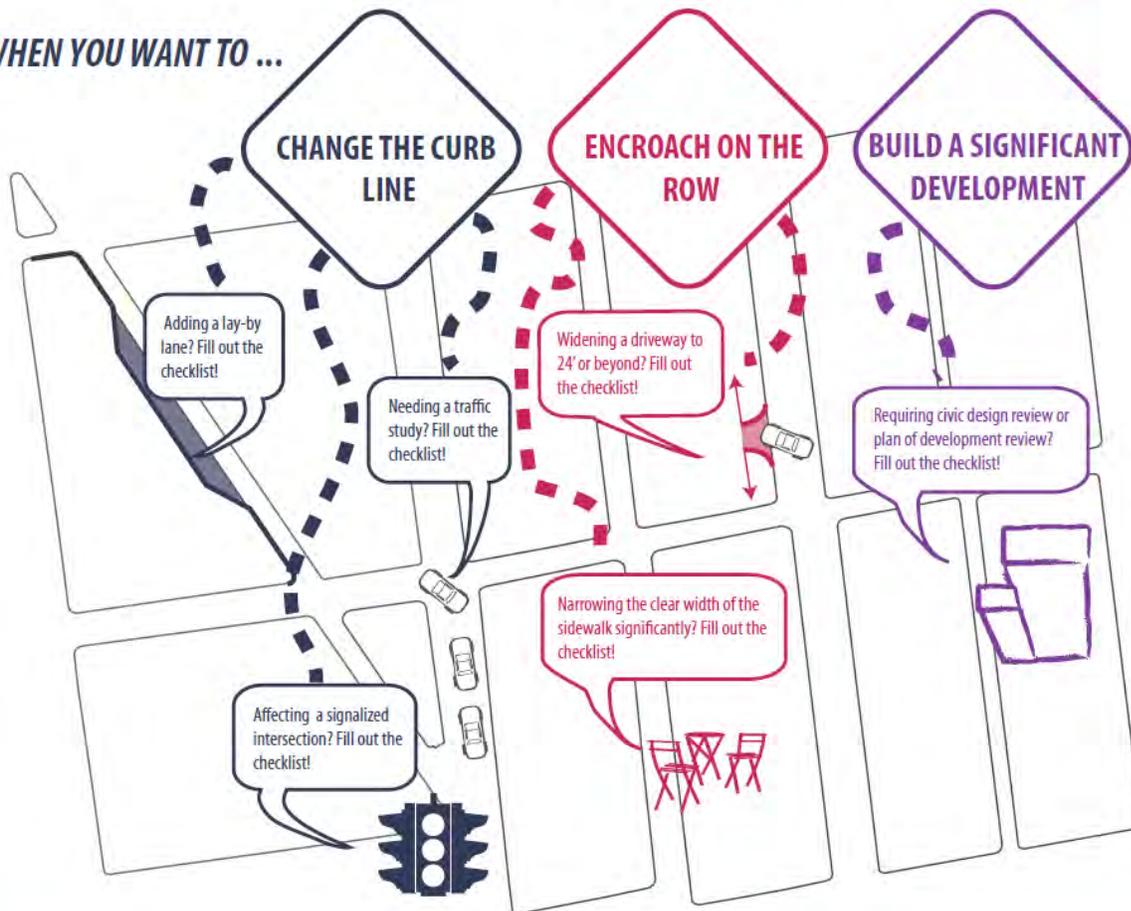
This Checklist is an implementation tool of the *Philadelphia Complete Streets Handbook* (the “Handbook”) and enables City engineers and planners to review projects for their compliance with the Handbook’s policies. The handbook provides design guidance and does not supersede or replace language, standards or policies established in the City Code, City Plan or Manual on Uniform Traffic Control Devices (MUTCD).

This checklist is used to document how project applicants considered and accommodated the needs of all users of city streets during the planning and/or design of projects affecting the public right-of-way. Departmental reviewers will use this checklist to confirm that submitted designs incorporate complete streets considerations (see §11-901 of The Philadelphia Code). Applicants for projects that require Civic Design Review or Plan of Development Review shall complete this checklist and attach it to plans submitted to the Philadelphia City Planning Commission for review.

The Handbook can be accessed at <http://philadelphiastreet.com/complete-streets.aspx>.

## WHEN DO I NEED TO FILL OUT THE COMPLETE STREETS CHECKLIST?

WHEN YOU WANT TO ...



# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## INSTRUCTIONS (continued)

### APPLICANTS ARE ADVISED TO NOTE:

- This checklist is designed to be filled out electronically. Text fields will expand automatically as you type.
- This checklist is estimated to take 60–90 minutes to complete for applicants familiar with the Handbook.
- Answering “No” or “Not Applicable” (N/A) to questions in this checklist does not result in an automatic denial of approval. Applicants shall provide adequate explanation and comments to justify any such responses in the space provided at the end the checklist.
- All plans submitted for review must clearly dimension the widths of the Furnishing, Walking, and Building Zones (as defined in Section 1 of the Handbook). “High Priority” Complete Streets treatments (identified in Table 1 and subsequent sections of the Handbook) should be identified and dimensioned on plans.
- All plans submitted for review must clearly identify and site all street furniture, including but not limited to bus shelters, street signs and hydrants.
- Any project that calls for the development and installation of medians, bio-swales and other such features in the right-of-way may require a maintenance agreement with the Streets Department.
- The coordination of all changes to the placement of street furniture (including but not limited to bus shelters, street signs and hydrants) is the responsibility of the developer throughout all phases of the project, from planning and design to construction management.
- ADA curb-ramp designs must be submitted to the City for review
- Any project that significantly changes the curb line may require a City Plan Action. The City Plan Action Application is available at [http://philadelphiastreet.com/pds/City\\_Plan\\_Applicatio.pdf](http://philadelphiastreet.com/pds/City_Plan_Applicatio.pdf). An application to the Streets Department for a City Plan Action is required when a project plan proposes the:
  - Placing of a new street;
  - Removal of an existing street;
  - Changes to roadway grades, curb lines, or widths; or
  - Placing or striking a city utility right-of-way.

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## GENERAL PROJECT INFORMATION

1. PROJECT NAME  
\_\_\_\_\_ 1412-26 W. Dauphin St.
2. DATE  
\_\_\_\_\_ 08/18/2014
3. APPLICANT NAME  
\_\_\_\_\_ 1400 Dauphin Associates, LLC.
5. PROJECT AREA: list precise street limits and scope  
\_\_\_\_\_ 1412-26 W. Dauphin St.  
\_\_\_\_\_ & 2850-52 N. Carlisle St.
4. APPLICANT CONTACT INFORMATION  
\_\_\_\_\_ 1341 N. Delaware Ave., Suite 204  
\_\_\_\_\_ Philadelphia, PA 19125
6. OWNER NAME  
\_\_\_\_\_ 1400 Dauphin Associates, LLC.
7. OWNER CONTACT INFORMATION  
\_\_\_\_\_ 1341 N. Delaware Ave., Suite 204  
\_\_\_\_\_ Philadelphia, PA 19125  
Phone: 215-751-0205  
Email: hjreid@mazegroup.com
8. ENGINEER / ARCHITECT NAME  
\_\_\_\_\_ Ambric Technology Corporation  
cc: drbeauvais@zarwin.com  
mlferleger@zarwin.com
9. ENGINEER / ARCHITECT CONTACT INFORMATION  
\_\_\_\_\_ 100 Pine St.  
\_\_\_\_\_ Colwyn, PA 19023  
Phone: 215-928-8930  
Email: phil.ega@gmail.com
10. 10. STREETS: List the streets associated with the project. Complete Street Types can be found at [www.phila.gov/map](http://www.phila.gov/map) under the "Transportation and Utilities" field. Complete Street Types are also identified in Section 3 of the Handbook.

STREET	FROM	TO	COMPLETE STREET TYPE
W. Dauphin St.	N. 15th St.	N. Carlisle St.	"City Neighborhood"
N. Carlisle St.	W. Dauphin St.	W. Susquehanna Ave.	"City Neighborhood"
_____	_____	_____	_____
_____	_____	_____	_____

11. Does the Existing Conditions site survey clearly identify the following existing conditions?
  - a. Parking and loading regulations in curb lanes adjacent to the site YES  NO
  - b. Street Furniture such as bus shelters, honor boxes, etc. YES  NO  N/A
  - c. Street Direction YES  NO
  - d. Curb Cuts YES  NO  N/A
  - e. Utilities, including tree grates, vault covers, manholes, junction boxes, signs, lights, poles, etc. YES  NO
  - f. Building Extensions into the sidewalk, such as stairs and stoops YES  NO  N/A

### APPLICANT: General Project Information

Additional Explanation / Comments: \_\_\_\_\_

### DEPARTMENTAL REVIEW: General Project Information

Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## PEDESTRIAN COMPONENT (Handbook Section 4.3)

12. SIDEWALK: list Sidewalk widths for each street frontage. Required Sidewalk widths are listed in Section 4.3 of the Handbook.

STREET FRONTAGE	TYPICAL SIDEWALK WIDTH (BUILDING LINE TO CURB)	CITY PLAN SIDEWALK WIDTH
	Required / Existing / Proposed	Existing / Proposed
W. Dauphin St.	12' / 12' / 12'	12' / 12'
N. Carlisle St.	12' / 10' / 10'	10' / 10'
_____	____ / ____ / ____	____ / ____
=====	===== / ===== / =====	===== / =====

13. WALKING ZONE: list Walking Zone widths for each street frontage. The Walking Zone is defined in Section 4.3 of the Handbook, including required widths.

STREET FRONTAGE	WALKING ZONE
	Required / Existing / Proposed
W. Dauphin St.	6' / 5.9' / 6'
N. Carlisle St.	5' / 10' / 5'
_____	____ / ____ / ____
=====	===== / ===== / =====

14. VEHICULAR INTRUSIONS: list Vehicular Intrusions into the sidewalk. Examples include but are not limited to; driveways, lay-by lanes, etc. Driveways and lay-by lanes are addressed in sections 4.8.1 and 4.6.3, respectively, of the Handbook.

### EXISTING VEHICULAR INTRUSIONS

INTRUSION TYPE	INTRUSION WIDTH	PLACEMENT
none	_____	_____
_____	_____	_____
_____	_____	_____
=====	=====	=====

### PROPOSED VEHICULAR INTRUSIONS

INTRUSION TYPE	INTRUSION WIDTH	PLACEMENT
none	_____	_____
_____	_____	_____
_____	_____	_____
=====	=====	=====

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## PEDESTRIAN COMPONENT (continued)

15. Does the design limit block lengths to 500 feet or less?

YES  NO  N/A

16. When considering the overall design, does the design create a pedestrian environment that provides safe and comfortable access for all pedestrians?

YES  NO

### DEPARTMENTAL APPROVAL

YES  NO

YES  NO

**APPLICANT: Pedestrian Component** Existing Pedestrian Component on Dauphin St. is limited by a 1.7' intrusion by utility poles and 4.4' from existing  
Additional Explanation / Comments: ~~steps~~ coming from existing dwelling. (Therefore 5.9')

### DEPARTMENTAL REVIEW: Pedestrian Component

Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## BUILDING & FURNISHING COMPONENT (Handbook Section 4.4)

17. BUILDING ZONE: list the MAXIMUM, existing and proposed Building Zone width on each street frontage. The Building Zone is defined as the area of the sidewalk immediately adjacent to the building face, wall, or fence marking the property line, or a lawn in lower density residential neighborhoods. The Building Zone is further defined in section 4.4.1 of the Handbook.

STREET FRONTAGE	MAXIMUM BUILDING ZONE WIDTH	
	Existing / Proposed	
W. Dauphin St.	4.4'	3'
N. Carlisle St.	0'	2'
_____	_____	_____
_____	_____	_____

18. FURNISHING ZONE: list the MINIMUM, recommended, existing, and proposed Furnishing Zone widths on each street frontage. The Furnishing Zone is further defined in section 4.4.2 of the Handbook.

STREET FRONTAGE	MINIMUM FURNISHING ZONE WIDTH		
	Recommended / Existing / Proposed		
W. Dauphin St.	3'	2'	3'
N. Carlisle St.	3'	0'	3'
_____	_____	_____	_____
_____	_____	_____	_____

19. Identify proposed "high priority" building and furnishing zone design treatments that are incorporated into the design plan, where width permits (see Handbook Table 1). Are the following treatments identified and dimensioned on the plan?

- Bicycle Parking
- Lighting
- Benches
- Street Trees
- Street Furniture

YES  NO  N/A   
 YES  NO  N/A

### DEPARTMENTAL APPROVAL

YES  NO   
 YES  NO   
 YES  NO   
 YES  NO   
 YES  NO   
 YES  NO   
 YES  NO

20. Does the design avoid tripping hazards?

YES  NO  N/A

YES  NO

21. Does the design avoid pinch points? Pinch points are locations where the Walking Zone width is less than the required width identified in item 13, or requires an exception

YES  NO  N/A

YES  NO

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## BUILDING & FURNISHING COMPONENT (continued)

- |   |  |  |
|---|--|--|
| 22. Do street trees and/or plants comply with street installation requirements (see sections 4.4.7 & 4.4.8)                         | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/> | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 23. Does the design maintain adequate visibility for all roadway users at intersections?  | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>                              | YES <input type="checkbox"/> NO <input type="checkbox"/> |
| 24. When considering the overall design of the Building & Furnishing Component, does the design enhance the pedestrian environment? | YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>                              | YES <input type="checkbox"/> NO <input type="checkbox"/> |

**APPLICANT: Building & Furnishing Component** Existing Furnishing Components along Dauphin St. are utility poles for overhead wires with attached street lights. No street trees or other typical furnishing components exist along Dauphin St.

### DEPARTMENTAL REVIEW: Building & Furnishing Component

Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## BICYCLE COMPONENT (Handbook Section 4.5)

25. List elements of the project that incorporate recommendations of the Pedestrian and Bicycle Plan, located online at <http://phila2035.org/wp-content/uploads/2012/06/bikePedfinal2.pdf>

Bike racks

26. List the existing and proposed number of bicycle parking spaces, on- and off-street. Bicycle parking requirements are provided in The Philadelphia Code, Section 14-804.

BUILDING / ADDRESS	REQUIRED SPACES	ON SIDEWALK OR STREET	OFF-STREET
		Existing / Proposed	Existing / Proposed
1412-26 W. Dauphin St.	_____	0 / 18	_____ / _____
_____	_____	_____ / _____	_____ / _____
_____	_____	_____ / _____	_____ / _____
_____	_____	_____ / _____	_____ / _____

27. Identify proposed "high priority" bicycle design treatments (see Handbook Table 1) that are incorporated into the design plan, where width permits. Are the following "High Priority" elements identified and dimensioned on the plan?

- Conventional Bike Lane
- Buffered Bike Lane
- Bicycle-Friendly Street

YES  NO  N/A

YES  NO  N/A

YES  NO  N/A

**DEPARTMENTAL APPROVAL**

YES  NO

YES  NO

YES  NO

YES  NO

YES  NO

28. Does the design provide bicycle connections to local bicycle, trail, and transit networks?

YES  NO

29. Does the design provide convenient bicycle connections to residences, work places, and other destinations?

YES  NO

**APPLICANT: Bicycle Component** No existing bike lanes, bicycle friendly streets or connections to trails or transit networks currently exist  
 Additional Explanation / Comments: on W. Dauphin or N. Carlisle St. Proposed on W. Dauphin St. are 5 bike racks and 4 bike racks on N. Carlisle St in the sidewalk "furnishing" area, accommodating 2 bicycles on each rack.

**DEPARTMENTAL REVIEW: Bicycle Component**  
 Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## CURBSIDE MANAGEMENT COMPONENT (Handbook Section 4.6)

			DEPARTMENTAL APPROVAL	
			YES	NO
30.	Does the design limit conflict among transportation modes along the curb?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
31.	Does the design connect transit stops to the surrounding pedestrian network and destinations?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
32.	Does the design provide a buffer between the roadway and pedestrian traffic?	YES <input checked="" type="checkbox"/> NO <input type="checkbox"/> N/A <input type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
33.	How does the proposed plan affect the accessibility, visibility, connectivity, and/or attractiveness of public transit? Adding Street Trees improves the visual characteristics and attractiveness of the area and the proposed bike racks help improve the accessibility and connectivity for those not using personal vehicles.		YES <input type="checkbox"/>	NO <input type="checkbox"/>

**APPLICANT: Curbside Management Component** There are transit (bus) stops located at 15th & W. Dauphin St., Broad & W. Dauphin. On Broad St, between Dauphin & W. Susquehanna is a BSL subway entrance.

### DEPARTMENTAL REVIEW: Curbside Management Component

Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## VEHICLE / CARTWAY COMPONENT (Handbook Section 4.7)

34. For each street frontage, identify existing and proposed lane widths and the design speed.

STREET	FROM	TO	LANE WIDTHS		DESIGN SPEED
			Existing	Proposed	
W. Dauphin St.	N. 15th St.	N. Carlisle St.	26'	26'	25mph
N. Carlisle St.	W. Susquehanna Ave.	W. Dauphin St.	20'	20'	25mph
_____	_____	_____	_____	_____	_____
_____	_____	_____	_____	_____	_____

35. What is the maximum AASHTO design vehicle being accommodated by the design? 25mph

### DEPARTMENTAL APPROVAL

YES  NO

36. Will the project affect a historically certified street? An [inventory of historic streets](#)<sup>(1)</sup> is maintained by the Philadelphia Historical Commission.

YES  NO

YES  NO

37. Does the design plan incorporate roadway medians (a "high priority" vehicle / cartway design treatment for some street types)?

YES  NO  N/A

YES  NO

*\*Any proposed median may require a maintenance agreement with the Streets Department.*

38. Does the design facilitate safe and accessible, deliveries to local industries and businesses?

YES  NO

YES  NO

39. Will the public right-of-way be used for loading and unloading activities?

YES  NO

YES  NO

40. Does the design maintain emergency vehicle access?

YES  NO

YES  NO

41. Where new streets are being developed, does the design connect and extend the street grid?

YES  NO  N/A

YES  NO

42. Does the design support multiple alternative routes to and from destinations as well as within the site?

YES  NO  N/A

YES  NO

43. Overall, does the design balance vehicle mobility with the mobility and access of all other roadway users?

YES  NO

YES  NO

### APPLICANT: Vehicle / Cartway Component

Additional Explanation / Comments: \_\_\_\_\_

### DEPARTMENTAL REVIEW: Vehicle / Cartway Component

Reviewer Comments: \_\_\_\_\_

(1) <http://www.phila.gov/historical/PDF/Historic%20Street%20Paving%20District%20Inventory.pdf>

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## URBAN DESIGN COMPONENT (Handbook Section 4.8)

			DEPARTMENTAL APPROVAL	
44. Does the design incorporate windows, storefronts, and other active uses facing the street?	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
45. Does the proposed project have a Philadelphia Water Department (PWD) Work Number? If so, please provide.	<u>2014-1412-2554-01</u>			
46. List the stormwater management and drainage features incorporated into the design of the Right of Way (see Section 4.8.4).  _____	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
47. Does the design provide driveway access that safely manages pedestrian / bicycle conflicts with vehicles (see Section 4.8.1)?	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>
48. Does the design provide direct, safe, and accessible connections between transit stops and building access points and destinations within the site?	YES <input checked="" type="checkbox"/>	NO <input type="checkbox"/>	N/A <input type="checkbox"/>	YES <input type="checkbox"/> NO <input type="checkbox"/>

**APPLICANT: Urban Design Component** There are transit (bus) stops located at 15th & W. Dauphin St., Broad & W. Dauphin. On Broad St, between Dauphin & W. Susquehanna is a BSL subway entrance.

**DEPARTMENTAL REVIEW: Urban Design Component**  
Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## INTERSECTIONS & CROSSINGS COMPONENT (Handbook Section 4.9)

49. Identify Existing and Proposed Signal Cycle lengths

SIGNAL LOCATION	EXISTING CYCLE LENGTH	PROPOSED CYCLE LENGTH
_____	_____	_____
_____	_____	_____
_____	_____	_____
_____	_____	_____

	YES	NO	N/A	DEPARTMENTAL APPROVAL	
50. Does the design minimize the signal cycle length to reduce pedestrian wait time?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
51. Does the design provide adequate clearance time for pedestrians to cross streets?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
52. Does the design minimize pedestrian crossing distances by narrowing streets or travel lanes, extending curbs, reducing curb radii, or using medians or refuge islands to break up long crossings? <i>* If yes, City Plan Action may be required.</i>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
53. Identify "High Priority" intersection and crossing design treatments (see Handbook Table 1) that will be incorporated into the design, where width permits. Are the following "High Priority" design treatments identified and dimensioned on the plan?				YES <input type="checkbox"/>	NO <input type="checkbox"/>
▪ Marked Crosswalks	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
▪ Pedestrian Refuge Islands	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
▪ Signal Timing and Operation	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
▪ Bike Boxes	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
54. Does the plan simplify complex intersections where possible?	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
55. Does the design reduce vehicle speeds and increase visibility at intersections?	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>
56. Overall, do intersection designs limit conflicts between modes and promote pedestrian and bicycle safety?	YES <input type="checkbox"/>	NO <input type="checkbox"/>	N/A <input checked="" type="checkbox"/>	YES <input type="checkbox"/>	NO <input type="checkbox"/>

**APPLICANT: Intersections & Crossings Component**

Additional Explanation / Comments: Intersection of Dauphin & Carlisle Streets is controlled by stop signs and no design changes are proposed to the intersection.

**DEPARTMENTAL REVIEW: Intersections & Crossings Component**

Reviewer Comments: \_\_\_\_\_

# COMPLETE STREETS HANDBOOK CHECKLIST

Philadelphia City Planning Commission



## ADDITIONAL COMMENTS

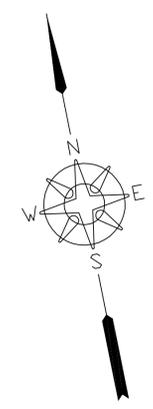
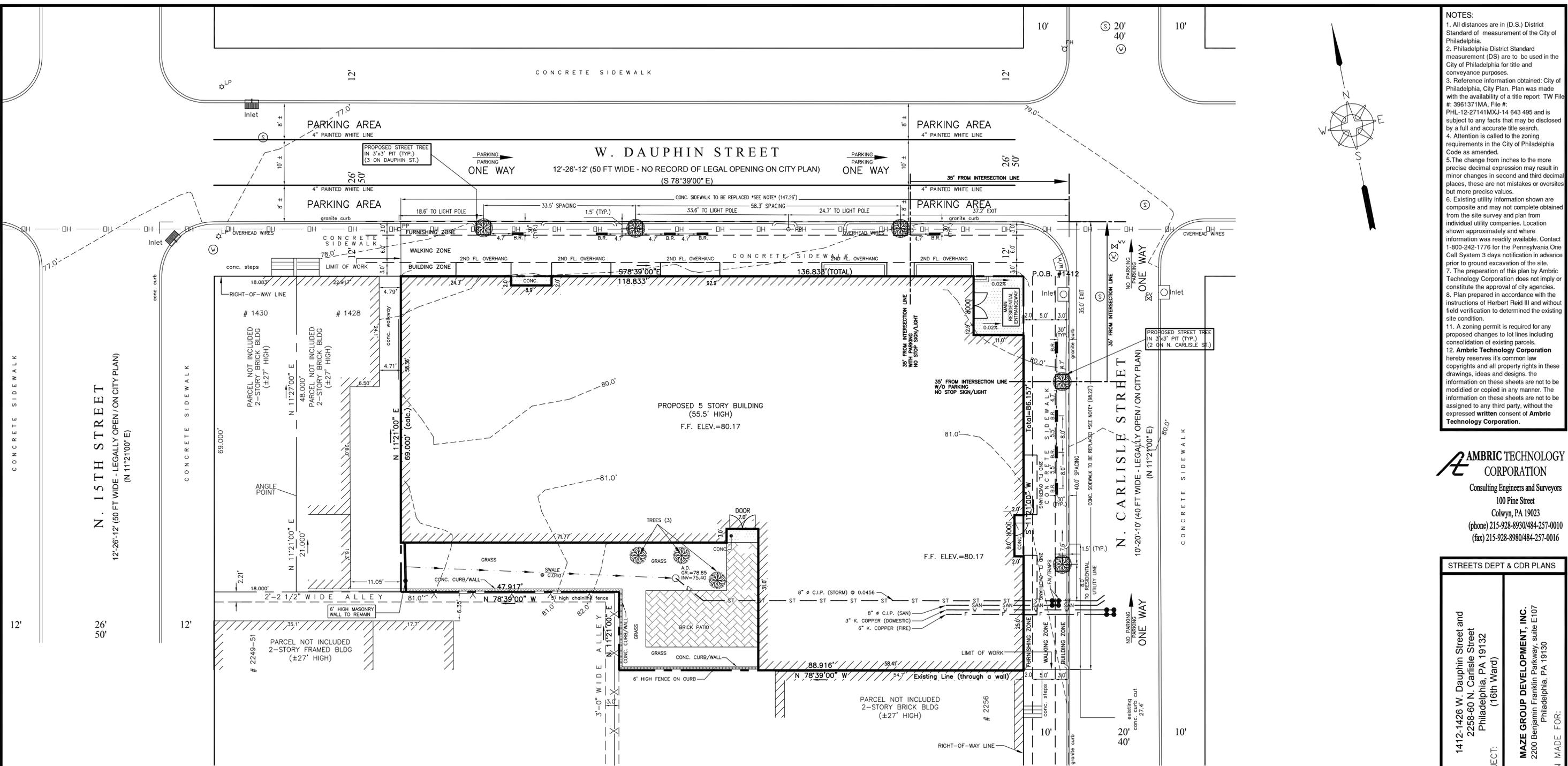
### APPLICANT

Additional Explanation / Comments: \_\_\_\_\_

### DEPARTMENTAL REVIEW

Additional Reviewer Comments: \_\_\_\_\_

8



**NOTES:**

- All distances are in (D.S.) District Standard of measurement of the City of Philadelphia.
- Philadelphia District Standard measurement (DS) are to be used in the City of Philadelphia for title and conveyance purposes.
- Reference information obtained: City of Philadelphia, City Plan. Plan was made with the availability of a title report TW File #: 3961371MA, File #: PHL-12-27141MXJ-14 643 495 and is subject to any facts that may be disclosed by a full and accurate title search.
- Attention is called to the zoning requirements in the City of Philadelphia Code as amended.
- The change from inches to the more precise decimal expression may result in minor changes in second and third decimal places, these are not mistakes or oversights but more precise values.
- Existing utility information shown are composite and may not complete obtained from the site survey and plan from individual utility companies. Location shown approximately and where information was readily available. Contact 1-800-242-1776 for the Pennsylvania One Call System 3 days notification in advance prior to ground excavation of the site.
- The preparation of this plan by Ambric Technology Corporation does not imply or constitute the approval of city agencies.
- Plan prepared in accordance with the instructions of Herbert Reid III and without field verification to determine the existing site condition.
- A zoning permit is required for any proposed changes to lot lines including consolidation of existing parcels.
- Ambric Technology Corporation hereby reserves its common law copyrights and all property rights in these drawings, ideas and designs. The information on these sheets are not to be modified or copied in any manner. The information on these sheets are not to be assigned to any third party, without the expressed written consent of Ambric Technology Corporation.

**AMBRIC TECHNOLOGY CORPORATION**  
 Consulting Engineers and Surveyors  
 100 Pine Street  
 Colwyn, PA 19023  
 (phone) 215-928-8930/484-257-0010  
 (fax) 215-928-8980/484-257-0016

**STREETS DEPT & CDR PLANS**

1412-1426 W. Dauphin Street and  
 2256-60 N. Carlisle Street  
 Philadelphia, PA 19132  
 (16th Ward)

**PROJECT:**  
**MAZE GROUP DEVELOPMENT, INC.**  
 2200 Benjamin Franklin Parkway, suite E107  
 Philadelphia, PA 19130

PLAN MADE FOR:

**RAINWATER CONDUCTOR NOTE:**  
 1. ALL DOWNSPOUTS SHALL BE CONNECTED VIA 5" Ø C.I.P. (INTERIOR RUNS CAN BE PVC PIPE) TO THE PROPOSED 8" STORMWATER CONVEYANCE SYSTEM AS SHOWN.  
 2. ALL R.W.C. HORIZONTAL RUNS MUST BE AT A 2% MINIMUM GRADE.

**RESIDENTIAL BUILDING BAY NOTE:**  
 EACH RESIDENTIAL BAY TO PROJECT A MAXIMUM OF 3'-0", EXTEND NO LOWER THAN 10'-0" ABOVE SIDEWALK, AND BE NO WIDER THAN 15'-0".

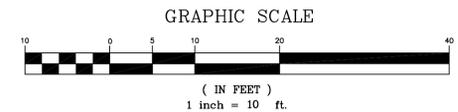
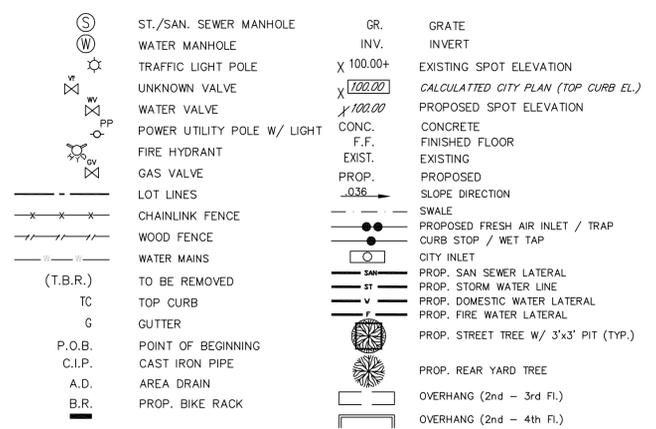
**BICYCLE RACK NOTES:**  
 1) RACKS SHOULD BE INSTALLED AT THE OPTIMAL DISTANCE OF 30" FROM THE FACE OF CURB AND SPACED AT LEAST 4' APART TO PROVIDE ACCESS BETWEEN THEM.  
 2) BIKE RACKS MUST PROVIDE 4" CLEARANCE TO UTILITY ACCESS, TREES, FIRE HYDRANTS, STREET POLES, BUS STOPS ETC.  
 3) RACKS SHOULD NOT BE PLACED WITHIN 5' FROM THE CENTER LINE OF ANY DOORWAYS.  
 4) REFER TO CITY OF PHILADELPHIA STREETS DEPARTMENT "BIKE RACK PERMIT AND GUIDE" AND "COMPLETE STREETS HANDBOOK" FOR COMPLETE RULES AND REGULATIONS ON RACK TYPES, PLACEMENT, RESTRICTIONS, SETBACKS, AND MORE.  
 5) RACKS SHOWN ON THIS PLAN ARE A STANDARD 2'-6" IN LENGTH PER CITY OF PHILADELPHIA BIKE RACK APPLICATION DIAGRAM, PG 7.

**CONC. SIDEWALK NOTE:**  
 REMOVE TOP CONC. LAYER OF EXISTING 12'x10' WIDE CONC. WALKWAYS ALONG PROPERTY FRONTAGE AS SHOWN. LEAVE BASE LAYER IN PLACE AND REPLACE WITH CONC. FOOTWAY (12'x10' WIDE RESPECTIVELY).

**BIKE PARKING REQUIREMENT**  
 PARKING FOR NUMBER OF BIKES=18  
 NUMBER OF SPACES PROVIDED=18\*

\* - 9 BIKE DOUBLE SIDED BIKE RACKS PROVIDE PARKING FOR 2 BIKES ON EACH

**LEGEND:**



PWD TRACKING NO. 2014-1412-2554-01

REV.	DATE	REVISION
1	06/13/14	PER CITY SURVEYORS REVIEW
2	08/11/14	SHEET NUMBERING / EAS PLAN

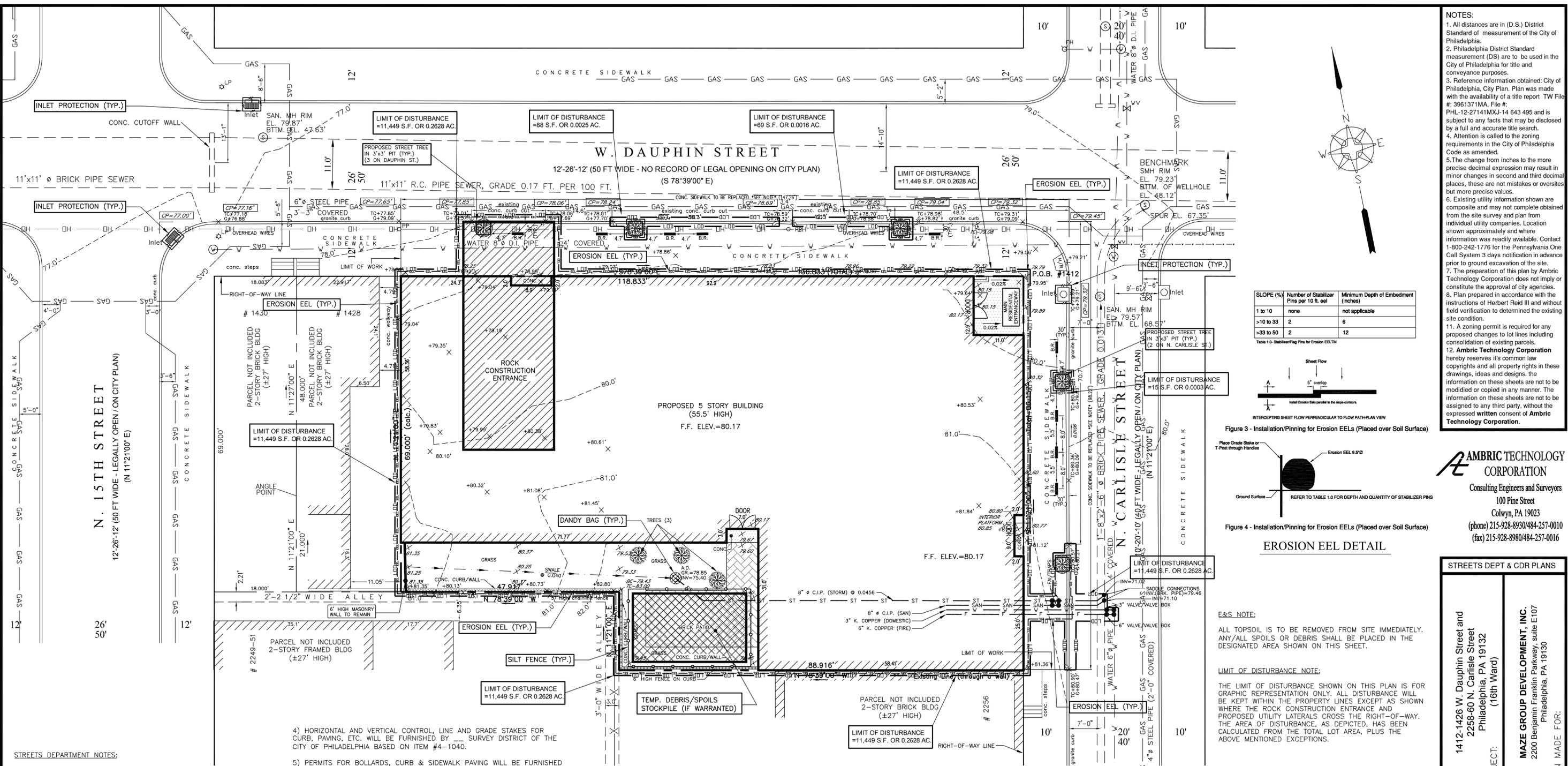
  

DESIGNED MK	<b>STREETS DEPARTMENT ZONING PLAN</b>
DRAWN PC	
CHECKED MK	
APPROVED MK	

SHEET TITLE:  
**STREETS DEPARTMENT ZONING PLAN**

SCALE: 1" = 10'	SHEET NO. <b>3</b>
PROJECT #: 9510-ENG-14	
DATE: 05/29/14	

9



**NOTES:**

- All distances are in (D.S.) District Standard of measurement of the City of Philadelphia.
- Philadelphia District Standard measurement (DS) are to be used in the City of Philadelphia for title and conveyance purposes.
- Reference information obtained: City of Philadelphia, City Plan. Plan was made with the availability of a title report TV File #: 3961371MA, File #: PHL-12-27141MXJ-14 643 495 and is subject to any facts that may be disclosed by a full and accurate title search.
- Attention is called to the zoning requirements in the City of Philadelphia Code as amended.
- The change from inches to the more precise decimal expression may result in minor changes in second and third decimal places, these are not mistakes or oversights but more precise values.
- Existing utility information shown are composite and may not be complete obtained from the site survey and plan from individual utility companies. Location shown approximately and where information was readily available. Contact 1-800-242-1776 for the Pennsylvania One Call System 3 days notification in advance prior to ground excavation of the site.
- The preparation of this plan by Ambric Technology Corporation does not imply or constitute the approval of city agencies.
- Plan prepared in accordance with the instructions of Herbert Reid III and without field verification to determine the existing site condition.
- A zoning permit is required for any proposed changes to lot lines including consolidation of existing parcels.
- Ambric Technology Corporation hereby reserves its common law copyrights and all property rights in these drawings, ideas and designs. The information on these sheets are not to be modified or copied in any manner. The information on these sheets are not to be assigned to any third party, without the expressed written consent of Ambric Technology Corporation.

SLOPE (%)	Number of Stabilizer Pins per 10 ft. eel	Minimum Depth of Embedment (inches)
1 to 10	none	not applicable
>10 to 33	2	6
>33 to 50	2	12

Table 1.0 - Stabilizer/Pinning for Erosion EEL™

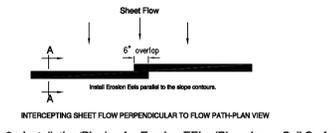


Figure 3 - Installation/Pinning for Erosion EELs (Placed over Soil Surface)

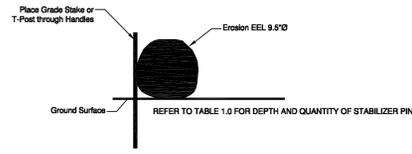


Figure 4 - Installation/Pinning for Erosion EELs (Placed over Soil Surface)

**EROSION EEL DETAIL**

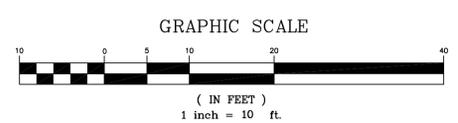
**E&S NOTE:**  
ALL TOPSOIL IS TO BE REMOVED FROM SITE IMMEDIATELY. ANY/ALL SPOILS OR DEBRIS SHALL BE PLACED IN THE DESIGNATED AREA SHOWN ON THIS SHEET.

**LIMIT OF DISTURBANCE NOTE:**  
THE LIMIT OF DISTURBANCE SHOWN ON THIS PLAN IS FOR GRAPHIC REPRESENTATION ONLY. ALL DISTURBANCE WILL BE KEPT WITHIN THE PROPERTY LINES EXCEPT AS SHOWN WHERE THE ROCK CONSTRUCTION ENTRANCE AND PROPOSED UTILITY LATERALS CROSS THE RIGHT-OF-WAY. THE AREA OF DISTURBANCE, AS DEPICTED, HAS BEEN CALCULATED FROM THE TOTAL LOT AREA, PLUS THE ABOVE MENTIONED EXCEPTIONS.

**LEGEND:**

- (S) ST./SAN. SEWER MANHOLE
- (W) WATER MANHOLE
- (\*) TRAFFIC LIGHT POLE
- (X) UNKNOWN VALVE
- (V) WATER VALVE
- (P) POWER UTILITY POLE W/ LIGHT
- (F) FIRE HYDRANT
- (G) GAS VALVE
- (L) LOT LINES
- (X-X) CHAINLINK FENCE
- (- - -) WOOD FENCE
- (- - -) WATER MAINS
- (T.B.R.) TO BE REMOVED
- (TC) TOP CURB
- (G) GUTTER
- (P.O.B.) POINT OF BEGINNING
- (C.I.P.) CAST IRON PIPE
- (A.D.) AREA DRAIN
- (GR.) GRATE
- (INV.) INVERT
- X 100.00+ EXISTING SPOT ELEVATION
- X 100.00 CALCULATED CITY PLAN (TOP CURB EL.)
- X 100.00 PROPOSED SPOT ELEVATION
- CONC. CONCRETE
- F.F. FINISHED FLOOR
- EXIST. EXISTING
- PROP. PROPOSED
- 0.36 SLOPE DIRECTION
- SWALE
- PROPOSED FRESH AIR INLET / TRAP
- CURB STOP / WET TAP
- CITY INLET
- SAN PROP. SAN SEWER LATERAL
- ST PROP. STORM WATER LINE
- F PROP. FIRE WATER LATERAL
- DOM PROP. DOMESTIC WATER LATERAL
- EEL CONSTRUCTION EROSION EEL
- LD LIMIT OF DISTURBANCE
- SILT FENCING
- DANDY BAG
- ROCK CONSTRUCTION ENTRANCE
- TEMP. SPOILS/DEBRIS STOCKPILE
- CITY INLET W/ INLET PROTECTION

**TOTAL LIMIT OF DISTURBANCE = 11,621 S.F. OR 0.2668 AC.**



- STREETS DEPARTMENT NOTES:**
- WORK TO BE DONE IN ACCORDANCE WITH STANDARD SPECIFICATIONS, APPROVED DRAWINGS, AND REGULATIONS OF THE DEPARTMENT OF STREETS, PHILADELPHIA WATER DEPARTMENT, PHILADELPHIA PARKS & RECREATION DEPARTMENT, AND SPECIAL PROVISIONS OF THE PROPOSAL.
  - PURSUANT TO THE REQUIREMENTS OF PENNSYLVANIA ACT 287 (1974), AND AS AMENDED, THE CONTRACTOR SHALL CONTACT THE PENNSYLVANIA ONE CALL SYSTEM AT 1-800-242-1776, AT LEAST 3 WORKING DAYS PRIOR TO EXCAVATION. PENNSYLVANIA ONE CALL SYSTEM # 20141123161 AND WARD # 16.
  - UTILITIES SHOWN ARE TAKEN FROM PUBLIC RECORD. THE CONTRACTOR MUST VERIFY THE EXACT LOCATION AND DEPTH.
  - HORIZONTAL AND VERTICAL CONTROL, LINE AND GRADE STAKES FOR CURB, PAVING, ETC. WILL BE FURNISHED BY SURVEY DISTRICT OF THE CITY OF PHILADELPHIA BASED ON ITEM #4-1040.
  - PERMITS FOR BOLLARDS, CURB & SIDEWALK PAVING WILL BE FURNISHED BY THE HIGHWAY DISTRICT OF THE CITY OF PHILADELPHIA.
  - THE CITY OF PHILADELPHIA SHALL PROVIDE INSPECTION SERVICES FOR PAVING AND RELATED WORK, TO BE PAID UNDER ITEM #4-1041 AT A COST OF \$345 PER DAY. THE CONTRACTOR SHALL CONTACT THE CONSTRUCTION UNIT OF THE DIVISION OF SURVEYS, DESIGN & CONSTRUCTION AT (215) 686-5539. A MINIMUM OF 2 WEEKS PRIOR TO THE START OF WORK, THIS ITEM, INSPECTION SERVICES, SHALL BE INCLUDED IN THE CONTRACTOR'S BID.
  - STREET LIGHT POLE LOCATIONS ARE NOT FINAL. THE STREETS DEPARTMENT STREET LIGHTING ENGINEER WILL DETERMINE THE EXACT LOCATIONS OF THE STREET LIGHT POLES DURING CONSTRUCTION. CONTACT THE STREET LIGHTING ENGINEER AT (215) 686-5517 TO COORDINATE STREET LIGHT POLE LOCATIONS.
  - FOR PROJECTS ON STATE ROUTES, NOTICE IS HEREBY GIVEN THAT THE RECEIPT OF A PERMIT FROM EITHER THE PHILADELPHIA STREETS DEPARTMENT, OR THE PENNSYLVANIA DEPARTMENT OF TRANSPORTATION (PENNDOT) DOES NOT IMPLY A PERMIT FROM THE OTHER. ALL PERMITS MUST BE OBTAINED PRIOR TO THE START OF CONSTRUCTION.

**BEFORE YOU DIG ANYWHERE IN PENNSYLVANIA CALL 1-800-242-1776. NON-MEMBERS MUST BE CONTACTED DIRECTLY. ALL WORK REQUIRES THREE WORKING DAYS NOTICE TO THE PENNSYLVANIA ONE CALL SYSTEM, DRILL, BLAST OR DREDGE.**

**PWD TRACKING NO. 2014-1412-2554-01**

**AMBRIC TECHNOLOGY CORPORATION**  
Consulting Engineers and Surveyors  
100 Pine Street  
Colwyn, PA 19023  
(phone) 215-928-8930/484-257-0010  
(fax) 215-928-8980/484-257-0016

REV.	DATE	REVISION
1	06/13/14	PER CITY SURVEYORS REVIEW
2	08/11/14	SHEET NUMBERING / E&S PLAN

DESIGNED: MK  
DRAWN: PC  
CHECKED: MK  
APPROVED: MK

DESIGNED BY: Marcus Kaplan, RLA # 375-L

**SHEET TITLE:**  
PWD LIMIT OF DISTURBANCE / EROSION & SEDIMENT CONTROL PLAN

SCALE: 1" = 10'  
PROJECT #: 9510-ENG-14  
DATE: 05/29/14

**SHEET NO. 4**

**10**

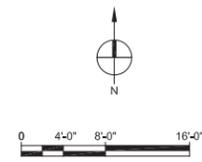
NORTH 15TH STREET

WEST DAUPHIN STREET

NORTH CARLISLE STREET



GROUND FLOOR AND LANDSCAPE PLAN





**11**

NORTH 15TH STREET

WEST DAUPHIN STREET

NORTH CARLISLE STREET



TYPICAL UPPER FLOOR PLAN (LEVELS 2-5)





**12**



CARLISLE STREET ELEVATION (EAST ELEVATION)



DAUPHIN STREET ELEVATION (NORTH ELEVATION)

DAUPHIN HOUSE

1412-1426 WEST DAUPHIN STREET  
2258-2260 NORTH CARLISLE STREET  
PHILADELPHIA, PA 19132

1400 DAUPHIN  
ASSOCIATES, LLC

1341 NORTH DELAWARE AVE.  
SUITE 204  
PHILADELPHIA, PA 19147



**Wallace Roberts & Todd, LLC**

1700 Market Street  
28th Floor  
Philadelphia, PA 19103

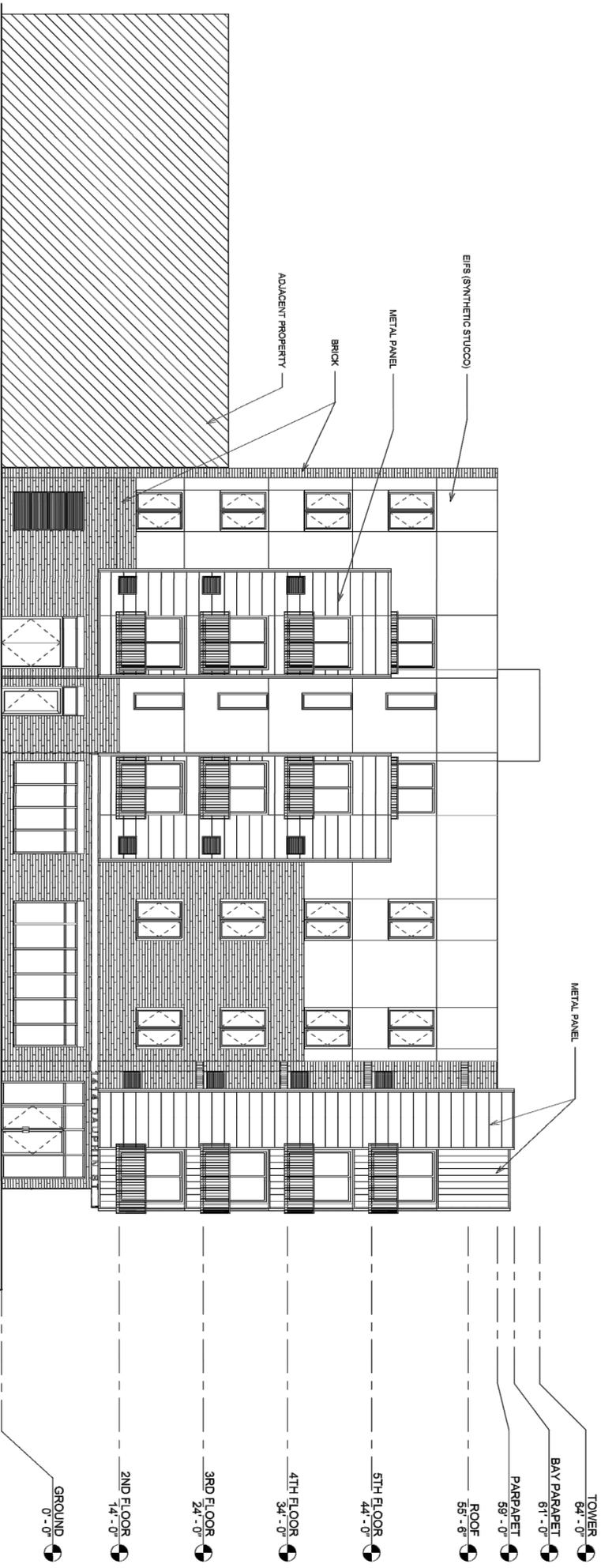
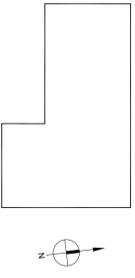
tel 215.732.25215  
fax 215.732.2551

OWNERS CONSULTANTS:  
CIVIL ENGINEERING

**AMBRIC TECHNOLOGY CORPORATION**

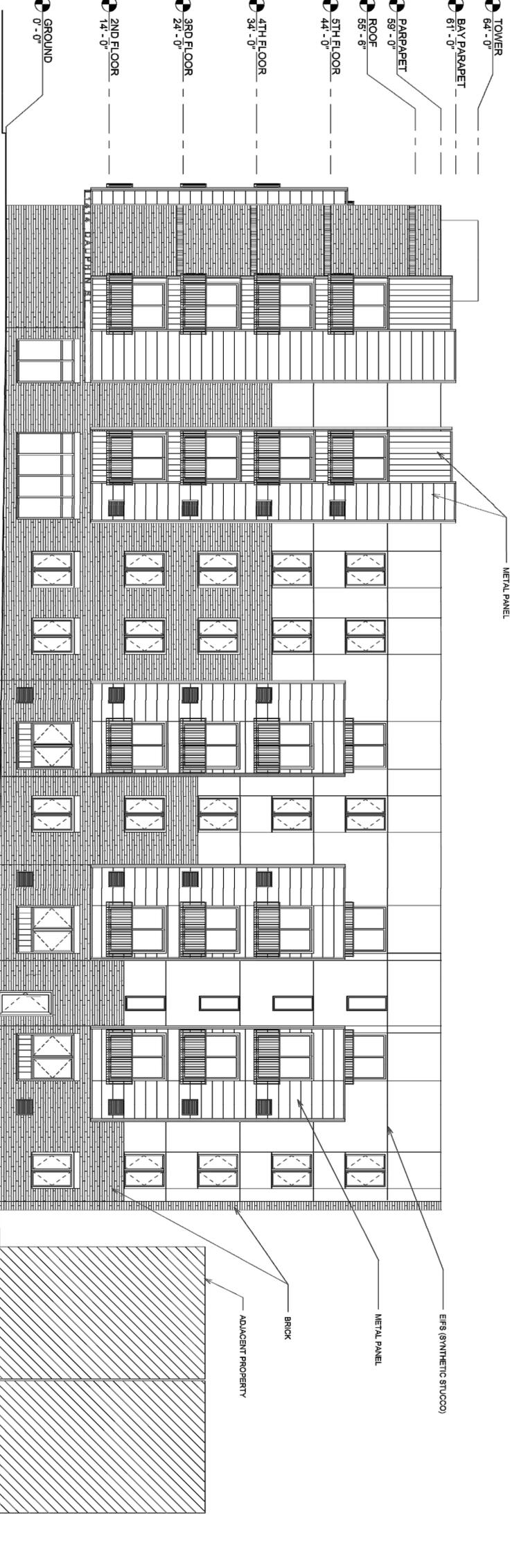
100 PINE STREET  
DOWNTOWN, PA 19220  
Phone: (215) 325-5830 Fax: (215) 929-8904

KEY PLAN



CARLISLE STREET ELEVATION (EAST ELEVATION)

SCALE: 1/8" = 1'-0"



DAUPHIN STREET ELEVATION (NORTH ELEVATION)

SCALE: 1/8" = 1'-0"

REV.#	DATE	DESCRIPTION
1	07/08/2014	ZONING SUBMISSION

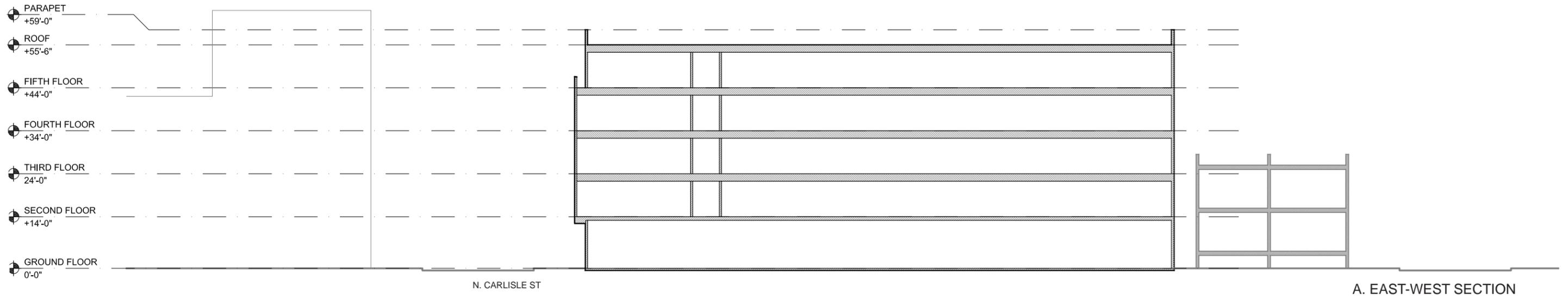
SEALS

DATE: 07/21/6,01  
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SCALE: AS NOTED  
DWG: [blank]

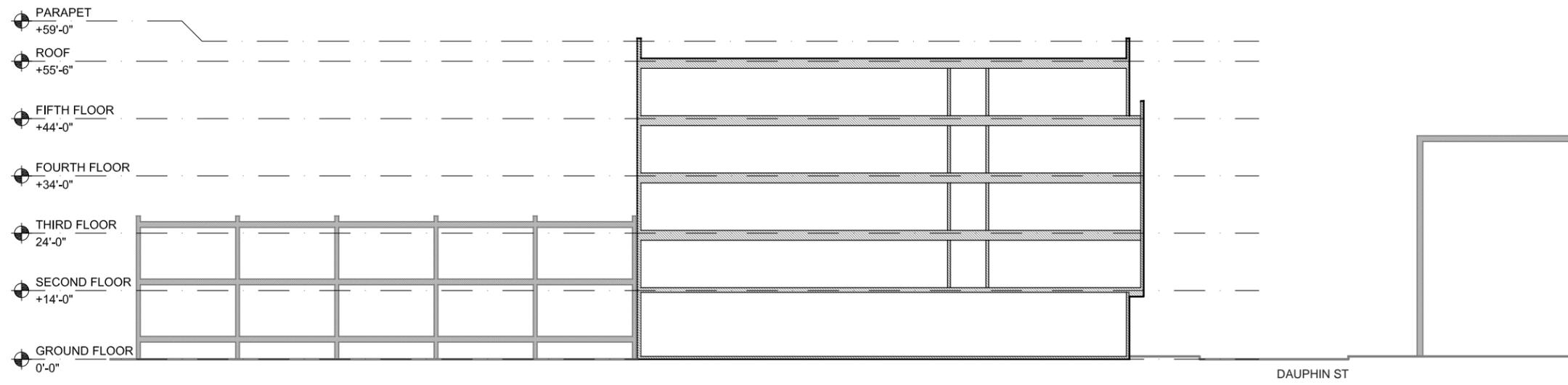
BUILDING ELEVATIONS

Z-004

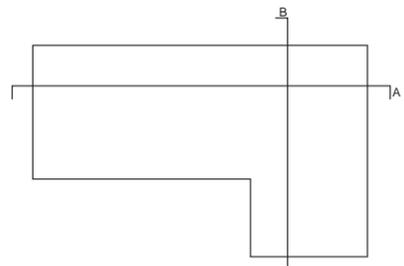
**13**



A. EAST-WEST SECTION



B. NORTH-SOUTH SECTION



KEY PLAN



**14**





VIEW FROM BROAD STREET LOOKING DOWN DAUPHIN ST.



Wallace Roberts & Todd, LLC

1700 Market Street  
28th Floor  
Philadelphia, Pa 19103  
tel 215.732.5215  
fax 215.732.2551

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DAUPHIN HOUSE  
WEST DAUPHIN & NORTH CARLISLE STREETS

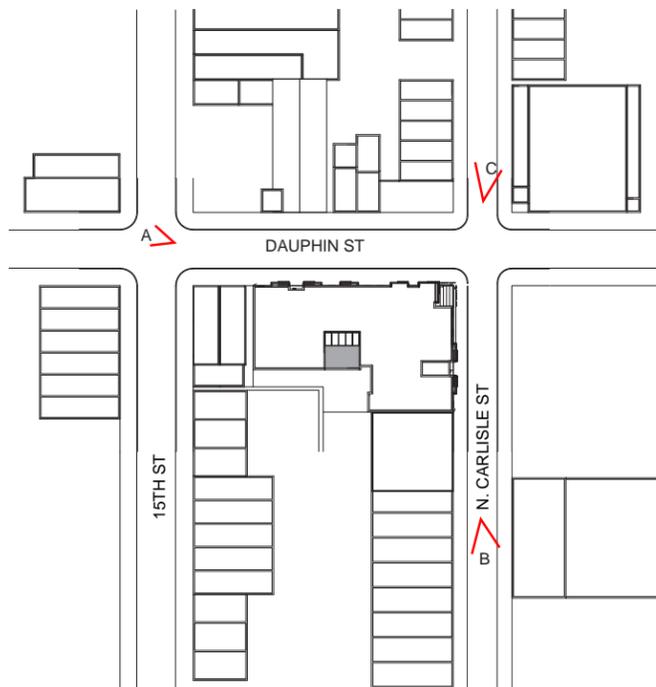
**15**



A. VIEW FROM 15TH ST AND W. DAUPHIN ST



BIRDS EYE VIEW



KEY PLAN



B. VIEW LOOKING NORTH UP N. CARLISLE ST



C. VIEW LOOKING SOUTH DOWN N. CARLISLE ST

DAUPHIN HOUSE  
WEST DAUPHIN & NORTH CARLISLE STREETS