



4

PLANNING PRINCIPLES & GOALS

Plan goals and principles were developed in response to an evaluation of the existing conditions.

4.1 Principles

Goals were developed to address the following six broad neighborhood-wide principles.

1. Enhance the diverse functions of the Logan Square Neighborhood - Parkway as a thriving Center City residential neighborhood, downtown office and employment center, and international cultural destination.
2. Assure that any adverse impacts on the quality of life of the neighborhood that may result from each of these functions are adequately resolved.
3. Improve communications between the stakeholders representing the diverse functions of the neighborhood, so that cooperation can be enhanced and issues of

conflict can be expeditiously resolved.

4. Conserve the character of established residential neighborhoods.
5. Encourage a diverse residential population, in terms of age, income and other attributes.
6. Create a sustainable community in terms of environmental impacts of buildings and infrastructure.

◀ *Image 44. Logan Square, Bird's-eye View*

4.2 Quality of Life Goals

1. Reduce the adverse impacts of Parkway events by limiting the number of events and assuring that permitted events contribute to the cultural life of the Parkway.
2. Improve recycling and trash management for both residential and commercial uses.
3. Address the problem of chronic homelessness in the neighborhood, in particular along the Parkway and North Broad Street areas, by encouraging evening activities, developing more active uses and programming of open space, and improving landscaping and lighting, as well as through appropriate interventions, including the provision of necessary services and enforcement of applicable laws and regulations.
4. Encourage high-quality neighborhood schools.
5. Encourage the continued development of the Parkway as an international cultural destination, the “Museum Mile,” with attractive landscaping and small scale-retail amenities (e.g. cafes).
6. Continue to improve access to and enhance recreational facilities along the Schuylkill River Trail.
7. Increase the utilization of FranklinTown Park and realize its potential as a community resource through encouragement of appropriately scaled residential and retail development on abutting sites.
8. Continue and enhance the successful program of public art within the Logan Square neighborhood.

Image 45. Coxe Park 





9. Install trees, other landscaping, lighting, appropriately designed street furniture, and lighting to provide a greater sense of comfort, safety, and cleanliness to the neighborhood.
10. Encourage more restaurants and other neighborhood-oriented commercial uses within the Logan Square-Parkway community.
11. Establish “Green Streets” that link the Squares of Center City and connect public spaces within the Logan Square neighborhood.
12. Increase community-oriented recreational space (*Image 45*).

4.3. Traffic and Transportation Goals

1. Minimize adverse impacts of traffic and parking at museum attractions, through improved transit service to Parkway institutions and adequate parking, including possible weekend use of nearby non-residential parking facilities.
2. Enhance the walkability and pedestrian safety of the neighborhood.
3. Reduce the barrier effect of the Benjamin Franklin Parkway on pedestrian movement between residential areas to the north and south by reducing the number of vehicle travel lanes, as well as by providing pedestrian-oriented landscaping and appropriate small-scale single-story commercial development (e.g. cafes) at north-south streets.
4. Reduce the barrier effect of the Vine Street

Expressway on north-south pedestrian movement by making streetscape improvements at overpasses and by covering open sections of the depressed expressway.

5. Improve pedestrian connections, including across North Broad Street to the Callowhill neighborhood and Convention Center expansion, and reduce the barrier effect of JFK Boulevard and the SEPTA tracks west of 20th Streets, which separate Logan Square from neighborhoods to the south, through streetscape improvements, vertical connections, and development design controls on north-south streets (*Image 46*).
6. Enhance transit service by increasing the frequency of bus, regional rail, subway and trolley routes; improving station areas; enhancing bus stops with seating, real-time information, and shelters; and supporting new transit initiatives serving the Parkway.

Image 46. SEPTA tracks underpass ▶





7. Improve bicycling conditions by providing continuous east-west and north-south bike lanes, bicycle racks, and access to the Schuylkill River Path.
8. Decrease traffic speeds on streets through the Logan Square neighborhood, including the Parkway.
9. Address traffic congestion at critical intersections and corridors throughout the Logan Square neighborhood, including the Parkway and the intersection of I-676 West at 22nd Street.
10. Address parking deficiencies in the neighborhood through encouraging car share and improving management of on-street parking spaces.
11. Encourage Parkway Institutions to collectively address parking issues by creating new parking facilities or setting up alternative access options.

4.4 Development Goals

1. Encourage a balanced variety of housing types, including housing types that can attract more families with school age children, to increase age diversity within the community.
2. Ensure that new development enhances the existing community and minimizes the adverse impacts on daylight and sunlight access, nighttime light pollution, and views enjoyed by existing residential buildings through appropriate development density, building height, massing, building spacing and location, and street level design.
3. Encourage highest-intensity commercial mixed-use development along the Market Street and JFK Boulevard corridor south of (and including) the SEPTA tracks, in areas east of 19th Street and south of Race and east of 18th between Race and Callowhill (including air rights development over the Vine Expressway), and along the North Broad Street corridor west to 16th Street.
4. Discourage high-intensity office-core-type commercial/mixed-use in other areas of the community not included in (3) development, and instead encourage appropriate density residential or mixed-use developments.
5. Enhance and protect the small-scale low-rise residential character of the historic residential core of Logan Square.
6. Encourage the development of Callowhill Street between 18th and 21st Street as a lively dining and arts-oriented, pedestrian-scaled commercial corridor that can serve both neighborhood residents and visitors to Parkway institutions.
7. Eliminate the adverse aesthetic impacts of

Images 47 & 48. Tivoli Townhouse Condominiums (left); and Callowhill Street (right) 



exposed above-ground parking garages by requiring that all parking be underground, or wrapped in active uses.

8. Protect the unique character and historic resources of the Benjamin Franklin Parkway and Logan Square, through continued application of the longstanding Benjamin Franklin Parkway height controls.
9. Provide the opportunity for limited small-scale cafes and similar uses to enliven the Benjamin Franklin Parkway, while maintaining the long established green and open character of the Parkway.
10. Assure the preservation of historic resources in the Logan Square Neighborhood - Parkway, including preservation of the historic integrity of the Family Court building, in any potential reuse plan.

11. Assure that adequate private and/or public usable open space is provided in all new residential developments.

12. Encourage pedestrian-friendly streetscapes in all new development, through high-quality pedestrian-scale architectural and landscape design elements, and through active uses at street level.

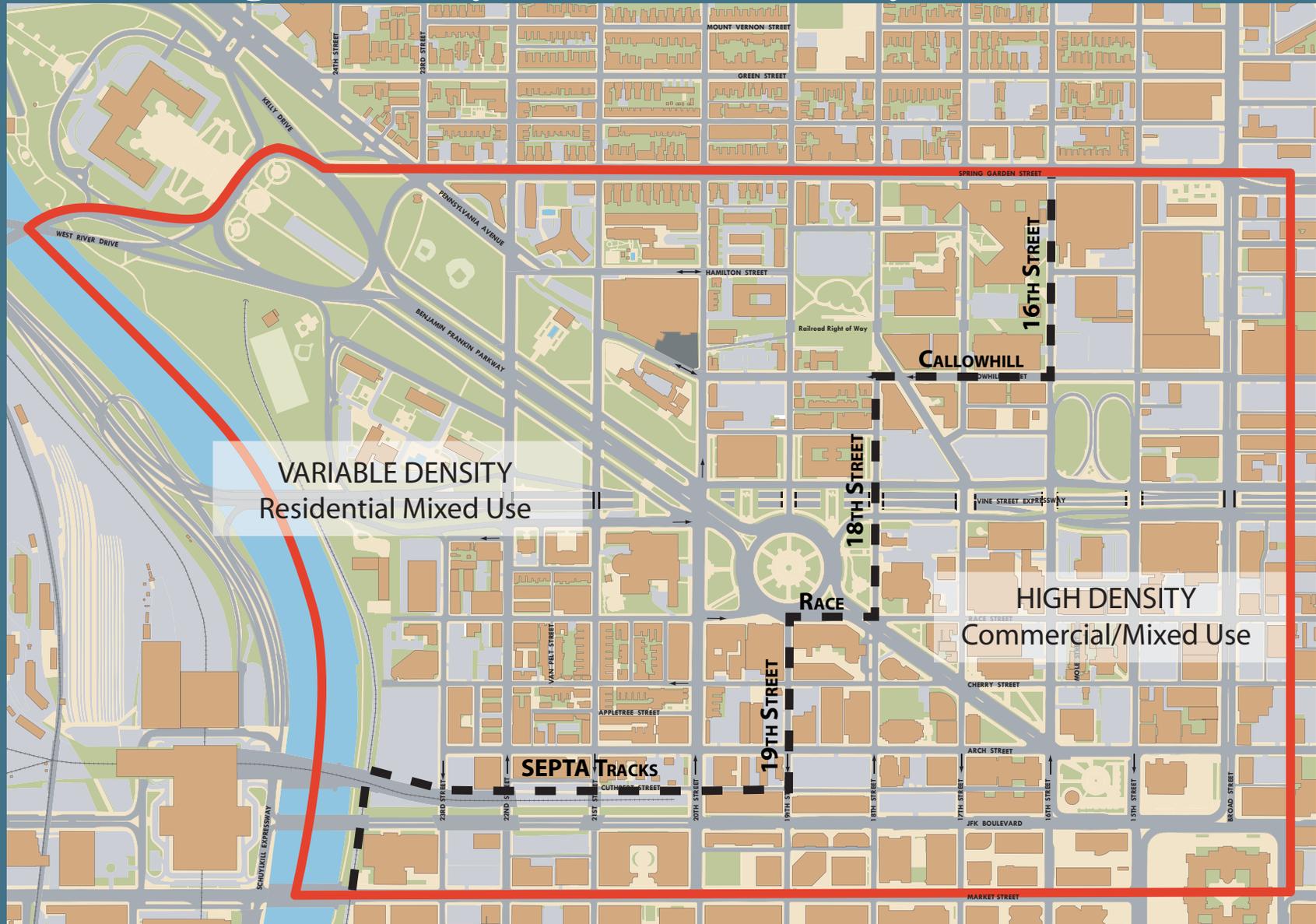
13. Mitigate construction impacts with standards for street closures, street lane and sidewalk closures, noise pollution, and site cleanliness.

14. Establish a neighborhood design review process for all major projects (public, private, etc.), to assure community input into the development approval process.

15. Incorporate sustainable design principles in all developments.

Map 19. Limit of High Intensity Commercial Development 

LIMIT OF HIGH INTENSITY COMMERCIAL DEVELOPMENT





4.5 Community Empowerment Goals

1. Ensure that all organizations in the Logan Square neighborhood, especially LSNA and the Parkway Council Foundation, come together to work on issues of mutual concern and to implement the recommendations of the Plan.
2. Provide a consistent review process for infrastructure and development projects that ensures appropriate input from developers, public entities, and neighborhood stakeholders.
3. Establish funding mechanisms for community amenities through a variety of sources.
4. Enhance LSNA's role as an important clearinghouse for information of interest to the community.
5. Encourage all neighborhood stakeholders to become more involved in LSNA, and use it as a resource.
6. Enhance linkages between the Parkway institutions and North Broad Street institutions, including the Pennsylvania Academy of Fine Arts and Pennsylvania Convention Center.
7. Ensure appropriate community and stakeholder input into all actions impacting the neighborhood.
8. Enhance cooperation between LSNA and community organizations representing abutting neighborhoods to address issues of mutual concern.

 *Image 49. Philadelphia Family Court Building - a reuse opportunity*



5

PLAN RECOMMENDATIONS

5.1 Quality of Life

Parkway	Dilworth Plaza	Other Open Space	Homelessness	Sustainability	Light/Noise Pollution
Additional cafés along Parkway.	Rehabilitate plaza.	More pocket parks (like Coxe Park).	Adequately enforce laws and regulations addressing public behavior and encampments.	Increase tree canopy (500 trees in 3 years).	Minimize impacts of outside light sources on interior residential spaces.
Infill openings over Vine Expressway.	Reduce barriers to City Hall views.	Open Science Park for neighborhood children.	On Parkway, cease outdoor food distributions and install public restrooms.	Promote green streets.	Minimize noise impacts of building mechanical equipment.
Widened bridge crossings over Vine Expressway.	Streetscape	Require new development to provide open space	Reevaluate all policies and activities that contribute to the disproportionately large chronic street homeless population in the neighborhood.	Encourage alternate energy systems.	Parkway Events
Better landscape on north-south streets across Parkway.	Green Cherry Street between Convention Center and Parkway.	Provide dog parks.	Inform community of proposals for homeless/social service facilities.	Encourage green roofs.	Create committee to evaluate number and type of events.
New sidewalks and benches.	Green Race Street between Convention Center and Schuylkill River.	Active Open Space	Encourage the hiring of the formerly homeless.	Encourage rainwater capture (rain barrels).	Limit events to signature events (Thanksgiving Parade, July 4th, etc.)
Schuylkill River Park	Enhance neighborhood gateways (Vine Expressway to 22nd and 23rd Streets).	Picnic grove on trail at Martin Luther King Drive.	Encourage the hiring of the formerly homeless.	Discourage private auto ownership/encourage transportation alternatives.	Provide shuttles to off-site parking areas.
Wayfinding signage from Art Museum.	Enhance historic small streets.	Reprogram existing playing fields/add new fields for soccer.	Education	Community gardens on neighborhood side of CSX tracks.	Encourage alternatives to driving to events.
Bicycle link between Rivers Edge and Cherry Street.	Add trash receptacles in commercial areas.	Construct tennis courts (under the Expressway at 24th Street?)	Support expansion of charter and private schools.	Promote recycling/composting.	Extend parking time limits in residential areas to weekends/holidays during events.
Restroom on River trail.	Improved transit information at bus stops.	Complete Paines Park skateboard venue.	Support expansion of higher education facilities.	Require trash recycling for multi-family developments.	
Bridge over rail tracks to trail from 24th Street.	Improve JFK/SEPTA track underpasses.	Community use of Roman Catholic High playing fields.		Require car share and bicycle facilities.	
Public dock between Arch and Vine.	Pedestrian lighting throughout neighborhood.	Community use of rooftop tennis courts at Community College.		Promote LEED for new developments.	
	Green parking garages with vertical gardens.				

5.1.1 Passive Open Space and Streetscape

Logan Square-Parkway

- Locate a limited number of cafes similar to Cafe Cret at 3 Parkway (**Image 46**), kiosks and other site-appropriate, small-scale retail (e.g., bookstalls) along the Parkway (for example, in front of Park Towne Place, in front of the Library, and in front of the play equipment at Von Colln Field), as well as public restrooms.
- Infill openings over the Vine Street Expressway at the Free Library and Family Court Building and north of Franklin Institute to create passive landscaped open spaces, integrated with the green spaces of the Parkway and Logan Square.
- Enhance proposed replacement north-south bridge spans over the Vine Expressway with widened bridge structures and extensive landscaping, to create more pedestrian-friendly crossings.
- Install pedestrian-scale landscaping on adjacent north-south streets within the Parkway as a

welcoming tool and to link areas north and south of the Parkway.

- Replace sidewalks and benches along the Parkway from Logan Square to the Art Museum.

Schuylkill River Park

- Provide more way-finding signage for pedestrians and bicyclists connecting the Art Museum with the Schuylkill River Trail.
- Create a pedestrian/bicycle link on the city-owned parcel on the east side of the CSX tracks between CSX tracks and Rivers Edge Condominium, to connect the Schuylkill River Trail to Cherry Street via the Race Street crossing. This will allow westbound cyclists legal access to the trail. This link should be extended to Arch Street, with development of the sites at 23rd and Arch Streets.
- Locate a water fountain and public restroom

between Vine Street and Locust Street along the Schuylkill River Trail to support trail users in the area.

- Build a bridge across the CSX tracks at the Schuylkill River Park on piers already owned by the City to make a connection at 24th Street (*Image 50a*).
- Build a public dock for small craft (e.g. kayaks and canoes) with a secure storage facility along the Schuylkill River within the Logan Square-Parkway area.
- Investigate feasibility of installing swimming and recreation barges at Race Street.

Dilworth Plaza

- Rehabilitate Dilworth Plaza to make it more user-friendly and enhance pedestrian connections to the subway concourse.
- Reduce barriers to views of and from City Hall, including removal of dense tree cover.
- Rehabilitate and enhance landscaped areas with a mix of attractive low seasonal plantings and planters for additional seasonal interest.



▲ *Image 50a. Location of proposed pedestrian bridge over CSK tracks at 24th Street*

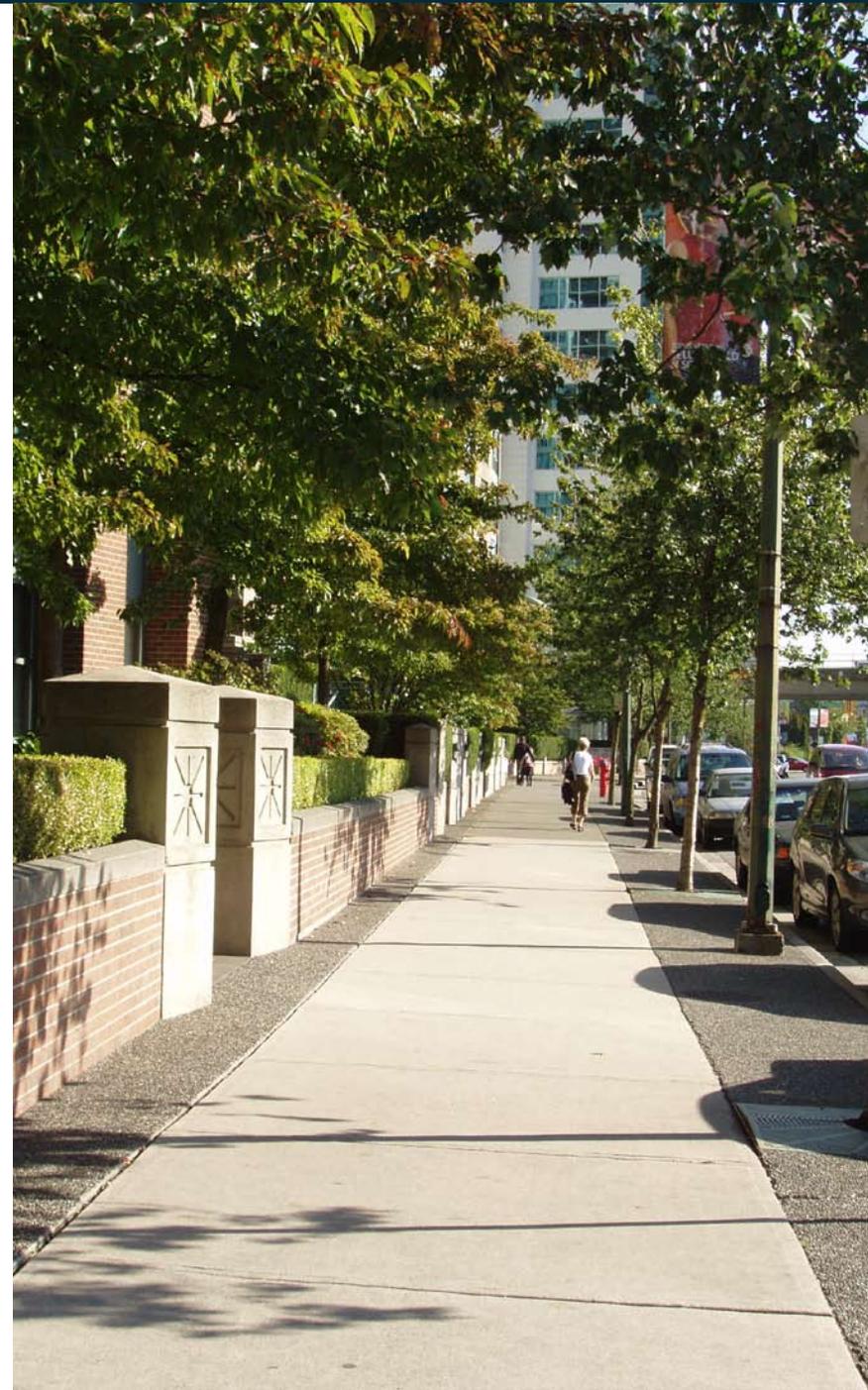
▼ *Image 50b. Cafe Cret at 3 Parkway*



Streetscape

- Streetscape and “green” Cherry Street from Broad to 16th Street with street trees and planters to provide a better connection from PAFA and the Convention Center Expansion to the Parkway.
- Streetscape and “green” Race Street from Broad Street to the Schuylkill River Trail to provide a green link between the Convention Center Expansion to the Schuylkill River and Logan Square.
- Enhance the gateways to the community with landscaping and lighting, such as the exits from I-676 on 23rd Street and 22nd Street from Park Towne Place to Race Street.
- Enhance and extend the “small streets” of Logan Square as an interconnected pedestrian walkway system with street trees, pedestrian-scaled street lights, appropriate paving, and improved sidewalks.
- Locate trash and recycling receptacles throughout commercial areas, along the Parkway, in public parks, and at bus shelters. Trash cans in residential areas are not appropriate, except for small pet waste disposal systems.
- Locate transit shelters with flip-down seating, graffiti resistant surfaces, real-time information, and city wide transit maps throughout the neighborhood.
- Repair leaking underpasses and provide better lighting at 23rd, 22nd, and 21st Streets between Market and Arch Street. Provide pedestrian connections between JFK Boulevard and numbered streets in conjunction with development of adjacent parcels.
- Install more pedestrian-scaled street lighting throughout Logan Square to improve safety and livability.
- Encourage the installation of “vertical gardens” as a means of beautifying and “greening” parking garages that lack active first-floor uses or any screening of the parking decks and serving as an alternative to murals.

Images 51a & b. Well-streetscaped corridor in the study area (left) and; and example of high quality streetscape (right) 



Other Open Space

- Create small pocket parks, similar to Coxe Park, with play equipment for children ages 5-12.
- Work with the Franklin Institute to open the Science Park for neighborhood children.
- Require new development to provide passive private and public open space such as plazas, enhanced streetscape, balconies, and courtyards. Establish long-term maintenance agreements between developers and the neighborhood in order to provide the ongoing care these parks require.
- Improve the deteriorated and derelict area under the Vine Expressway Bridge in the vicinity of 24th Street and the CSX tracks to connect open space areas north and south of the elevated expressway.
- Identify locations for neighborhood dog parks, possibly including one in the vicinity of 24th Street at Vine Street.

5.1.2 Active Open Space

- Create a picnic grove in the area of Martin Luther King Drive and the Schuylkill River Trail for grilling and gathering after race events in coordination with the location of Paine's Park.
- Reprogram existing fields in the neighborhood (without significantly detracting from public use) and/or add soccer fields to accommodate neighborhood schools' need for soccer fields.
- Determine locations to incorporate tennis courts into the neighborhood, possibly under the Vine Expressway in the vicinity of 24th Street.
- Support the creation of Paine's Park as a skateboard venue.
- Work with Roman Catholic High School and the City to establish community use of Fairmount Park athletic field, currently leased to the school, west of 24th Street, including tennis courts and other facilities.
- Work with Community College to establish community use of rooftop tennis courts.

Image 52. Area under Vine Street Expressway offers an opportunity for recreational development 



5.1.3 Homelessness

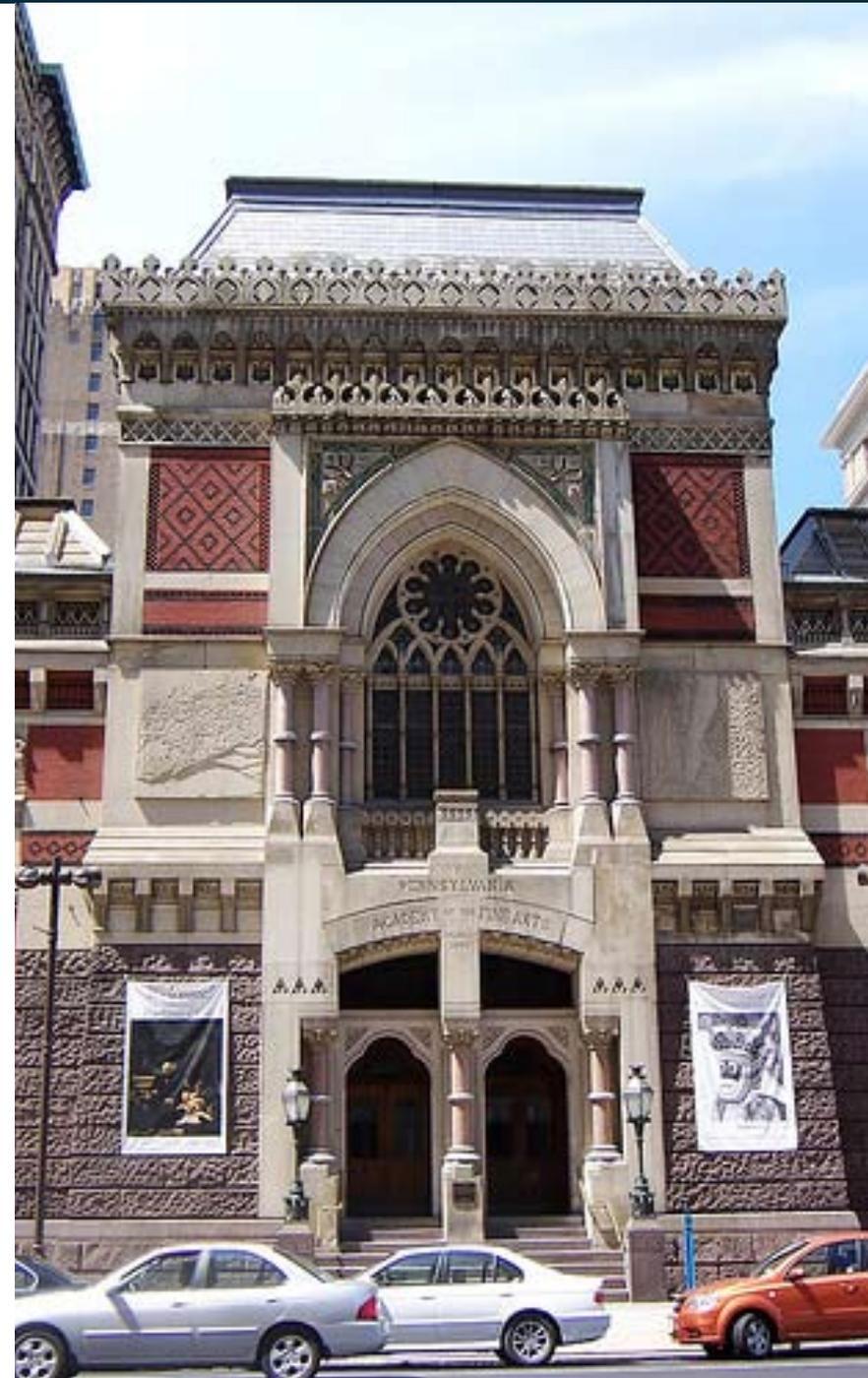
- Increase public awareness of and support for effective, “best practices” responses to chronic homelessness (e.g., the “Housing First” approach) and panhandling by conducting public forums with professional homeless services providers and other public education.
- Remove all homeless encampments throughout the Logan Square Neighborhood - Parkway, including but not limited to those located at Vine Street and 16th Street, along the Parkway (especially in front of the future Barnes site), and beneath the underpasses of JFK Boulevard.
- Cease food distribution to the chronic street homeless in outdoor public spaces through:
 - (a) continued discussions with religious and other volunteer groups distributing food to build their awareness of best practices for addressing chronic homelessness and the importance of linking food recipients with other needed services in dignified, indoor settings;
 - (b) continued efforts to pair food distributors with Center City churches and other indoor sites; and
 - (c) regulatory action to protect public health and ensure appropriate siting of distributions.
- Install site-appropriate public restroom facilities at key locations along the Parkway, provided there are funds for their continual maintenance.
- Enforce all existing laws and park regulations in the Logan Square Neighborhood - Parkway, especially along the Parkway and in all parks and plazas.
- Encourage businesses, institutions, and government agencies located in the Logan Square Neighborhood - Parkway to employ suitable individuals who are homeless or formerly homeless.
- Respond to proposals to locate homeless or other social services facilities within the Logan Square Neighborhood - Parkway by notifying the surrounding community at the earliest opportunity and facilitating appropriate dialogue.

- Critically reexamine all policies, activities, and laws/regulations (or enforcement thereof) that contribute to the disproportionately large chronic street homeless population in the Logan Square Neighborhood - Parkway, recognizing its critical role as a principal destination for local, national, and international visitors.

5.1.4 Education

- Support the expansion of public, charter, and private schools in the neighborhood to provide an asset for residential development and encourage an age-diverse population. Sites that could be considered for expanded educational facilities include the parcels on the south and north sides of the 1900 block of Arch Street, currently occupied by surface parking.
- While respecting the existing residential fabric, support the expansion of Community College, PAFA (*Image 53*), Hahnemann Hospital, Moore College, Drexel University, Temple University, and Strayer University as providers of higher education in the neighborhood, consistent with the goals of the adjacent residential community.

Image 53. Philadelphia Academy of Fine Arts ▶



5.1.5 Sustainability

“Sustainability” is defined as meeting the needs of the present without compromising the ability of future generations to meet their own needs. It involves the integration of environmental, social, human, and economic goals in policies and activities. Within the context of sustainability “sustainable design” is defined as design that seeks to avoid depletion of energy and water and raw material resources; prevent environmental degradation caused by facilities and infrastructure development over their life cycle; and create environments that are livable, comfortable and safe, and promote productivity.

Neighborhood sustainability initiatives should include measures that enhance the quality of life in the neighborhood, including the following:

- Increase the tree canopy in the neighborhood by planting 500 new trees over the next three years.
- Promote “green streets” where current sidewalks are replaced with porous surfaces, such as the historic brick paving found on several blocks of Logan Square; plantings are added in space that does not impede pedestrians; and traffic or parking lanes are replaced with bus or bike lanes.
- Encourage new developments and owners of existing buildings to install solar and alternative energy systems to reduce power demands from existing utilities.
- Encourage “green” roofs, particularly accessible green roofs that would provide an open space recreational resource as well as a visual amenity.

▼ *Images 54-57. Neighborhood Sustainability Initiatives, left to right: tree canopy, “green” roofs, community gardens, car share*



- Support rainwater capture techniques (rain barrels) for new and existing construction townhouse projects and promote the use of rainwater for watering plants and street cleaning.
- Discourage private auto ownership and encourage transportation alternatives including car share, bicycling, walking, and enhanced transit.
- Establish community gardens along the neighborhood side of the CSX tracks at the river.
- Aggressively promote trash reduction through recycling and composting of organic material.
- Require trash recycling for multi-family developments.
- Require car share and bicycle facilities in total required parking provision of new developments (1 per 50 units)
- Promote LEED certification or equivalent for major new developments
- Encourage the installation of “vertical gardens” to “green” and beautify parking garages and blank walls.



5.1.6 Parkway Events

The concentration and collection of museums and institutions along the Parkway’s “Museum Mile” makes this a unique area in the country. The area is a major draw for residents, as well as tourists and visitors. The City should promote this area and connect these institutions in a meaningful way to capitalize on the cultural and educational offerings located in Logan Square.

With that in mind, the Parkway has also come to be known as an event location, with many events each year causing traffic and congestion for institutions and local residents and resulting in trash and noise. Institutions along the Parkway have reported reductions in visitorship during these major events, despite a marketing campaign designed to let visitors and residents know about ongoing events and encouraging them to “Make it a Day on the Parkway.”

- Parkway institutions should be in communication regarding upcoming events and potential connections that can be made between them.
- Create a committee comprised of various stakeholders (institutions, residents, and businesses) to meet regularly with City officials

to evaluate the number and type of events that will utilize the Parkway and to plan the street closures and other logistics necessary.

- Major events that currently occur on the Parkway should be reviewed to determine if they would be better served in other areas of the city.

Currently, there is little overlap of function between events and the institutions on the Parkway, as those participating in charity runs do not tend to support the museums during the same visit.

- The Parkway should not be used as a fairground, but rather host events that include signature events (such as the Thanksgiving Day Parade, Welcome America festivities, July 4th celebrations, and the June bicycle race) and Parkway-oriented events like the Parkway Fun Day, which are institution-focused, family-oriented events that highlight the beauty of the Parkway and provide opportunities for the institutions and neighborhood to relate to the larger community. Institutions should similarly see these events as opportunities to increase their visibility to a larger audience.

EVENTS IN OR NEAR THE LOGAN SQUARE NEIGHBORHOOD, 2008

- January
Mummers Parade – Broad Street
- March
Saint Patrick's Day Parade – The Parkway
- April
Turkish Festival and Parade – The Parkway
Walk America – The Parkway
Armenian Genocide Walk – The Parkway
Kick Butts Day – JFK Plaza
- May
Broad Street Run
Dad Vail Regatta – Kelly Drive
Susan G. Komen Race for the Cure – Fairmount Park
World Lupus Day – JFK Plaza
Jewish Martyrs Memorial
Israel Independence Day
- June
Wachovia US Pro Cycling Championship – The Parkway
- June–July
Welcome America Festival – The Parkway
- August
Unity Day – The Parkway
Recreation Department Summer Olympics – JFK Plaza
- September
Philadelphia Distance Run – The Parkway
General von Steuben Parade – The Parkway
Puerto Rican Festival Parade – The Parkway
Ratha–Yatra Parade/Festival
Heal Our Lord Parade/Festival
CollegeFest
- October
Campus Philly Kick Off – The Parkway
Pulaski Day Parade – The Parkway
AIDS Walk
Step Out Diabetes
- November
Philadelphia Marathon – The Parkway
Thanksgiving Day Parade – The Parkway
- December
Tree Lighting Festival and Parade – City Hall / Broad Street

- A single contact at the City level should be in place to coordinate and serve as a liaison for all aspects of events on the Parkway. This individual should be in contact with the LSNA and the Parkway Council to inform them of upcoming events and related issues.
- Trash should be cleaned up promptly after events. Parties sponsoring events on the Parkway should be required to contract for cleaning services. Fines should be assessed if trash is not picked up within 24 hours of the event.

Traffic issues resulting from these events can be mitigated in several ways.

- Traffic behavior should be closely monitored before, during, and after Parkway events with a highly visible enforcement presence.
- Alternatives to driving should be advertised in print and broadcast media as well as aggressively encouraged on the web sites of event sponsors, LSNA, Parkway Council, SEPTA, and Parkway institutions.
- Transit services should only be detoured as a last resort. When transit is rerouted it should be the

burden of the event coordinator to ensure that proper rerouting information is placed at each affected transit stop. Detours of transit services should be consistent for all events, allowing residents of the area to maintain access to their homes and destinations. Detours should be prominently advertised in advance, including on those websites listed above.

- Shuttles to offsite parking areas should be implemented as another alternative for driving to Parkway events. These have been used effectively at other sites, including Eastern State Penitentiary.
- During Parkway events and on holidays in residential areas, standard weekday parking time limits should be enforced in residential areas, so that on-street spaces “turn over” regularly and are available for residents with parking permits.
- When appropriate, utilize the “No drive zone” policy similar to that used during Live 8.

Image 58. Event on the Parkway ▶



5.1.7 Light and Noise Pollution

- Sources of light, including street lights, parking lot lights, and illuminated works of public art, should be designed to minimize adverse light impacts on interior residential spaces and the public environment. Direct light sources should be screened and “cut off” fixtures used where feasible.
- Noise generated by exterior building mechanical equipment can have an adverse impact on quality of life in a dense residential neighborhood. Equipment such as air conditioning condensers vary in the amount of noise they generate. Low decibel rated equipment should be required. Condensers should not be located in side or rear yards where they can adversely affect neighbors’ enjoyment of their yards; instead they should be roof-mounted. Large roof-mounted mechanical equipment should be screened with enclosures to reduce noise impacts. Exhaust fans should extend to top roofs not through building walls, to minimize noise impacts.

5.1.8 Community Organization

- Establish a seat on the Parkway Council Foundation for an LSNA Board member.
- Regularly provide minutes of the LSNA board meetings to the Parkway Council Foundation.
- Seek adjacent neighborhood organization’s support for relevant projects undertaken by LSNA.
- Participate in building a coalition of support for large projects or initiatives to improve Center City.
- Consider sharing other neighborhood group staff time to support grantwriting and public relations efforts.
- Seek grants and other funding for community amenities.
- Assure that the community is fully informed about city regulations and guidelines that impact the physical environment of the neighborhood.
- Provide a consistent review process for infrastructure and development projects that includes developers, public entities, and residents.

5.2 TRANSPORTATION

5.2.1 Circulation

Capacity Adjustment

The Logan Square Neighborhood - Parkway has matured beyond the point where traffic bottlenecks can be permanently solved by common traffic engineering “quick fixes” such as road widening and additional turn lanes. This is due to space constraints that preclude such actions and the ephemeral benefits of such fixes in light of continuing development.

Attempts to “force” conventional traffic-engineering solutions run a strong risk of doing irreparable

harm to the walkability that is a core asset of the neighborhood and that is targeted along with the bicycle network for further strengthening through this neighborhood plan.

A number of potential engineering-based measures could help alleviate several trouble-spots identified in the course of this study without damaging the underlying urban structure. While the “built-out” nature of the neighborhood makes substantial traffic-capacity enhancement impossible, there are several locations where adjustments to signal timing or intersection geometries could potentially alleviate specific traffic bottlenecks:

- Signal timing adjustments at 22nd/Parkway, 16th/Spring Garden, and Broad/Callowhill

▼ *Images 59-62. Traffic enhancement locations, left to right: 20th/Parkway, 22nd/Parkway, Eakins Oval, 18th St/Callowhill*



(acknowledging potential for two-way traffic operation on the latter—see below).

- Rationalization/simplification of intersections at 20th/Parkway, 15th/Vine, and Broad/Vine.
- Conversion of Callowhill Street between Franklinton Boulevard and Broad Street or 15th Street to two-way traffic to support proposed redevelopment goals, pending the results of DRPA's Center City Access Study (which may affect Callowhill Street's relationship with the Vine Street on/off-ramps).
- Introduction of a traffic signal on 22nd Street at the I-676 on/off-ramps, to better meter traffic flows and to accommodate safer pedestrian crossings.

- Major overhaul of traffic/pedestrian patterns at Eakins Oval.
- Reestablish the 18th Street corridor north of Callowhill Street and eliminate the "jog" at Callowhill Street. This would require the elimination of Franklin Town Boulevard north of Callowhill Street as a through street, reconstruction of the section of 18th Street between Callowhill Street and the former rail right-of-way, and conversion of the former street bed in front of the Community College building at the corner of Callowhill Street to a pedestrian plaza.

Furthermore, the potential conversion of Market Street and JFK Boulevard to two-way traffic should be seriously considered through further study. The



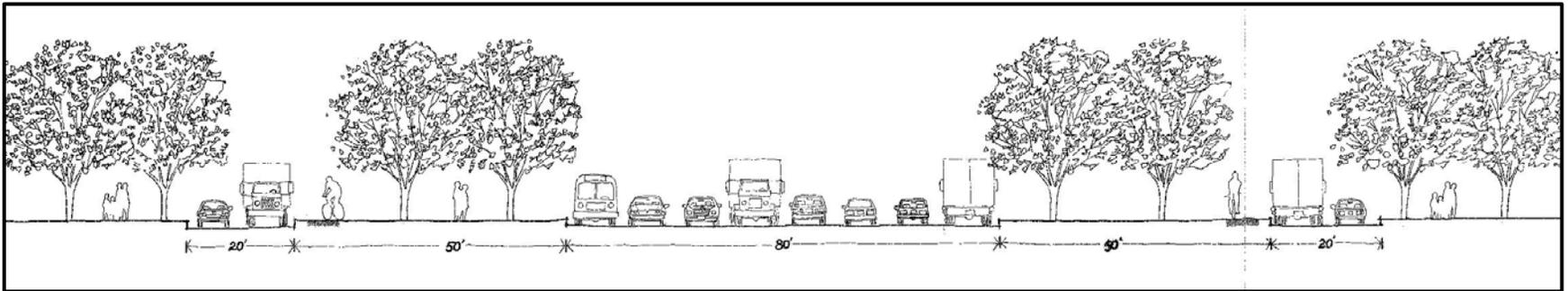
primary benefits of such an action would be the simplification of circulation patterns, traffic calming, the provision of opportunities for a better retail environment with increased visibility for purveyors, and the potential for attractive “boulevard-type” amenities such as a planted median. The primary challenges to this notion are complication of intersection movements and signal timings, prohibition of left turns, rerouting of buses, uncertain impacts on traffic flows and altered access to several existing parking structures which could require entry/exit modifications. The Center City District conducted a very preliminary study of this idea in 2005, yet it would require a more formalized traffic/access study before it could be acted upon.

A major defining element of the Logan Square neighborhood is the Benjamin Franklin Parkway. It is essential that any redesign effort for this corridor reflect the key neighborhood transportation principles of improved pedestrian safety, better-managed traffic circulation, and controlled speeds. The most recent planning studies for redesigning the Parkway—by Central Philadelphia Development Corporation (CPDC) in 2003 and the City Planning Commission (PCPC)/Commerce Department in 2007—have adhered to these principles in their proposals for a modified cross-section, illustrated in

Figure 4.

The most notable elements of these proposals, currently undergoing further evaluation by the Fairmount Park Commission, are:

- reduction of “outer” Parkway sections to function as two traffic lanes during the peak periods and one traffic lane plus parking lane in the non-peak periods, leading to a calmer, slower-speed traffic environment that is easier to cross;
- relocation of the bicycle lanes to the rebuilt, traffic calmed, outer sections; and
- widened/lengthened center median islands for better pedestrian refuges.



▲ *Figure 4. Proposed Cross-Sections of Eakins Oval: PCPC 2007 (top) and CPDC 2003 (bottom)*

The Parkway is framed by two complicated junctions at Eakins Oval and 20th Street. Design solutions for these areas—in early phases of evaluation—include a number of alternate concepts.

Site Analysis: Eakins Oval

Eakins Oval represents a complex circulation challenge. In its current condition, it “fails” by any definition for all modes of travel: automotive, pedestrian, and bicycle. The main facets of the problem for each of these elements are:

- **Automotive Traffic:** Heavy flows and failing levels-of-service at various locations in the morning and afternoon peak periods; and uncomfortable, uncontrolled “merging” and “weaving” movements required for most trips.
- **Pedestrian Traffic:** Pedestrian crossings are missing at several key desire-lines.
- **Bicycle Traffic:** Uncomfortable shifting and merging across unpredictable traffic lanes.

Over the years, Eakins Oval has been studied in several contexts with two concepts having emerged that would calm traffic by tightening up intersections and that would add crosswalks at key locations, including signalized pedestrian crossings to the Art Museum steps. These concepts, each of which requires further study, are illustrated in **Figure 5** and described below.

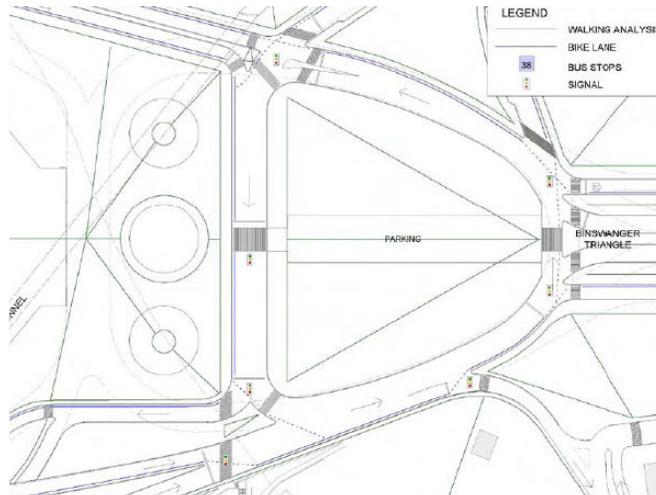
Eakins Triangle (Origin: Philadelphia City Planning Commission Benjamin Franklin Parkway: Circulation, Parking, and Transit Study, 2007)

In the Eakins Triangle concept, traffic circulation would function similar to the way it does now, but in a smaller space and at a slower pace. The roadway that currently travels between the Art Museum and Eakins Oval would be relocated to the east side of the Washington monument and fountains. This plan creates a continuous public open space between Art Museum steps and the Washington Monument, and rationalizes and tightens several intersections. However, the concept requires detailed intersection-by-intersection traffic study or modeling.

Figure 5. Previous Eakins Oval Site Analysis: PCPC 2007(left) and CPDC 2004 (right) 

Greber Oval (Origination: Philadelphia Museum of Art Concept Study Restoring Greber's Plan for Eakins Oval, 2000; Central Philadelphia Development Corporation Public Improvements and New Urban Design Framework for the Benjamin Franklin Parkway, 2004)

The Greber Oval concept attempts to re-create the original 1917 plan for the Parkway, retrofitted for modern conditions. The concept de-concentrates traffic by re-rerouting cars to/from Martin Luther King Drive along the restored Crescent roadway. In addition, it urbanizes” the Oval and its intersections through tighter design and right-angle street approaches. However, connection of the Crescent with Martin Luther King Drive requires major reconstruction, and it conflicts with existing and planned recreational amenities along the river.



In addition, overall vehicular service levels are not significantly improved, as several congested movements continue to encounter bottlenecks (including a difficult left-turn movement from the Parkway outbound to the Crescent/Martin Luther King Drive).

While the redesign of Eakins Oval represents a major effort requiring a dedicated traffic study, it is important that the Logan Square neighborhood be involved in future planning efforts to ensure that the quality-of-life principles documented in this report are reflected in the eventual Eakins Oval solution.

Any redesign of Eakins Oval should include elimination of the unsightly surface parking, and focus on the principle goal of improving the safety and number of pedestrian crossings.



Site Analysis: 20th Street/Parkway Intersection

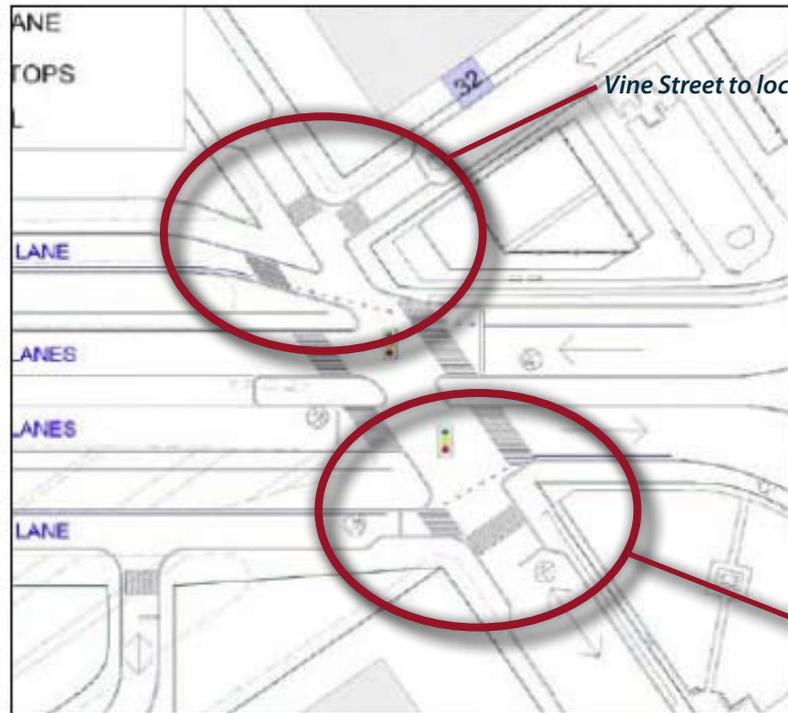
As with Eakins Oval, two distinct concepts have emerged from two separate efforts (again managed by the Philadelphia City Planning Commission and the Center City District) for improvement to the intersection of the Parkway and 20th Street (*see Figure 6*). These have resulted in very similar conclusions about appropriate design solutions to achieve the goal of pedestrian safety.

The main difference is that the PCPC concept is more aggressive in its treatment of traffic restrictions at westbound Vine Street and the eastbound Parkway (local lanes) as they approach the intersection, limiting these movements to right-turn only to achieve a simpler signal operation and more crossing time for pedestrians.

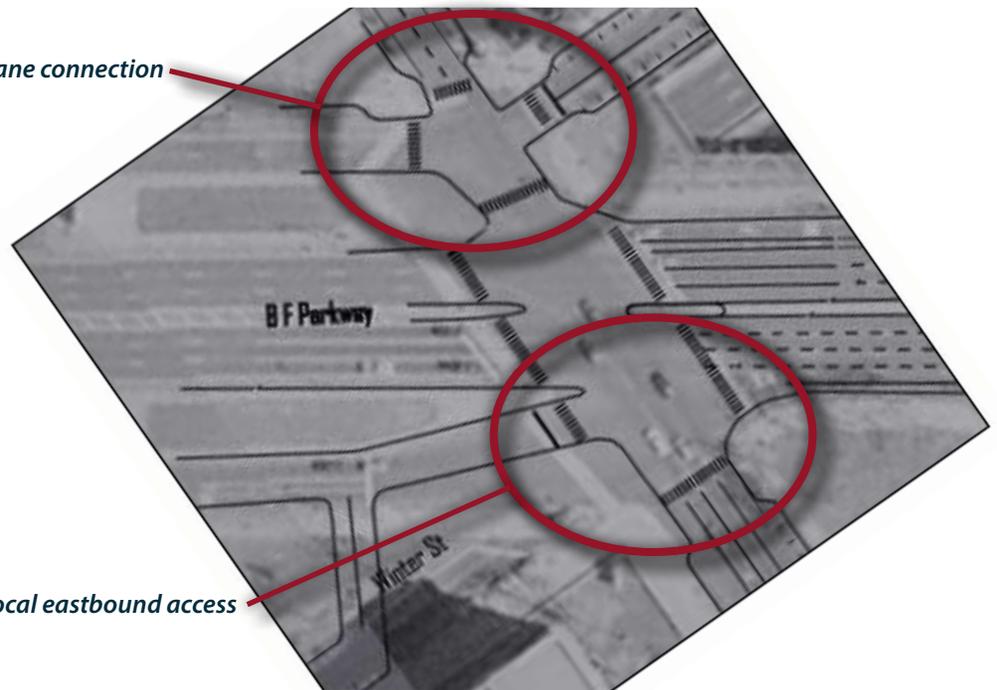
It is recommended that the Logan Square Plan support the implementation of one of these concepts as they are studied further and reviewed with the City Streets Department, mindful of the overarching objectives of reducing pedestrian crossing distances and improving visibility. It should be noted that any action here requires coordination with two separate additional efforts:

- The proposed Benjamin Franklin Parkway reconstruction which would affect the curb alignments on the west side of the intersection (*see Figure 4 for proposed cross-sections*).
- The programmed reconstruction of the overpasses of the Vine Street Expressway. If a final design for the 20th Street/Parkway intersection were complete prior to bridge replacement at 20th Street, the proposed changes to the intersection could be implemented as the bridge reconstruction is completed and the surface treatments restored, resulting in minimal cost and disruption of traffic for the combined projects.

It is important that LSNA monitor developments on this issue and participate in the redesign process to ensure adherence to the goals and objectives of the neighborhood plan, including enhancement of pedestrian safety and convenience.



Philadelphia Planning Commission Plan



Center City District Plan

Vine Street to local lane connection

Local eastbound access

▲ *Figure 6. Intersection Improvement at Pennsylvania Avenue and 20th Street.*

Demand Reduction

An important component to addressing the neighborhood's traffic issues is to strive to make Center City less traffic-dependent in the longer term, i.e. to establish the pedestrian, bicycle, and transit systems as stronger options for discretionary travel. These efforts must be sufficiently aggressive that a discernible percentage of local trips within the neighborhood and to/from other areas of Center City are shifted to these alternate modes, such that street capacity is freed up for trips having no practical modal alternatives.

A further benefit of this strategy is that it would provide residents with viable, safe options to car travel for a variety of shorter-distance trip-types.



▲ *Image 63. Bike lanes in Copenhagen*

Images 64, 65 & 66. Current bicycle and pedestrian conditions in the study area. ►



5.2.2 Parking

Potential parking improvement measures include actions that could increase the parking supply, decrease the parking demand, and improve the efficiency of existing parking spaces.

- Support the construction of a new institutional parking facility within the area bounded by 21st Street, Hamilton Street, Vine Street, and 18th Street. An appropriate location would be a multi-level underground parking facility beneath the new Free Library Expansion or under the new Barnes Museum, or as a potential redevelopment scheme for the Whole Foods site, incorporating parking serving mixed uses, including commercial uses on the ground floor.
- Replace underutilized traffic lanes with parallel parking lanes.
- Remove unnecessary parking restrictions on various blocks.

Potential Parking Spaces

- East side of the 100 block of N 22nd Street just below Race Street
- East side of the 200 block of N 22nd Street between Race and Winter streets
- North side of the 2100 block of Race Street just East of 22nd Street
- South side of the 1600 block of Arch Street
- North side of the 1600 block of Arch Street
- West side of 19th Street between Logan Circle/Race Street and Cherry Street

- Maximize the number of viable parking spaces (through re-striping and re-signing) on stretches of road where some parking is restricted.
- Incorporate car share into developing parcels as well as into areas currently not optimally served (i.e. gaps between car share pods of greater than three blocks).
- Create safer perceived connections between the northeast quadrant of the neighborhood (at Philadelphia Community College and the adjacent office buildings to the east) and nearby residential areas to encourage evening and weekend use of parking resources in these areas.
- Rehabilitate the deteriorated parking garage under JFK Plaza/Love Park, including installing new ADA-accessible elevators.

Overall, the parking situation can be isolated into four components: residential parking; business

parking; institutional parking; and special-event parking. Strategies for addressing these issues are partly physical and partly related to management. The recommendations described below include specific strategies for each component.

Residential Parking

The following are recommendations with respect to residential parking.

- Request that the City Streets Department review the site-specific on-street parking opportunities identified in **Figure 2-1**. (Several of the noted areas of parking restrictions are due to current or recent construction; attention should be focused on these to ensure they revert to the allowance of parking as soon as possible after construction is completed.)
- Maintain the existing parking-space-per-unit ratio (0.7) for new development with a maximum of one per unit (0.5 south of Vine and east of 20th streets).

- Encourage the use of car sharing through:
 - ◇ An “overlay” requirement of 1 car share space (any operator) per 50 units.
 - ◇ An allowance of a reduction of 4 general-use spaces for every car share space provided, within reasonable limitations and in coordination with other such City programs.

The purpose of the car share incentives would be to maximize the number of new residents who do not bring their cars (and associated parking/traffic demands) to the Logan Square neighborhood.

- Extend the provisions of § 14-1631 Off-street Parking Control District to the Logan Square Neighborhood - Parkway. The provision prohibits off-street parking for any one or two-family dwelling, unless accessed by a common driveway easement that serves three or more abutting properties. It thus prohibits garage-front townhouses, which eliminate an on-

street public parking resource. The provision currently applies to Center City south of Chestnut Street and the Northern Liberties area.

- Make available temporary visitor parking permits under the residential permit parking program.

Also, several concepts exist for a potential reconfiguration of Pennsylvania Avenue, intended to boost on-street parking by altering the alignment of traffic lanes and parking bays. Although the changes would be primarily outside the Logan Square neighborhood, they would extend as far east as 22nd Street and could provide an opportunity for the redesign of the 22nd and 23rd Street intersections.

Business Parking

- Business parking should continue to operate in its current manner, with garages in the Central Business District charging graduated rates for daily and hourly parking. However, the overall pricing structure should be reconsidered to reflect the recommendations of the Planning Commission's 2005 Center City Parking Policy Evaluation.
- In terms of neighborhood retail parking, it is recommended that new parking associated with the proposed business corridor on Callowhill Street be fitted with meters with a 2-hour time limit, effective until 6 PM.
- Adequate provision is required for business support service parking, including FedEx and UPS to minimize double parking problems.
- Though often tempting as a quick-fix solution, it is strongly recommended that creation of “temporary” surface parking lots through demolition of underutilized buildings not

be permitted. Such action strongly detracts from the consistency of the neighborhood's pedestrian-oriented character and could therefore lead to greater parking dependency in the longer term. Any durable solution to the neighborhood's parking challenges must be comprehensive and multi-modal in nature to ensure lasting effectiveness.



▲ *Image 67. Metered parking.*

Institutional Parking

- The Logan Square Neighborhood - Parkway Plan supports the main parking recommendations of the City Planning Commission's 2007 report Benjamin Franklin Parkway: Circulation, Parking, and Transit, including the construction of an underground parking facility beneath the Free Library expansion or the new Barnes Museum.
- Eakins Oval should no longer be used for unsightly surface parking, as the new institutional parking facilities should compensate for the current above-ground parking, which would enable this central public space and critical area in the Parkway viewshed to be used for ceremonial and recreational purposes.
- The proposed parking lanes along the Parkway should be for non-peak periods only and should largely serve institutional uses, consistent with the Philadelphia City Planning Commission's plan for reconstruction of the outer lanes to provide a safer environment for pedestrians and cyclists.
- Signage to parking facilities for visitors should

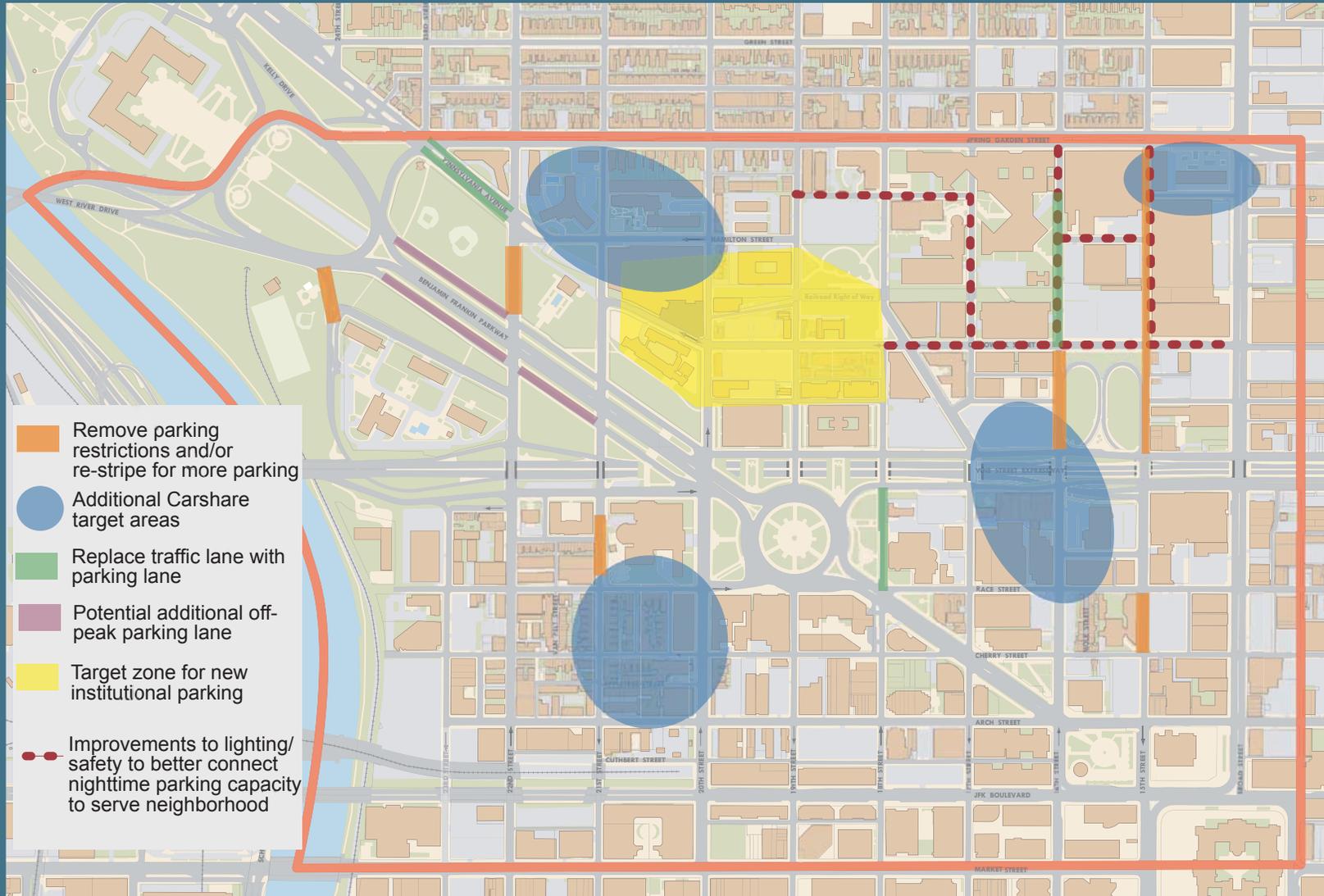
be improved with a coordinated program.

Special-Event Parking

- Philadelphia Community College has the largest available parking supply likely to be used on special-event days. Coordination, management, and improved wayfinding to the college's facilities could help alleviate the pressure on the on-street parking supply.
- The applicability of permit parking restrictions should extend to all days, including Sundays and holidays. Moreover, the "blanket" parking restrictions often implemented on event days on some streets should be revised to allow parking by residents with permits.
- Event-day enforcement procedures should be modified to ensure that local parking-permit holders should be permitted to bypass roadblocks to get to their homes.
- When appropriate, utilize the "No drive zone" policy similar to that used during Live 8.

Map 20. Proposed Parking Opportunities ►

PROPOSED PARKING OPPORTUNITIES



5.2.3 Pedestrians

In light of the overall goal of automobile “demand reduction,” one of the primary concerns with the street system should be to ensure that it is safe and comfortable for pedestrians, especially where this can be achieved without significant impact to traffic capacity. *Map 21* identifies a program of pedestrian enhancements that could help to fill the voids in the neighborhood’s strong historic walkable urban fabric.

Pedestrian system enhancements are aimed at balancing the needs of motorized vehicles, in terms of maneuverability and efficient flow, with the critical need for consistent pedestrian safety.

- Rebuild unsafe, traffic-centric intersections to “urbanized” standards.
- Add bulb-outs and tighten the curb radii along the Parkway (specifically at 21st and 22nd Streets) to slow the vehicle turning movements and shorten the pedestrian crossings.
- Add a four-way stop sign at 22nd and Cherry Streets, to slow traffic on 22nd Street.
- Prioritize traffic calming along Callowhill Street to support redevelopment goals; also prioritize traffic calming on streets with three or more traffic lanes and little/no protective pedestrian buffer, including 22nd Street south of Vine Expressway.
- Install a “thin buffer” at the curb where narrow sidewalks are not protected by parallel parking. Bike racks and honor box corrals can serve this purpose well.
- Advocate for a major overhaul of Eakins Oval (although this is not the best place for an underground garage as it would serve only PMA and would not prove to be cost-effective).
- Improve the intersection of 24th Street and Eakins Oval to eliminate the large radius, free flow right turn onto 24th Street. Create an urban intersection with a crosswalk to encourage pedestrian-access across 24th Street to the Schuylkill River Trail from the Parkway.
- Improve the intersection of 23rd Street and Vine Expressway off-ramp to eliminate the free

right turn and provide a crosswalk.

- Eliminate the right angle parking on Hamilton Street at 21st Street in front of the WaWa. (*Image 68*). The current location of the parking at the intersection causes congestion and has eliminated a section of the public sidewalk.

Traffic/Street design should accommodate vehicle movements in a “controlled” manner while establishing a consistent safety standard for pedestrians. Best practices to this end would include the following:

- Minimal curb radius to ensure slow vehicle turning movements and minimal length of pedestrian crossings. This minimal radius should assume that the designated design vehicle could encroach upon adjacent lanes and turn into the entire receiving area.
- Curb radius of zero (or as close as possible given site-specific drainage requirements) on corners that are not subject to turning movements.

- Bulb-outs across parking lanes wherever possible, to slow vehicle turn movements and shorten pedestrian crossings.
- Extension/maximization of all traffic islands where possible, to exert maximal control over traffic/turning speeds and to optimize the pedestrian refuge areas.
- Minimal cartway widths and elimination of excess (“unused”) paved space to better channelize—and improve the predictability of—vehicle movements.
- Crosswalks striped across all corners regardless of pedestrian volumes, partially as a visual traffic calming feature and partially to discourage unexpected pedestrian movements (i.e. jaywalking) that could otherwise take place.

See *Figures 10a, 10b and 10c* for prototypical examples of the application of these principles to several sample intersections. *Map 20* shows the location of these examples in the study area.

Additionally, the following opportunities should be explored with respect to traffic signals:

- Install pedestrian countdown timers on all crossings greater than three uninterrupted traffic lanes (specifically, the Benjamin Franklin Parkway, JFK Boulevard, Market Street, and Broad Street).
- Evaluate the potential for incorporating non-peak-period pedestrian-only signals at crucial locations where geometric considerations make it difficult to control the speeds of turning traffic.

The pedestrian environment on the sidewalk also needs protection from encroachment by physical obstructions, such as vending, street furniture, etc., and incursion by vehicles, in the form of driveways and lay-by lanes. Finally, there should be a strong emphasis on improving the overall level of pedestrian connectivity throughout the area. To this end, the potential for adding ramp and stair connections

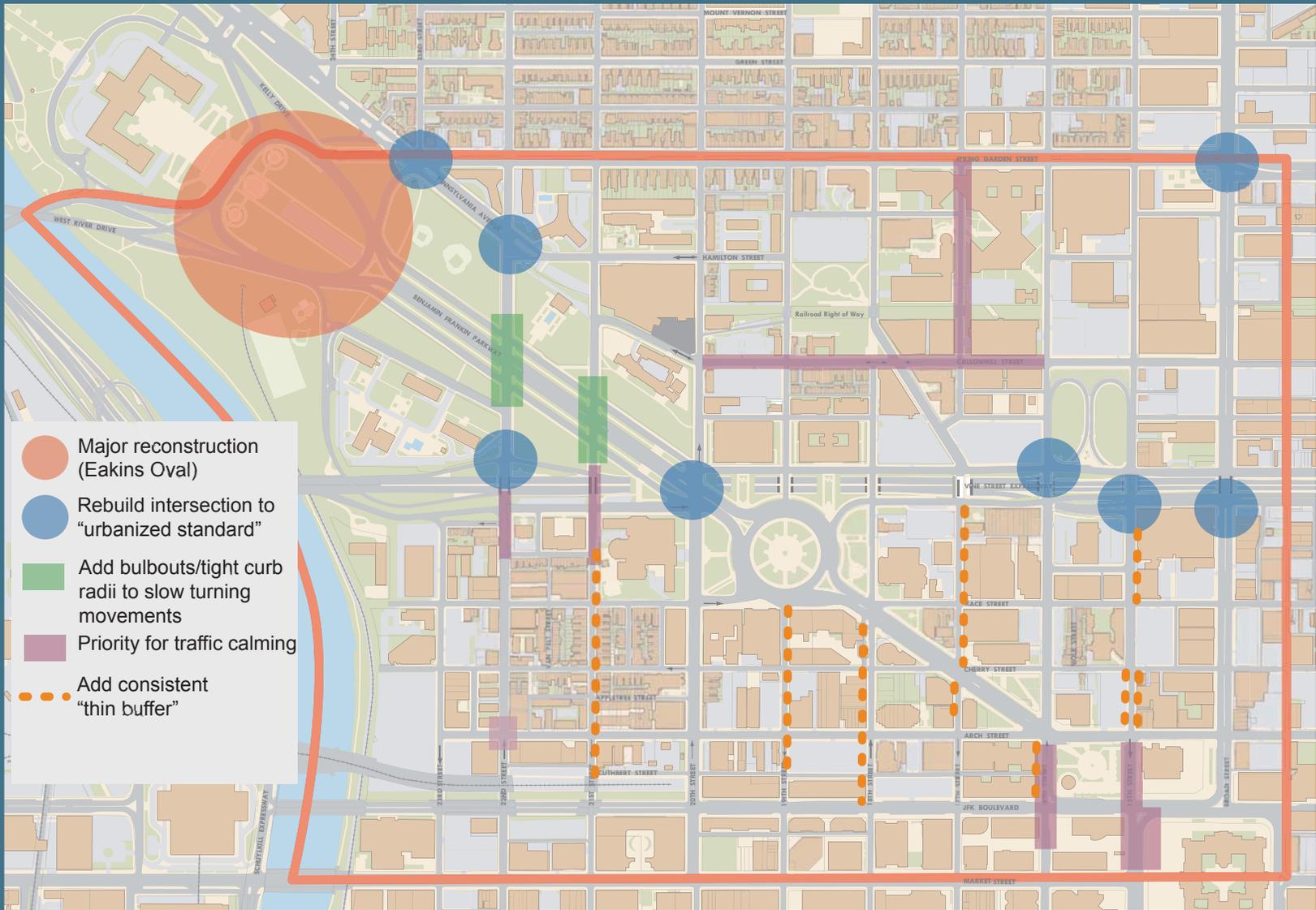
to/from JFK Boulevard at 21st, 22nd, and 23rd Streets should be evaluated, as well as additional opportunities for safe street crossings in the Art Museum area.

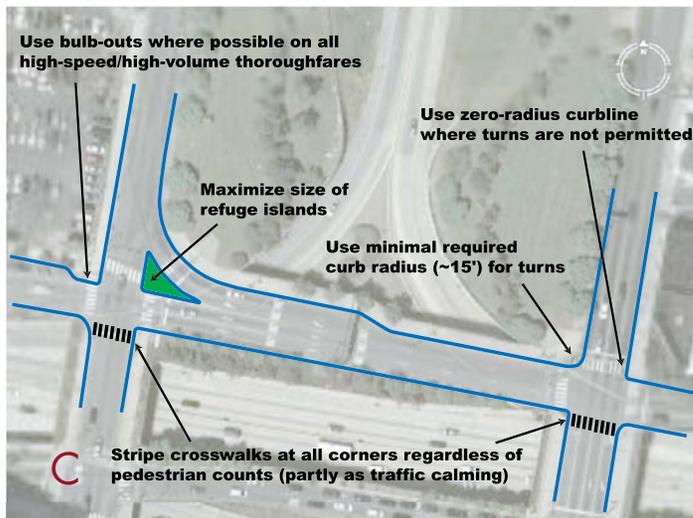
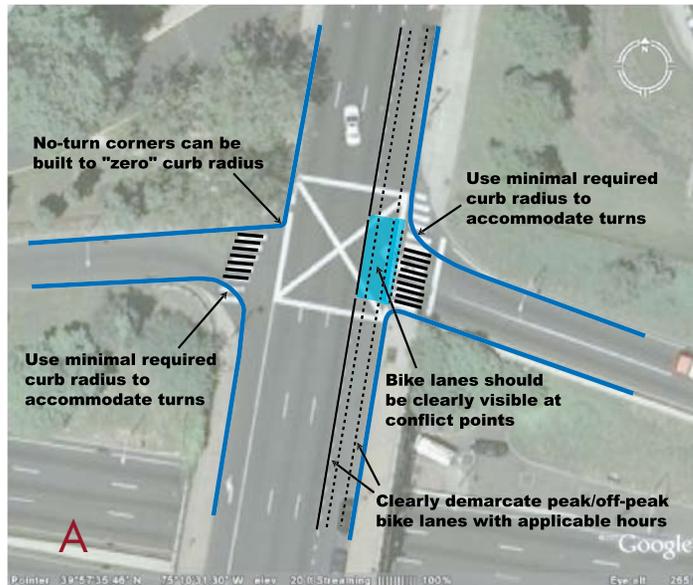


▲ *Image 68. Right Angle Parking at Hamilton and 21st Street impedes pedestrian sidewalk*

Map 21. Proposed Pedestrian Enhancements ►

PROPOSED PEDESTRIAN ENHANCEMENTS





◀ **Figures 8a, 8b and 8c. Intersection examples for Pedestrian Improvement: Clockwise from left: 22nd at I-676; 22nd at Pennsylvania Avenue; Vine Street at 15th and 16th Streets.**

5.2.4 Bicycles

The current bicycle system incorporates bike lanes into the neighborhood, but these have historically been limited to streets where they could be comfortably fit and thus suffers from the several deficiencies, including “gaps” where streets become too tight for bike lanes and awkward transitional geometries, particularly along the Benjamin Franklin Parkway.

The proposed bicycle system enhancements shown in **Map 22** are aimed at filling “gaps” in the existing network, and are focused primarily on streets where such enhancements are achievable given the available roadway width:

- Complete the bicycle lanes on 22nd Street as the primary northbound connector.
- Install bicycle lanes on 20th Street south of the Benjamin Franklin Parkway as the primary southbound connector, with 19th Street providing southbound circulation above the Parkway.

- Establish the Pennsylvania Avenue/Callowhill Street corridor as a contiguous neighborhood-serving east-west alternative to the Parkway, with Callowhill Street as a shared, heavily-traffic-calmed “destination” zone.
- Correct the existing problematic transitions—primarily along the Parkway—by revisiting their geometric alignments and increasing their visibility.
- Relocate Parkway bike lanes to the outer, traffic-calmed sections of the Parkway.
- Consider the installation of off-street bicycle pathway connections along the north/east edges of Love Park and the northern apron of City Hall Plaza, both of which consist of underutilized spaces.
- There is potential width for supporting northbound bicycle lane (feeding the Parkway) on 16th Street by eliminating the existing exclusive right-turn lane to Arch Street.

Image 69. Insufficient bicycle parking capacity 



- Add substantial bicycle parking capacity at Callowhill Street (to support the proposed neighborhood-scale retail), the Van Colln ballfields, and the Spring Garden subway station.
- Install combined bicycle parking/rental facilities at Race Street/Schuylkill River Park, Logan Square at the Free Library, and JFK Plaza/Love Park.
- Clearly mark (and maintain) specially colored bicycle lanes at all conflict points, i.e. wherever motor vehicles regularly cross the alignment of the bicycle lane.

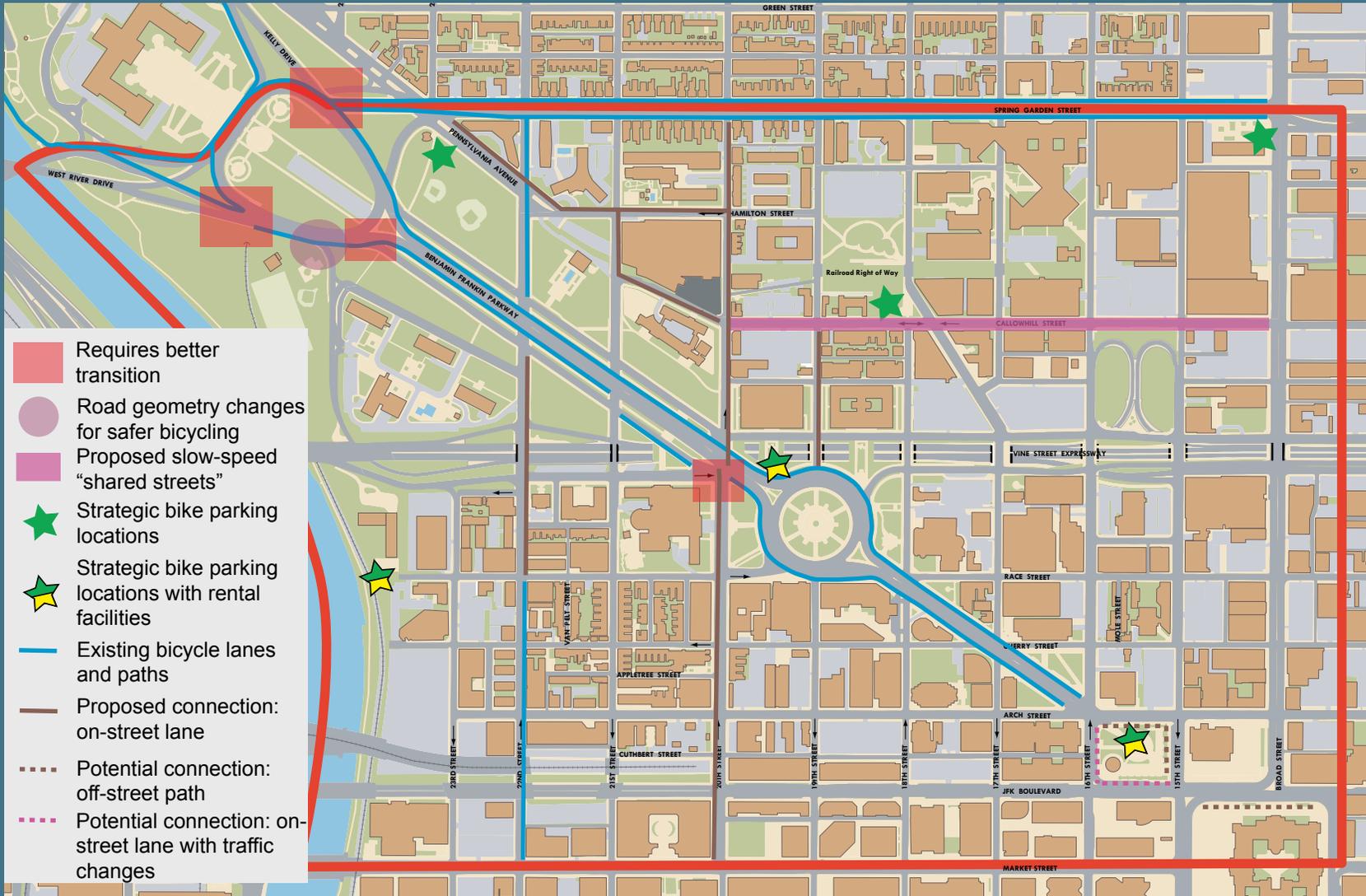
The bicycle system recommendations represent actions that may be do-able, with some adjustment of traffic patterns except where indicated in *Map 22*. In the longer term, these actions could be further complemented with aggressive implementation of a “Green Streets” program with supporting routes. However, virtually all remaining routing options except those indicated would require the elimination of either a traffic lane or parking lane in order to create sufficient width for a continuous bike lane.

It should also be noted that tour/school buses often idle in bike lanes when staging for pickups from institutions and hotels; fixing this issue requires better coordination and control of bus access patterns.

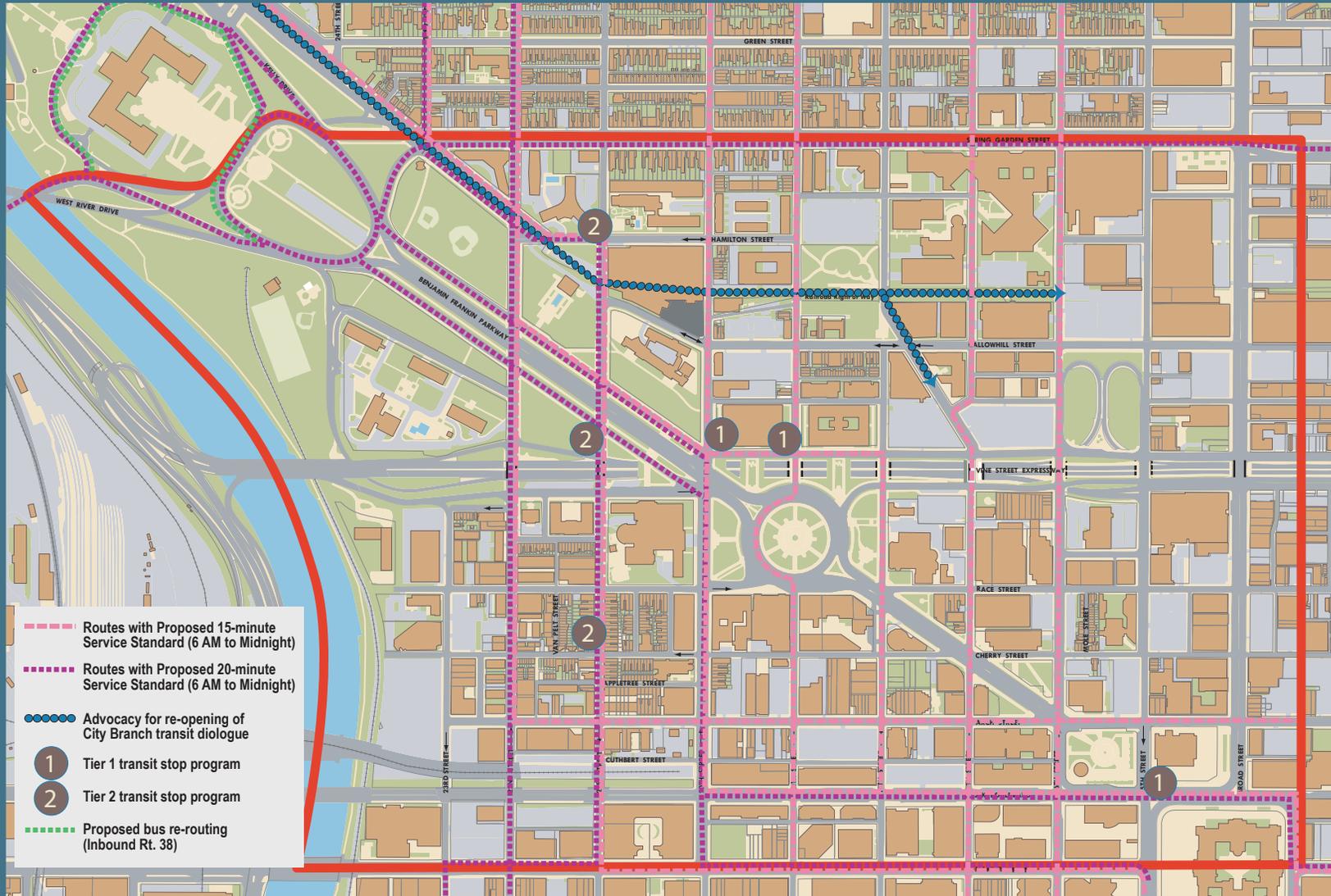
Also shown in *Map 22* is a proposed “shared” bicycle zone on Callowhill Street, with an emphasis on traffic calming at locations where the addition of a bike lane would be impractical due to physical constraints and/or alternative planning goals (i.e. the establishment of a two-way, retail-oriented destination).

Map 22. Proposed Bicycle System Enhancements 

PROPOSED BICYCLE SYSTEM ENHANCEMENTS



PROPOSED TRANSIT SERVICE ENHANCEMENTS



5.2.5 Transit

The primary recommendations for transit shown in **Map 23** include service enhancements, infrastructure investment, and informational improvements:

Service Enhancements

Given the nature and growth of the Logan Square neighborhood and its surroundings, the LSNA plan should call for a review of SEPTA service levels with the following potential improvement measures:

- Peak service targets of 10 minutes or better on Routes 17, 32, 33, and 48, and 20 minutes on Routes 7, 38, and 43.
- Off-peak service targets of 15 minutes (consistent frequencies) on all main routes connecting the Logan Square neighborhood with the central business district (Routes 17, 32, 33, and 48). This minimal “service standard” would also apply to Saturdays and Sundays.
- The principle of “clockface” schedules for all

routes during off-peak periods, i.e. such that all buses on a particular route would pass a certain stop at standardized arrival times; for instance, :07, :27, and :47 past each hour.

- Replacement of 40-foot buses with 60-foot articulated vehicles where feasible, which could potentially ease overcrowding on Routes 17 and 32.

These types of enhancements would require advocating for a transit-planning policy change from a “reactive” demand-responsive paradigm to a “proactive” demand-generating paradigm. The actions most dependent on this type of evolution include:

- Negotiating for a 15-minute “service standard” on radial routes 2, 32, 33, and 48, with minimum service frequencies of fifteen minutes every day (including weekends) between 6 AM and midnight.
- Negotiating for a 20-minute service standard on peripheral/supporting routes 7, 38, and 43.

Map 23. Proposed Transit Service Enhancements

While SEPTA's budget is limited, recent support for the agency has opened the door to the potential for system-wide enhancement. Meanwhile, Mayor Nutter's commitment to sustainable design provides further impetus for shifting to a more proactive stance with respect to transit planning. The purpose of establishing consistent, minimum baseline "service standards" on all routes would be to actively draw people to the service, and also potentially to influence the basic lifestyle decisions (i.e. car or no-car) of new residents.

A final service issue is the ongoing operation of the Phlash Shuttle along the Benjamin Franklin Parkway. While this will likely continue to be a tourist-oriented daytime shuttle for the next three years due to its funding stream, there may be a subsequent opportunity for it to evolve into a more multi-dimensional transit-type service with benefit to residents as well as visitors. As this opportunity presents itself, it is important that the Logan Square neighborhood participate in discussions about Phlash's future, advocating key usability elements such as minimal service hours and fare/Smartcard compatibility.

Private shuttle services should be encouraged to work together to coordinate routes and schedules, to reduce the overall number of shuttles in the area and/or to provide a more consistent service.

Infrastructure Investment

Infrastructure enhancements would consist of the following recommendations:

- Advocate for the use of the City Branch right-of-way as part of a high-frequency premium transit corridor (*Image 70*).
 - Advocate for frequent, year-round transit service to access and link the Parkway institutions, the Please Touch Museum, the Philadelphia Zoo, Boathouse Row, Penns Landing, Independence Hall, and other tourist attractions along this route.
 - Establish “Tier 1” transit stops at key locations, with the following amenities: substantial shelters, effective lighting, seating for four or more persons, and real-time information.
 - Establish “Tier 2” transit stops at other important key locations, with the following amenities: shelter, effective lighting, and seating for two or more persons.
- Ensure that all transit stops include at least the following basic elements: clear signs identifying the route; adequate lighting and posted timetables and route maps (pole- or shelter-mounted).
 - Increase the “reach” of the Race-Vine subway station by ensuring that Smartcard-operated “rotogates” are installed as part of the planned upgrade of SEPTA’s fare collection system, which would allow access into the station from Race Street as well as Vine Street.
 - Support efforts to remake City Hall Station and Dilworth Plaza into a more welcoming gateway, using environmentally sustainable design principles.

With respect to potential premium transit service along the City Branch right-of-way, it should be noted that rapid development of the Logan Square neighborhood and West Fairmount Park—as well as the Art Museum and Free Library expansion projects and relocation of the Barnes Museum—have

increased the potential viability of a dedicated transit service along this route as a Center City “Culture Express”. LSNA should support a reexamination of this issue.

It should also be noted that this right-of-way (*see Map 23*) has significant historical value in that it was used as a direct link into the iconic Baldwin Locomotive Works on Broad Street. The potential reinvention of this corridor as a passenger route could derive added cultural benefit from incorporation of historic rail/industrial heritage elements into its stations. Intermodal connections to the Broad Street Subway and with bus service will be an important consideration.

Informational Improvements

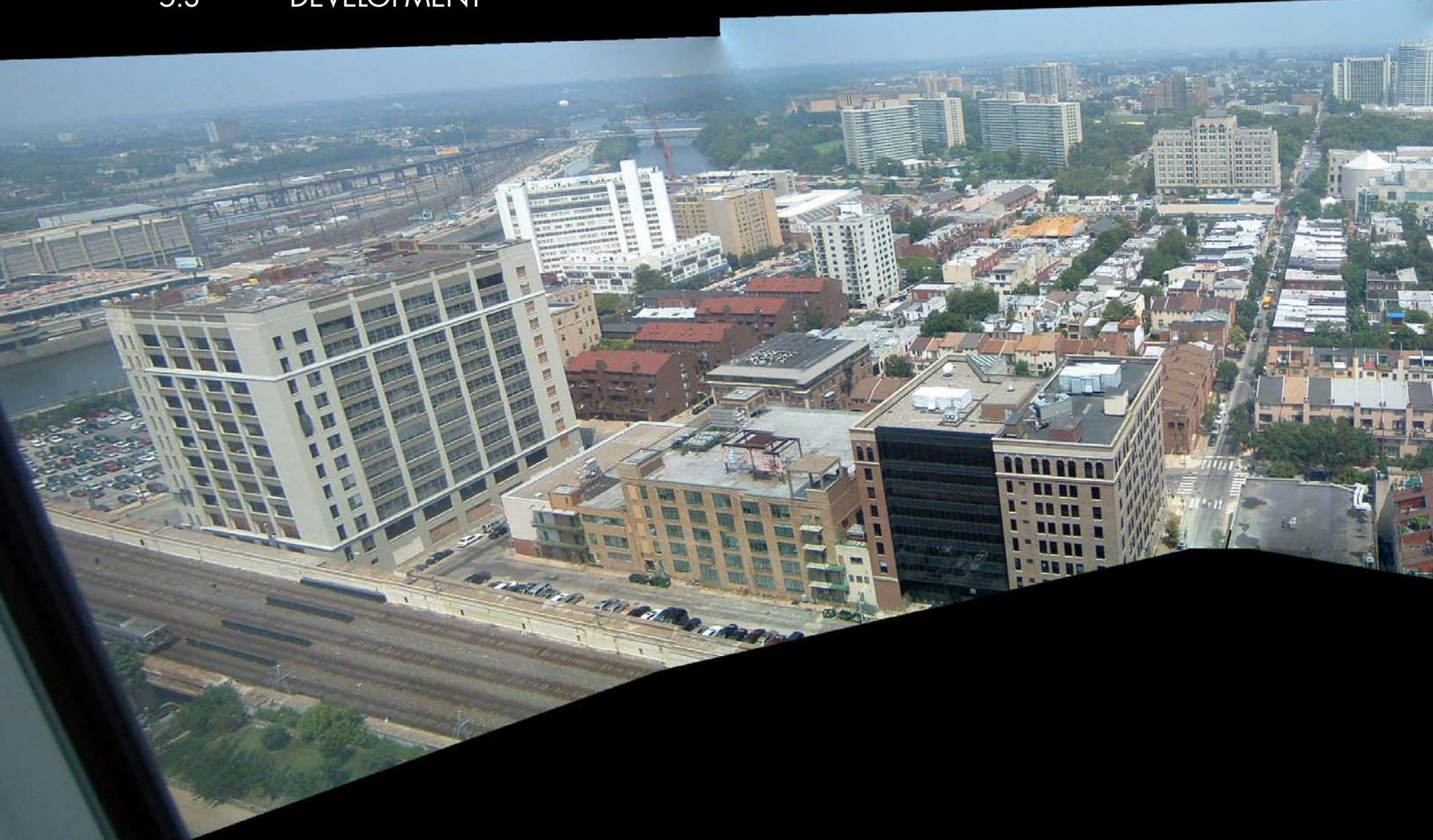
Finally, there are a number of basic informational improvements which could improve the convenience of using the current bus system, in particular:

- All bus stop signs should include the “STAR location number” which allows people to make a telephone call to an automated system to obtain an estimate of when the next bus will arrive.
- The automated system allowing people to call about their bus’ status should be simplified and made available 24 hours/day, 7 days/week.
- Bus detours for events should be consistent and well-advertised. The detour route for buses 32 and 38, for instance, could follow Spring Garden Street and 19th/20th Streets for all events, rather than the ad hoc detours representative of the current system dependent upon which streets are closed for which specific events. Permanent signs showing “Bus 32/38 Detour Route” could be installed given a consistent detour plan, along with signs along the regular routes indicating the event-day detour routes.

Image 70. City Branch Right-of-Way offers the opportunity for a new culture-oriented transit service. 



5.3 DEVELOPMENT





5.3.1 Neighborhood Conservation

Neighborhood Conservation Districts provide for control over the alteration of facades of existing buildings, and the design of new buildings in the District, to preserve the aesthetic fabric of distinctive Philadelphia neighborhoods. The controls affect the appearance of buildings, not the density of development, which is governed by the underlying zoning.

Consideration should be given to the adoption of a Neighborhood Conservation District for the historic core of Logan Square, within the area generally bounded by 20th, 23rd, Winter and Arch Streets, and/or other areas that meet the requirements of the City ordinance, to help maintain the character and quality of the residential district. A conservation district would involve the creation of community-based design guidelines developed in conjunction with the City Planning Commission. The design of any proposed exterior alterations to existing buildings, or design of new buildings in the district, would be subject to Planning Commission review for compliance with the guidelines, and issuance of a “Certificate of Compliance” prior to issuance of

a building permit. Some residents of the Parkway North area had suggested that a Neighborhood Conservation District should also be established for the residential area north of the Parkway, but the area does not seem to include the consistent physical characteristics required for a Neighborhood Conservation District.

To create an NCD, the Logan Square Neighborhood Association will need to submit a petition adopted by the Board to the Planning Commission requesting that they initiate the process. The Planning Commission then has up to 6 months to work with LSNA to establish the NCD boundaries and develop design guidelines, and an enabling ordinance. After a public meeting within the neighborhood, the ordinance would be forwarded to the District Councilperson, for introduction into Council. Before the council public hearing, the City Clerk’s office is required to inform the owners of all properties within the NCD boundaries. The ordinance would not be enacted if more than 51% of all property owners file statements of opposition to the designations.

Image 71. Overview of Potential Neighborhood Conservation District
Photo source: Phillyskyline.com 



5.3.2 Historic Resources

1. *Survey of Historic Resources for Local Register Listing*

A survey of potential additional historic resources within the Logan Square-Parkway district should be completed, to determine eligibility for listing in the Philadelphia Register of Historic Places. Listing would encourage preservation of existing low-rise historic buildings in the neighborhood that might otherwise be candidates for demolition and development. Examples of buildings that should be evaluated for local listing include the 4-story loft buildings at 1926-36 Arch Street and the 4-story townhouse building at 1924 Arch Street. 1924 Arch Street is already listed on the National Register. Historic listing of buildings on this block could provide the basis for an integrated development of the block that would include both new and existing buildings. Preservation of these low-rise structures could encourage a development that maintains views to the north from Kennedy House, while also permitting a development that is consistent with existing base zoning density.

2. *Transfer of Development Rights for Historic Preservation*

The C4, C5 and RC4 zoning districts permit additional floor area, on a one for one basis, for additional development rights transferred from a locally certified historic building, located in these zones. However, this provision has not been utilized. To qualify, the historic resource must have been designated by the Historical Commission as “threatened.” Currently, the Commission has no procedure for such a designation. The Planning Commission should develop detailed regulations to clarify this provision and encourage its use, as this transfer of development rights can potentially support the work of the Historical Commission by providing additional protections to historic resources.

The “threatened” designation requirement should be eliminated from the code. Designation of additional buildings as local historic resources would also potentially increase the number of buildings eligible for transfer of development rights. A land bank of develop right should be established, perhaps through the Preservation Alliance, so that such rights are available for purchase as needed by developers.

Image 72. Example of Historic Resources to be Surveyed ►



5.3.3 Zoning Map

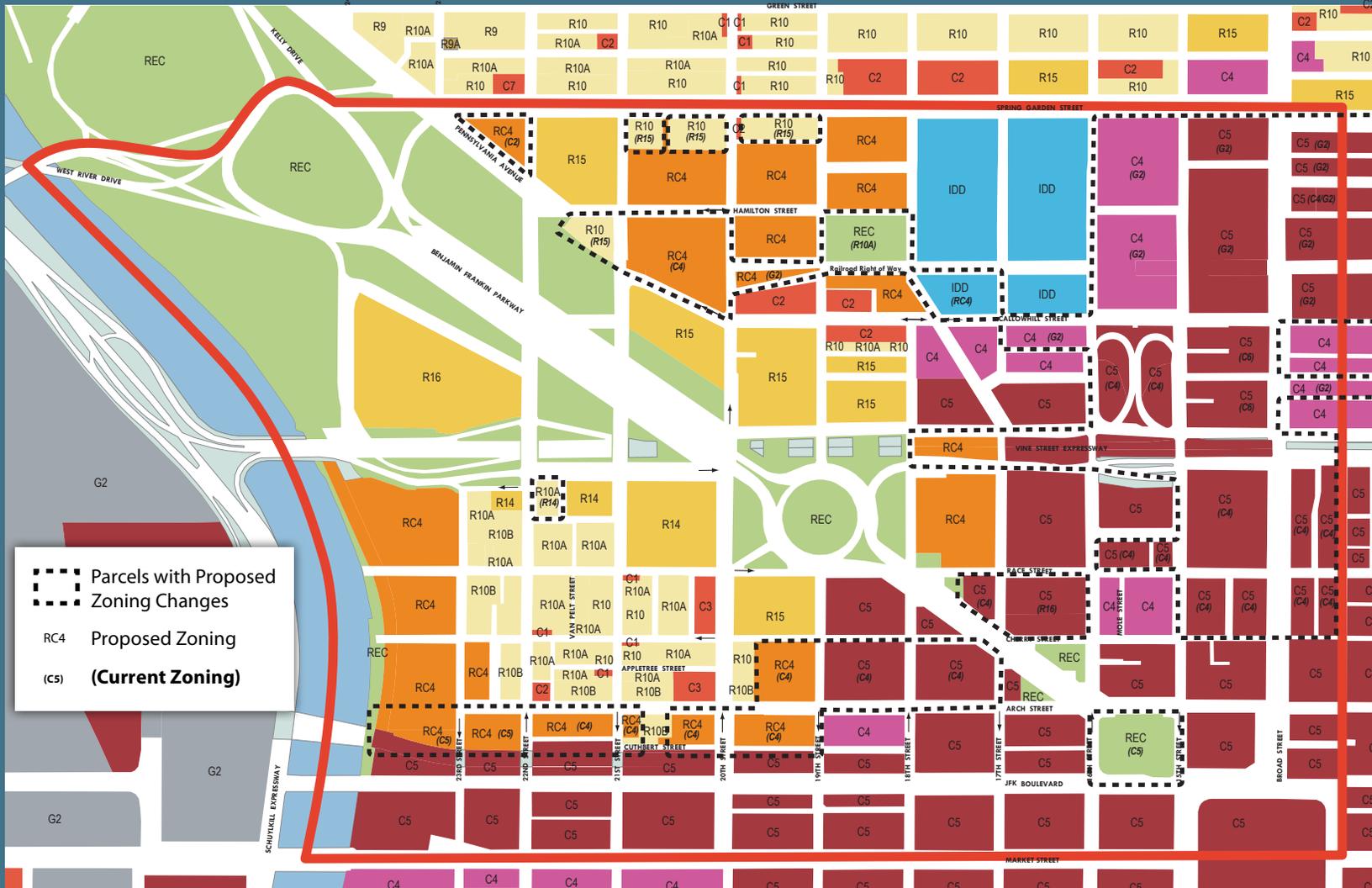
The Logan Square Neighborhood - Parkway zoning map is generally consistent with existing and proposed land use. Zoning supports high-rise, high-density office and mixed-use development along the east-west spine of Market Street and Kennedy Boulevard, and in the area between 18th and Broad Street, north of Kennedy Boulevard. It supports medium to high-density residential and mixed-use development adjacent the Parkway, north of Vine Street and west of 18th Street, in the Parkway North area. It supports low-rise townhouse development in the historic core of the Logan Square residential neighborhood, between 20th and 23rd Street and between Winter and Arch. However, there are some areas of inconsistent and inappropriate zoning that should be changed to reflect desired development goals. Where map zoning changes are proposed, they should serve a broad public interest that encompasses the “general welfare” of the population, and should be consistent with overall neighborhood planning goals.

Instead, the plan recommends changes that impact the types of permitted uses, eliminate obsolete zoning, reduce bonus density provisions or change bulk controls. These areas where changes are proposed include the following:

1. The parcels fronting the south side of Arch Street north of the SEPTA tracks, between 20th and Schuylkill River, currently zoned C4, should be rezoned to RC4, similar to the parcels west of 23rd Street, to create a medium to high-density residential transition between the historic townhouse core of Logan Square and high-rise high intensity commercial development along JFK Boulevard and Market Street.
2. The G2 industrially zoned parcels, located between Callowhill and Spring Garden and between 16th and Broad Streets, should be rezoned C4, to support medium-intensity commercial development along the Broad Street corridor north of Callowhill Street.

Map 24. Proposed Zoning Changes ►

PROPOSED ZONING CHANGES



 Parcels with Proposed Zoning Changes
 RC4 Proposed Zoning
 (C5) (Current Zoning)



3. The block between 20th and 21st Streets, Hamilton Street and Pennsylvania Avenue, currently zoned C4, should be rezoned RC4, consistent with the adjacent Franklin Town development parcels, to encourage residential development, with supporting commercial uses. This would not result in an increase in allowable density. The residents of the high-rise City View Condominiums would like the block to be down zoned to C2, low-density commercial, with a 35' height limit to assure protection of their uninterrupted views to the Center City skyline to the south. However, such a change would not be supported by the city. Appropriate bulk controls with the proposed RC4 zoning could limit impacts on views.
4. Franklin Town Park, currently zoned R10A, should be rezoned Recreation, to reflect its use as permanent open space.
5. JFK Plaza/Love Park, currently zoned C5, should be rezoned Recreation, to reflect its use as permanent open space (with underground parking).
6. Coxe Park, currently zoned R10A, should be rezoned Recreation, to reflect its use as a permanent open space.
7. Parcels along the Broad Street corridor should be rezoned to higher intensity C5 to encourage highest-intensity development in this area close to transit.
8. The City-owned Boy Scout headquarters buildings at 22nd and Winter Streets should be rezoned from R14 to R10A to protect this publicly-owned historic resource.

Image 73. Whole Foods Parking Lot at 20th/Pennsylvania Ave 



9. The 1900 and 2000 blocks of Spring Garden should be rezoned from R15 to R10 to reflect the existing 45' height limit and the fact that both blocks are within the Spring Garden Historic District.
10. The small triangular parcel and air rights parcel at 21st and Hamilton Streets behind the Rodin Museum should be rezoned from R15 to R10, to limit development on this small sensitive site.
11. The G2 parcel on 19th Street that includes the Granary Building could logically be rezoned to RC4. Since the parcel is already developed with a multi-story structure, rezoning would not result in additional development on this site. G2 does not reflect existing or likely future use. However, the adjacent residents have proposed that the parcel be rezoned C2, to assure that any future development is limited in height.
12. Possible rezoning of the north side of the 1800 and 1900 blocks of Callowhill Street from C2 to RC4 was discussed. The C2 zoning reflects

former 3-story commercial structures that occupied these blocks. Most have since been demolished, and there are many vacant lots. The current zoning no longer reflects existing land use. Such a rezoning to RC4 could encourage higher density mixed-use development with first floor retail that could help invigorate this neighborhood retail corridor. While this proposal is supported by the Planning Commission, it is opposed by residents in the vicinity, who would like to retain the 3-story height limit and low-density commercial development on this section of Callowhill Street.

Proposed zoning map changes are included on Map 24.

Some residents in the Logan Square-Parkway area had suggested that mandatory Transfer of Development rights be used as a tool to reduce permitted potential development density in certain areas zoned for high-density, such as Arch Street and the North Parkway area. However, Transfer of Development Rights is only appropriate where there is a significant broad public benefit served by the transfer provisions, such as the preservation of historic resources, or valuable farmland, or other public amenity.

5.3.4 Specific Zoning District Controls

The specific zoning district controls for several of the zoning districts that affect the Logan Square Neighborhood - Parkway are inadequate to assure that new developments enhance the community and minimize adverse impacts on the established residential community. The neighborhood believes that some of the zoning classifications lack adequate controls on density, building bulk, and building spacing. It is likely that within the next 5 years, the existing zoning classifications will be modified, under zoning reform. However, in the interim, a zoning overlay should be established for the Logan Square neighborhood that incorporates specific controls only applicable to the Logan Square neighborhood, while eliminating the existing blanket 125' height limit provision in some areas as described below. The Planning Commission does not support this recommendation at this time, but is committed to an ongoing discussion with community members about development in the area. However, in this case the overlay would be the most effective short-term way of addressing community concerns over development density and bulk.

The following changes to specific district controls are proposed.

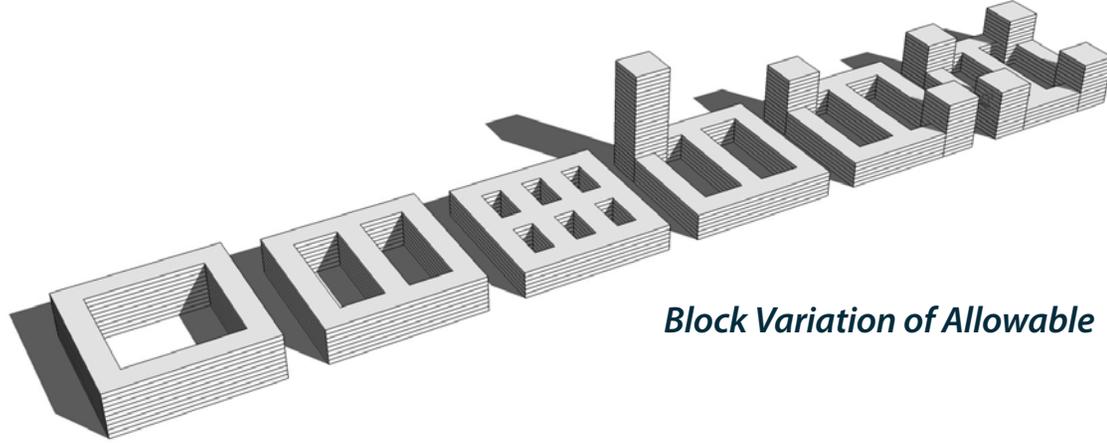
1. Excessive Additional FAR (Floor Area Ratio) Provisions in C4 and RC4 Zoning Classifications

The C4 and RC4 commercial and mixed-use zones in Logan Square function as a transition from the high-intensity C5 zoned office core, to the residential neighborhoods. The base FAR in the C4 and RC4 zones is 5, compared to 12 in C5. However, additional floor area of 8 FAR is granted in the C4, RC4, and C5 zones, for developments of larger parcels that meet specific requirements as outlined in the code and include public open space and other amenities. While a bonus FAR of 8 represents a 66% increase in FAR in C5, it represents an excessive 160% in C4 and RC4 zones. The base FAR of 5 in the C4 and RC4 zones provides an appropriate density of development to function as a transition from the C5 zone to townhouse residential neighborhoods and discourages encroachment of Center City office core functions on the residential neighborhood. The additional FAR provisions should be significantly

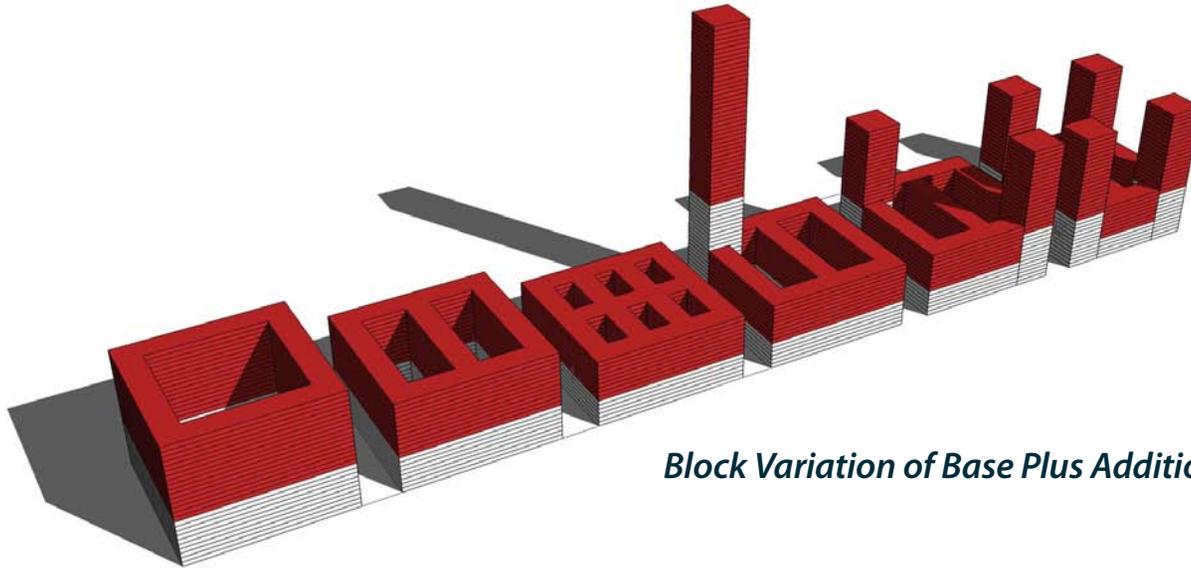
reduced for the RC4 zones in the Logan Square neighborhood. The additional FAR should be limited to an FAR of 2, representing 40% of the base FAR. The additional FAR in the C4 zone should also be limited to an additional FAR of 3, representing 60% of the base FAR. A maximum FAR of 7 (RC4) or 8 (C4) would be consistent with the maximum FAR achievable under the existing 125' height ordinance. The Planning Commission does not generally support reductions in the bonus provisions of the code. However, the River City proposal clearly illustrated the potential adverse neighborhood impact of development built to a FAR that includes all available density bonuses.

These changes should be addressed in the neighborhood overlay zone.

Figure 9. FAR 5 vs. 13 Diagrams 



Block Variation of Allowable Base Floor Area Ratio: FAR 5

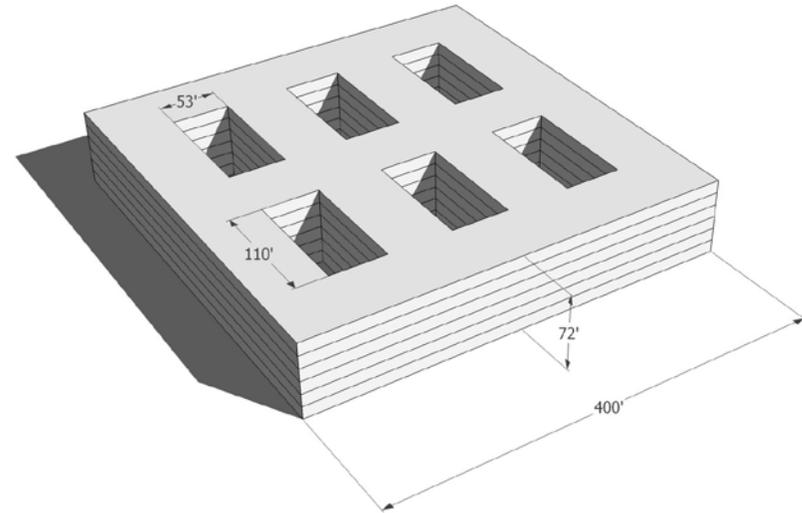


Block Variation of Base Plus Additional Floor Area Ratio: FAR 13

FAR 5

6 story
53' x 110' courtyards

A typical 400'x400' city block can be developed at FAR 5 with 6-story, 60-foot deep apartment buildings, facing public streets and small interior courtyards. This type of development is common in many historic European cities, such as the Medieval core of Barcelona, but is also found in the U.S., in areas such as the tenant blocks of the Lower East side of New York. There is a limited amount of open space, and views, sunlight, and daylight are restricted, but the pattern of development encourages active street life.



▲ Figure 10. FAR 5 Diagram: 6 Stories with Courtyards



▲ Image 74. Aerial of Medieval Barcelona, Spain



▲ Image 75. Ground level images of Medieval Barcelona, Spain

FAR 5

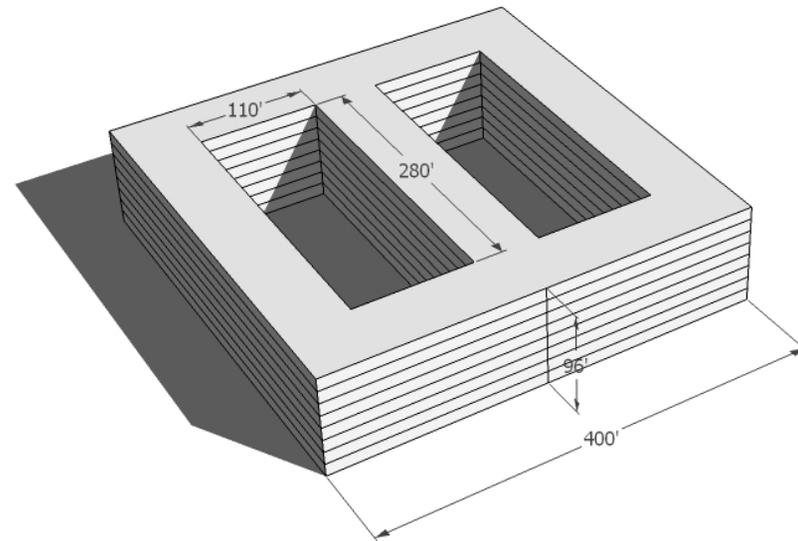
8 story

110' x 280' courtyards

The same city block developed at FAR 5 with 8-story, street facing apartment buildings offers the opportunity for more generous courtyards and open spaces, allowing greater penetration of light and air. However, views are limited, and the 8-story buildings require wider public streets to allow the penetration of daylight and sunlight. This type of development is typical of the 19th century Eixample district of Barcelona, where 7-8-story buildings front wide boulevards, but is also typical of new apartment developments in Seattle.



▲ Image 76. Aerial of L'Eixample, Barcelona



▲ Figure 15. FAR 5 Diagram: 8 Stories with Courtyards

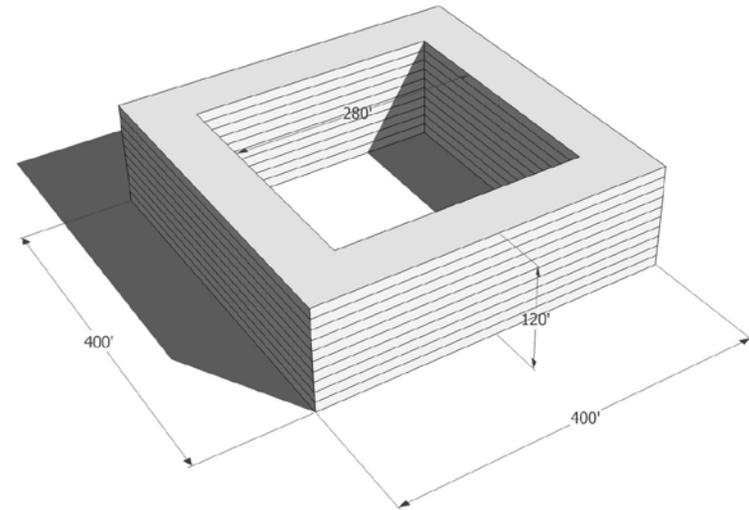


▲ Image 77. Image of L'Eixample district, Barcelona

FAR 5

10 story
280' x 280' courtyard

Ten-story street facing apartment buildings provide the opportunity for significantly larger semi-private courts that can promote the opportunity for a wide variety of activities, and good daylight and sunlight penetration. However, 10-story buildings require significantly wider streets – up to 100' wide, to assure adequate daylight and sunlight penetration. This type of development is typical of Washington, D.C.



▲ Figure 11. FAR 5 Diagram: 10 Stories with Courtyard



▲ Image 78. City Vista Development, DC



▲ Image 79. City Vista Development, DC

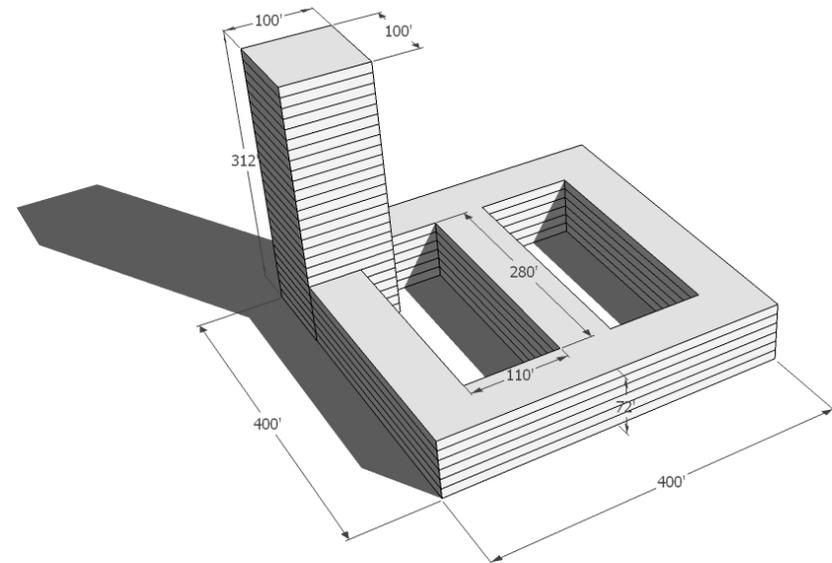
FAR 5

6 story podium
One 26 story tower

An FAR of 5 can also be achieved with a combination of a low-rise 6-story residential/mixed use podium and a small footprint (100'x100') high-rise resident building, 26-stories high. This configuration provides an attractive low-rise streetscape that can work on Philadelphia's narrow streets, but also provide the opportunity for widely spaced elegant high-rise towers that can offer views across the city, without blocking sunlight and daylight to adjacent low-rise buildings. This type of development is common in cities such as Vancouver, BC.



▲ Image 80. Aerial of Vancouver, CA



▲ Figure 12. FAR 5 Diagram: 6 Stories with Tower



▲ Image 81. Ground level images of Vancouver, CA

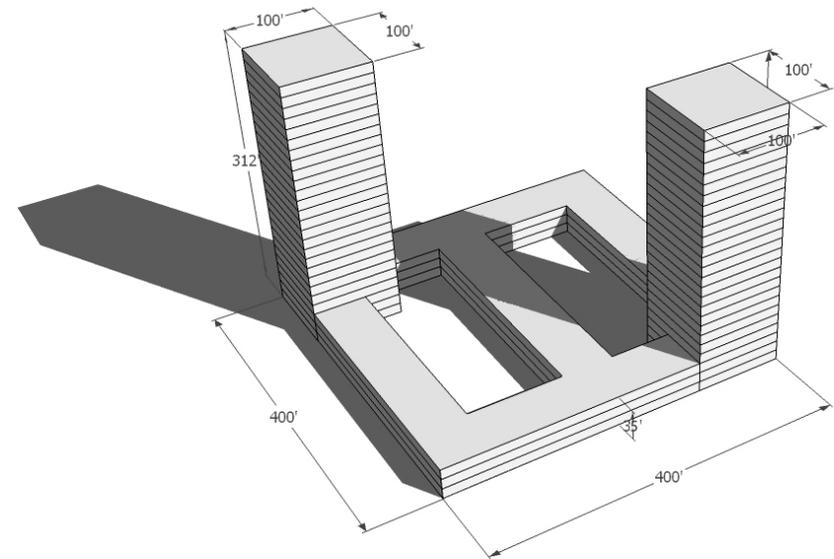
FAR 5

3-story townhouse podium
Two 26-story towers

An FAR of 5 on the same block can be achieved with a combination of 3-story single-family townhouses, and 2 small footprint residential apartment buildings, 26-stories high. This configuration provides the opportunity for both an attractive low-rise streetscape, with plenty of sunlight and daylight access, as well as for individual townhouses with private yards, in place of apartments. Widely spaced small footprint high-rise buildings, promote the opportunity for views, without creating adverse shadow impacts on adjacent low-rise development.



▲ Image 82. Aerial of Vancouver, CA



▲ Figure 14. FAR 5 Diagram: 3 Stories with 2 Towers



▲ Image 83. Ground level images of Vancouver, CA

2. *Lack of Adequate Building Bulk Controls in C4, RC4, and C5 Zones*

There are no height limits in the C4, RC4 or C5 zoning classifications. Under the existing code provisions, building footprint, setback and building spacing requirements are provided only for lots fronting the south side of JFK Boulevard, between 15th Street and the Schuylkill River, and south side of Market Street between 15th Street and 24th Street. On both the south side of JFK Boulevard and the south side of Market Street, the width of any building over 65' high is limited to 250', or 66% of the block frontage, whichever is less. The minimum spacing between buildings over 65' high is 75 feet. These requirements were established to maintain sunlight penetration to the sidewalks of Market Street and JFK Boulevard. These requirements do not apply to the north side of the street, and thus do not address daylight penetration to the street. While the requirements would prevent a block-long high-rise building such as Kennedy House on the south side of the street, they would not prevent additional such block-long buildings on the north side of the street, with consequent potential adverse impacts on

daylight access (*Image 80*).

While the existing RC4, C4 and C5 regulations require that buildings occupy no more than 75% of the lot area above 65 feet, up to 300 feet, throughout the districts, there are no further restrictions on building height, width or spacing under these individual zoning classifications.

The Logan Square Neighborhood - Parkway is covered by the Special Height Control District, passed in early 2007, which imposes a building height limit of 125', on much of the neighborhood west of 18th Street. However, the bill was passed as a height moratorium measure in response to the Barnes Tower/Parkway 22 controversy, with the anticipation that the height control issue would be more fully addressed as part of the Logan Square Neighborhood - Parkway Plan.

The 125' height restriction, absent bulk controls, could potentially create greater adverse neighborhood impacts than no height limit on some blocks, as the developer would need to build substantially a larger, bulkier building to achieve the same FAR, potentially

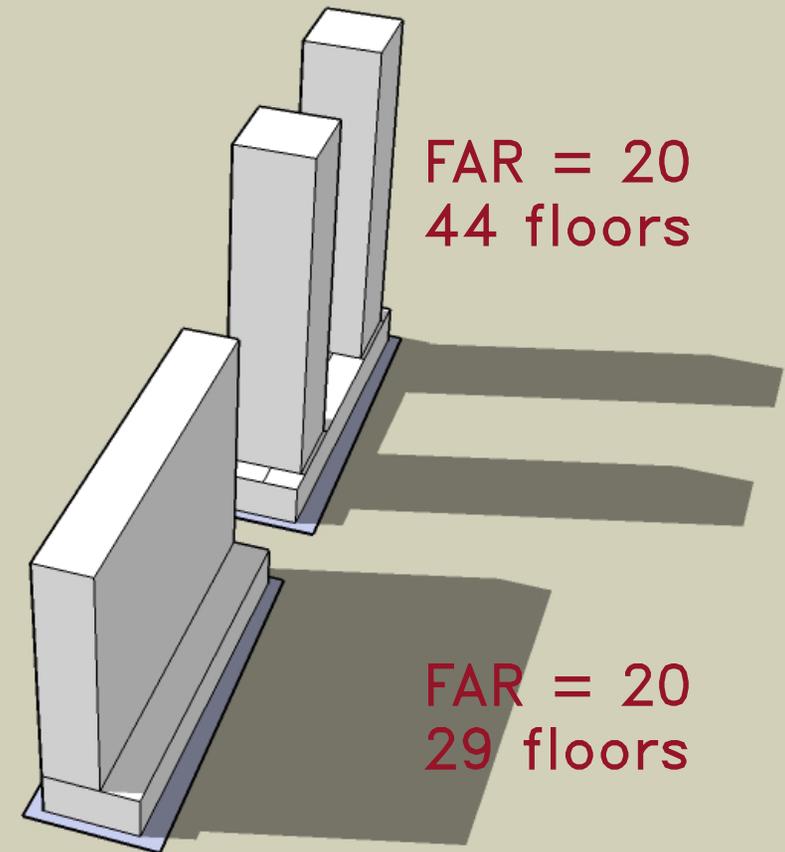
resulting in a 125-foot, 12-story street wall building that would block views, and limit daylight and sunlight access to floors in adjacent buildings below 12-stories. In contrast, a small-footprint higher tower, coupled with a low-rise 4- to 6-story podium building would have some impact on views, but permit daylight and sun penetration, as well as views between buildings.

Many blocks in the Logan Square Neighborhood - Parkway are zoned C4 or RC4, and have a base FAR of 5. An FAR of 5 can be achieved with a variety of building forms, each with distinct advantages and disadvantages. Theoretically a 5-story building that covers the entire site could achieve an FAR of 5, but such a building would not be feasible for most uses, except perhaps for a parking garage, because of the need to provide light, and air and open space for residential uses. Examples of 6-story developments that achieve an FAR of 5 include the medieval section of Barcelona, or the tenements of the Lower East Side of New York, where daylight and air to the interior of the block is provided by small interior courts and light wells. Contemporary building forms, such as double loaded corridor apartment developments can achieve an FAR of 5 with a street wall building of a

minimum 7-8 stories, while providing some open space. Alternatively an FAR of 5 can be achieved with a mix of 3-4-story single-family townhouses, coupled with widely spaced small footprint high-rise buildings, up to 30 stories.

Advantages and disadvantages of alternative development configurations to achieve an FAR of 5 are listed in **Table 4**. An 8-story street wall development will protect views from most existing high-rise developments (above 8 floors). In contrast, a mix of 3-4 story townhouses, with an occasional small footprint high-rise building can provide a wider variety of unit types, provide a development more conducive to attracting families, and result in a higher value development. While a 7-8 story street wall development can work well on wider streets, such as Arch Street, or in cities such as Washington, DC, with wide streets, a 7-8 story street wall development on both sides of the street can create a dark streetscape on Philadelphia narrow streets. While on some blocks a low-rise townhouse podium and high-rise configuration may be appropriate, on other sites a mid-rise “street wall” configuration may be appropriate.

Image 84. Kennedy House. 



Alternate configurations for
an FAR 20 developed on the
Kennedy House site

In many other cities that place a high value on the quality of the private and public built environment, there is careful control of building height and bulk on a block-by-block basis to protect daylight access and views. For instance, the San Francisco Zoning Code includes two separate zoning maps, one covering zoning use districts, and one for building height/bulk controls districts. Within any one zoning use district, bulk controls vary on a block-by-block basis, depending on the specific physical and contextual character of the block.

In San Francisco, under the height and bulk limitations, each block is assigned a numeric and letter designation. The numeric designation refers to the permissible building height in feet, while the letter designation relates to a table that provides a maximum building length and diagonal building plan dimensions that apply above a certain height. Some districts also provide for maximum spacing requirements between buildings above building podium height..

Similarly, in Seattle, the use district classification text in the zoning code does not include either building bulk or FAR restrictions. The building bulk

restrictions are instead indicated on the zoning map, where for instance, an area designated for downtown residential mixed-use is further subdivided into various height districts based on building height restrictions, designed to protect views. Permitted FAR varies within each zoning use district, depending on the specific height sub-district.

Key development opportunity sites in the Logan Square Neighborhood - Parkway were tested under various redevelopment scenarios, to develop appropriate guidelines for building height, massing, spacing and building location. Various alternative scenarios were analyzed for the same FAR, based on retaining the existing 125' height limit, and eliminating the 125' height limit, but imposing other building height/bulk and spacing provisions.

The parcels used to test these options included:

- 1900 block of Arch, through to Cuthbert
- 23rd and Arch parcels through to the Schuylkill River
- 2000 block of JFK Boulevard/north side/ SEPTA air rights

Table 4: Development Options for Achieving an FAR of 5, and Evaluation of Each Option

BLOCK CONFIGURATION	EVALUATION CRITERIA											
	Potential for Private Open Space	Potential for Street Setback Landscaping	Potential for Common/Public Open Space	Suitability for Families with Children	Value of Units	Street Life	Daylight/Sunlight Access to Units	Views from Units	Units Per Acre	Daylight/Sunlight at Street	Views from Lower Levels of Existing High-Rise	Views from Upper Levels of Existing High-Rise
1. 6-story full block buildings with light wells	Poor	Poor	Poor	Poor	Poor	Excellent	Poor	Poor	180+	Fair	Poor (below 6th floor)	Excellent (above 6th floor)
2. 8- to 12-story full block buildings with central court (existing height controls)	Poor	Fair	Excellent	Poor	Fair	Excellent	Fair/Good	Fair/Good	180+	Poor	Poor (below 12th floor)	Excellent (above 12th floor)
3. Low-rise townhouse buildings (4-story) with one small footprint high-rise tower	Excellent	Excellent	Excellent	Excellent	Excellent	Fair	Excellent	Good (excellent for high-rise)	130+	Good	Good	Good
4. Low-rise townhouse buildings (4) with 2 medium height larger footprint towers	Excellent	Excellent	Excellent	Good	Good	Fair	Excellent	Good (excellent for high-rise)	130+	Fair	Fair	Fair

▲ Table 4. Development Options for Achieving FAR5

- 2000 block of Hamilton/Pennsylvania Avenue (Whole Foods site)
- 1800 block of Hamilton Street, facing Franklin Town Park

Possible building height, bulk and spacing restrictions were developed based on the outcome of these studies.

The 3-dimensional models illustrate the impact of potential developments using the same FAR, with and without the 125' height restriction.

A possible building height and bulk district map for the Logan Square-Parkway area, as an overlay to the revised zoning use district map, is illustrated in **Map 25**. The height and bulk district map would include the following controls:

- Maximum threshold building height
- Heights above which maximum building footprint diagonal and spacing dimensions apply

- Maximum building footprint and diagonal dimension for each height limit
- Minimum spacing between buildings above heights at which maximum building dimensions apply

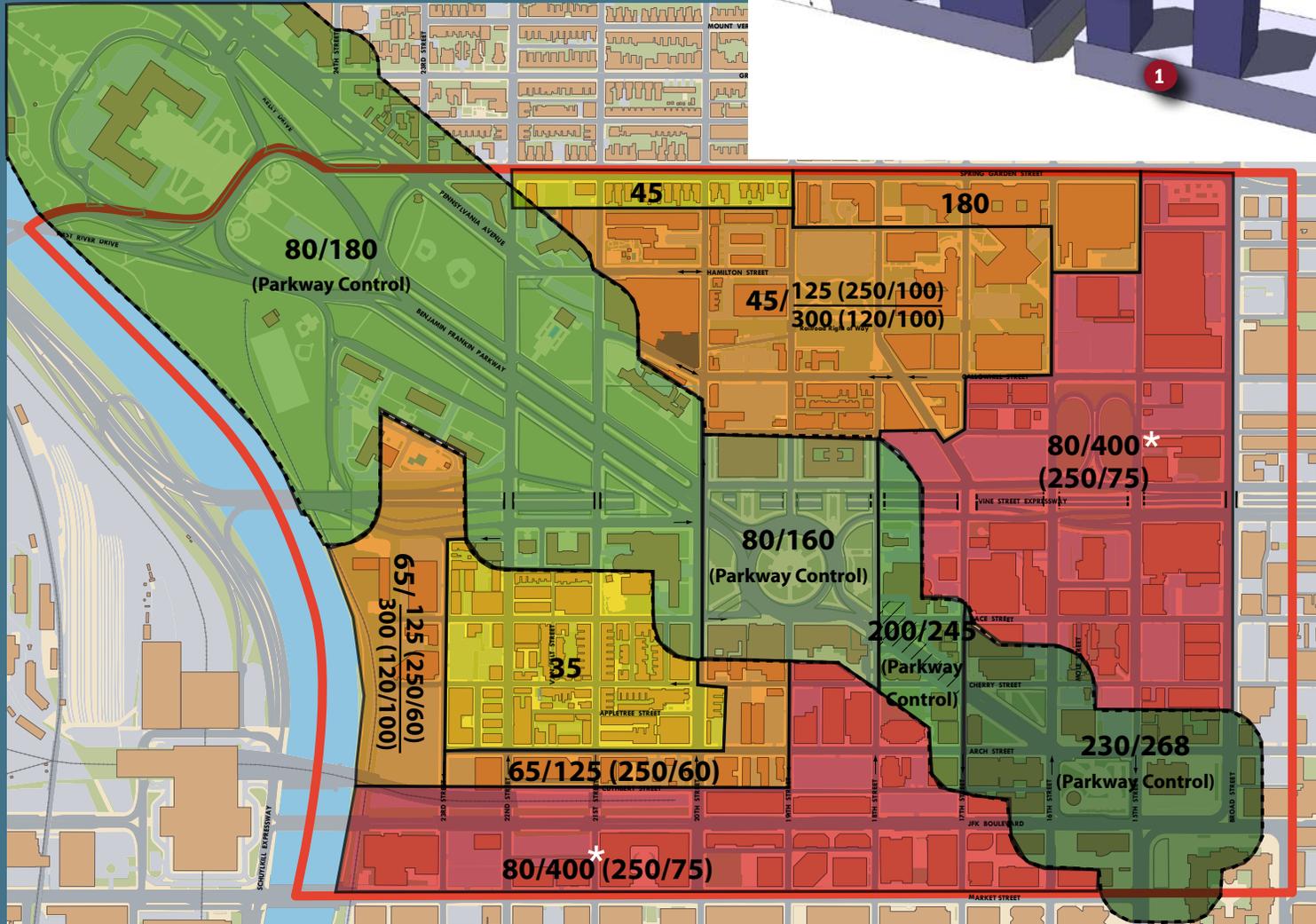
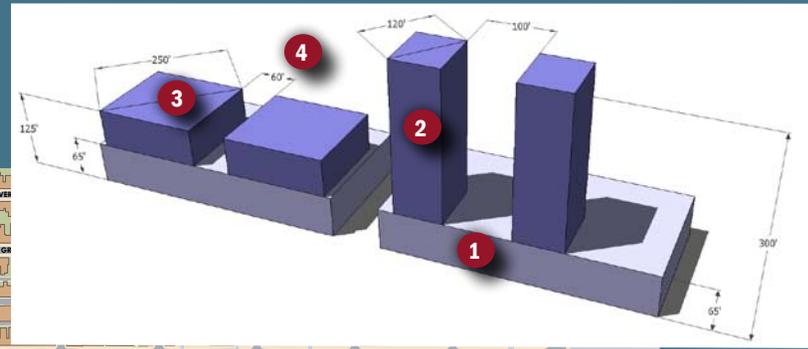
The details of the height and bulk controls would need to be developed in conjunction with the Planning Commission staff. Potential building height and bulk restrictions assume elimination of the existing 125' height limit. The conceptual height and bulk district map recognizes that the appropriate building footprint dimensions for an office building in the C4 and C5 zones are different than those for a residential building in the RC4 zone. Different bulk controls are appropriate for the RC4 zones abutting the historic residential core of Logan Square (where zoning providing a transition between the high-density office core, and low-rise residential neighborhoods), and the Parkway North area, where there is a mix of low-rise townhouses and high-rise buildings. Different height and bulk controls are appropriate for sites facing a wide street compared to sites facing a narrow street.

Map 25. Proposed Building Height & Bulk District 

PROPOSED BUILDING HEIGHT & BULK DISTRICT MAP

- 1** MAX HEIGHT OF PODIUM
- 2** MAX BUILDING HEIGHT LIMIT
- 3** MAX DIAGONAL DIMENSION OF TOWER
- 4** MIN SPACE BETWEEN BUILDINGS ABOVE PODIUM

65/125 (250/60)



* THRESHOLD HEIGHT LIMIT. GREATER HEIGHT MAY BE ALLOWED SUBJECT TO PLANNING COMMISSION REVIEW

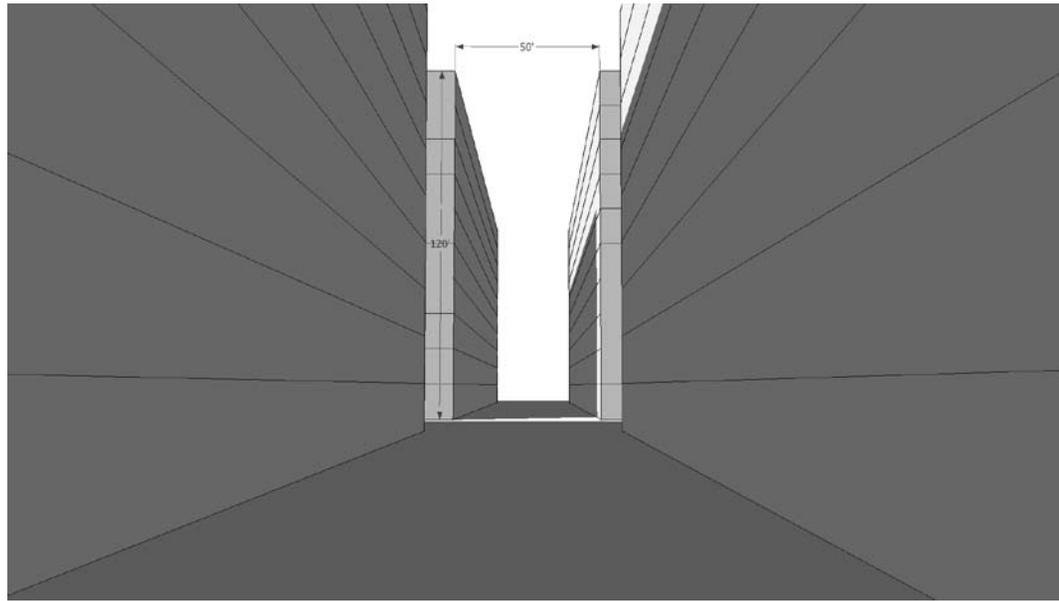


For high-rise commercial buildings in the C4 or C5 zone (above the 80' high podium), maximum building dimensions of 250', with a minimum 75-80' between buildings are appropriate. A maximum 250' building dimension would permit a typical office building footprint of 25,000 sq.ft., while a minimum 75-80 foot spacing would assure adequate daylight penetration between buildings and long distance views.

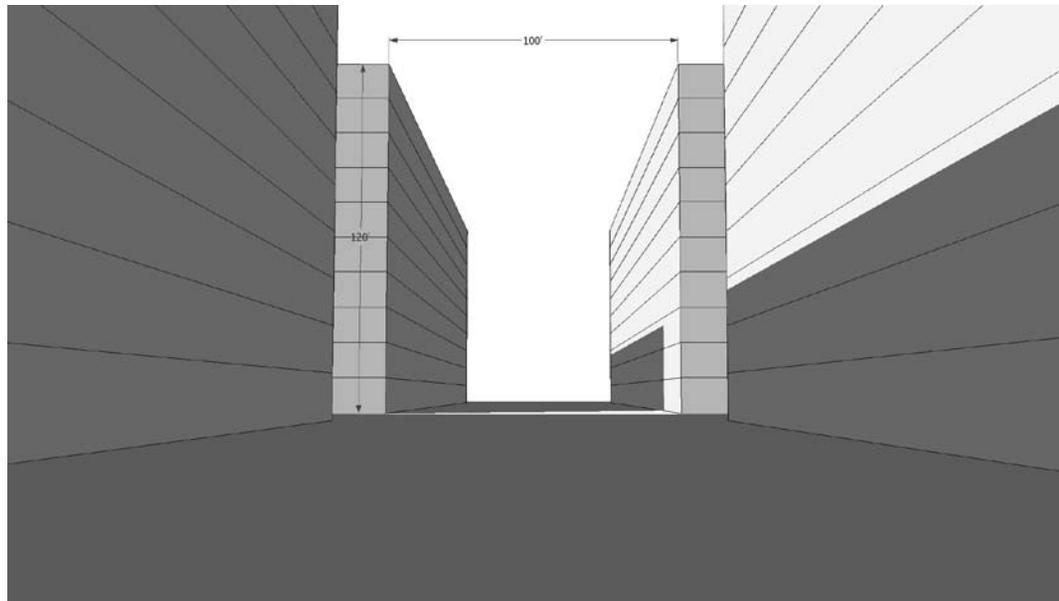
Maximum building diagonal dimensions and spacing in the RC4 zones are proposed to help to assure daylight and sunlight penetration between buildings, and maintenance of views. For high-rise residential buildings in the RC4 residential/commercial zone, maximum building dimensions of 100', with a minimum spacing of 100' between buildings are appropriate. A 100' building dimension would permit a small footprint 10,000 sq.ft. apartment tower with 4-8 units per floor, while a minimum 100 foot spacing would assure adequate daylight and sunlight penetration between buildings, provide privacy between residential units, and protect views (*Figures 15a and 15b*). For the RC4 Parkway North area, the height of buildings for which there

are no proposed bulk controls could be limited to 50 feet (4 stories), to encourage townhouse-style development. The appropriate maximum diagonal dimension of a building between 50' and 125' in height could be 200' (consistent with the Tivoli), with a minimum building spacing of 100'. For buildings between 125' and 300' in height, the appropriate maximum diagonal dimension could be 100' with a minimum building spacing of 100'. Such controls would prohibit the block-long slab buildings such as Kennedy House and the Sterling, which have had such an adverse effect on the character and quality of the environment of JFK Boulevard, or the City View Condominiums, which create a block-long visual barrier. They would encourage small footprint, fine-grained residential towers such as those in Vancouver (typically 85'x85', 7,300 sq.ft. footprint) or the classic Society Hill Towers (109x73, 8,000 sq.ft. footprint).

In the short-term, these requirements should be refined and included in the overlay district for the Logan Square Neighborhood - Parkway.



◀ **Figure 15a. 10 story building on 50' ROW limits daylight penetration.**



◀ **Figure 15b. 10 story building on 100' ROW permits good daylight penetration.**

3. *Inappropriate Area Bonuses for Setbacks in R15 Zone*

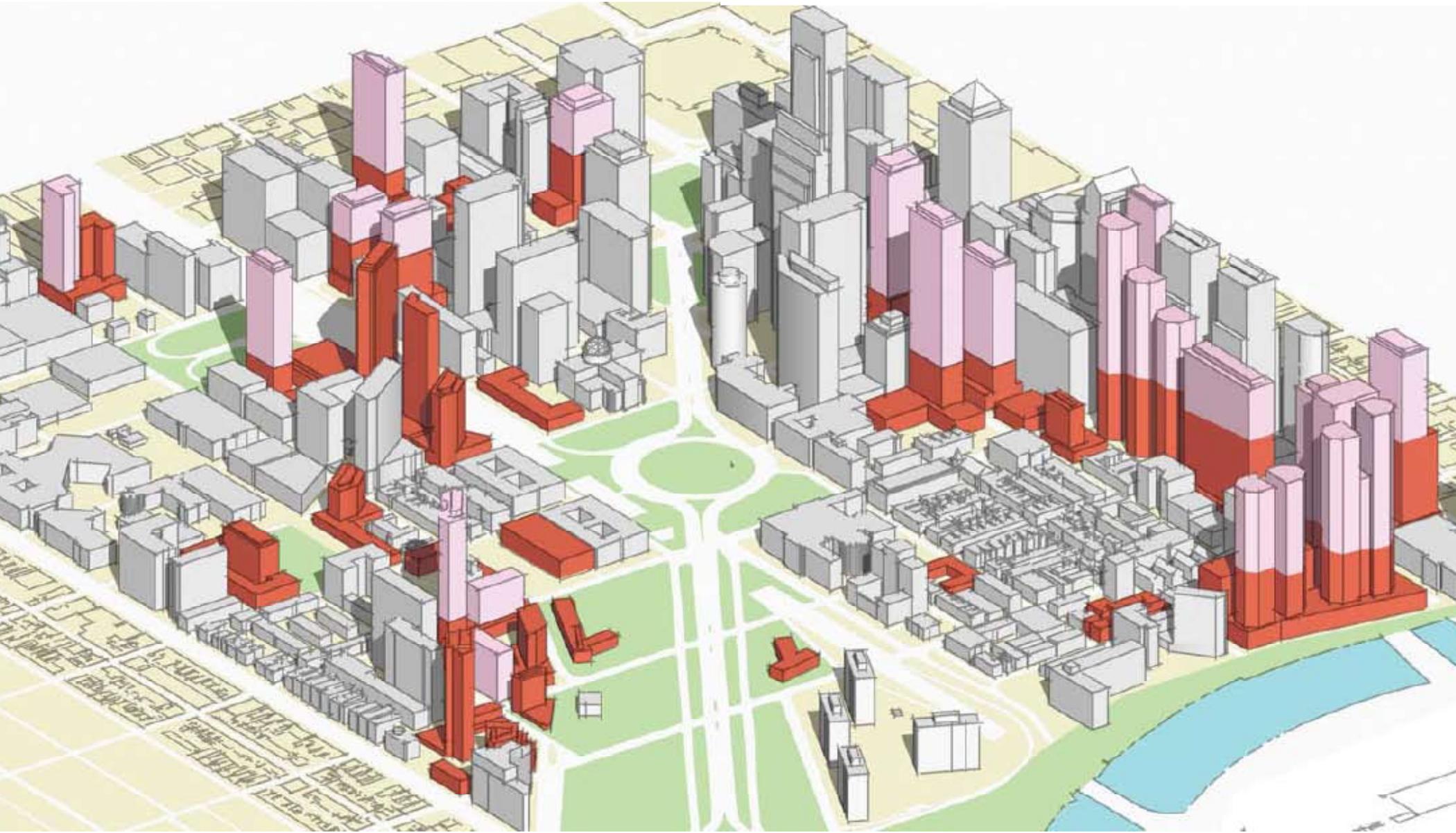
Typically, buildings should be built at or close to the street line (back of sidewalk) to maintain an attractive, lively, urban streetscape, or setback no more than 10' to create a widened sidewalk area for sidewalk cafes, stoops, small landscaped boxes, etc. However, zoning controls in R15 provide FAR bonuses of two sq. ft. for each square foot of setback for buildings setback a minimum 10' from the street line. The result is to encourage high-rise high-density developments, setback from the street, without any connection to the city, such as Park Towne Place. For R16 districts, Section 14-1303 of the Zoning Code, Additional Floor Area, permits an additional 15 sq.ft. of floor area for each square foot between the street line and building line. While these incentives may be appropriate for other areas of the city, both of these incentives encourage undesirable development forms in Logan Square. In the zoning overlay, the applicability of provisions of Section 14-208 relating to additional floor area in R15 for buildings setback from the street line 14-208(3)(a) and (b) should be

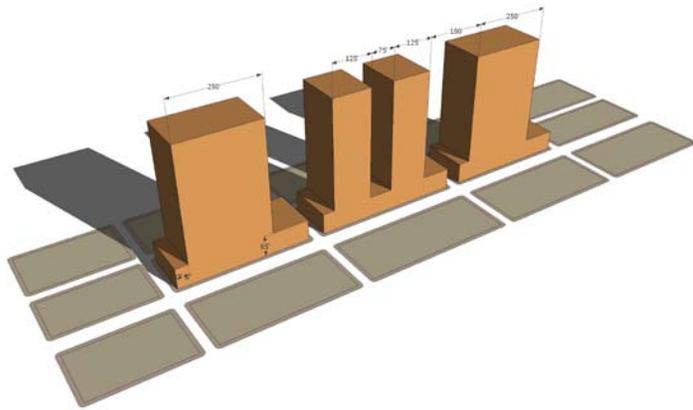
deleted, as well as provisions of Section 14-1303 relating to R16 buildings constructed back from street lines. Incentives for modest setbacks of up to maximum of 10' should be included in the bonus provisions for public open space.

Figure 16 shows potential development using the FAR bonuses currently available in the study area.

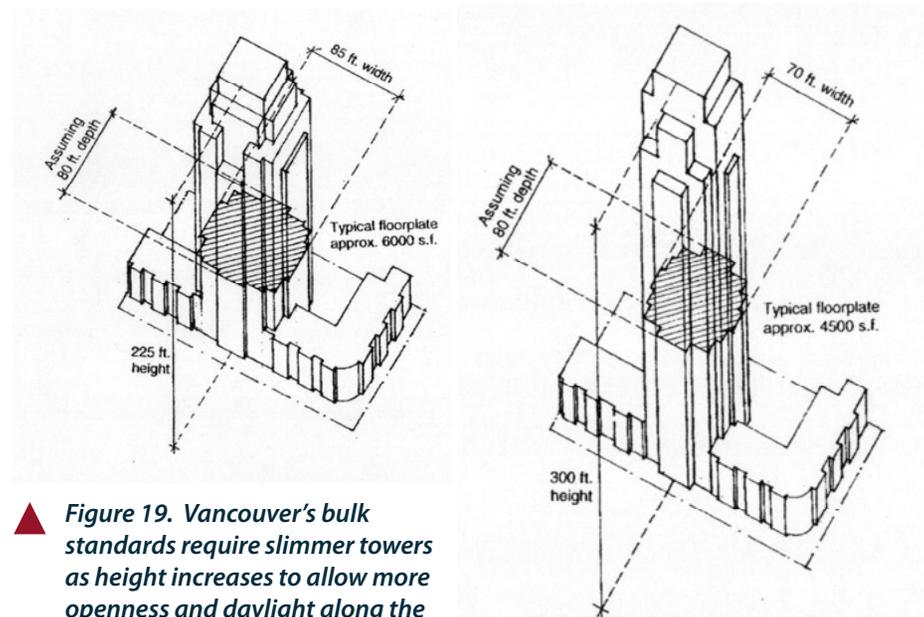
Figure 16. Potential Development based on allowable base FAR & FAR bonuses 

- Allowable base FAR
- FAR bonuses available

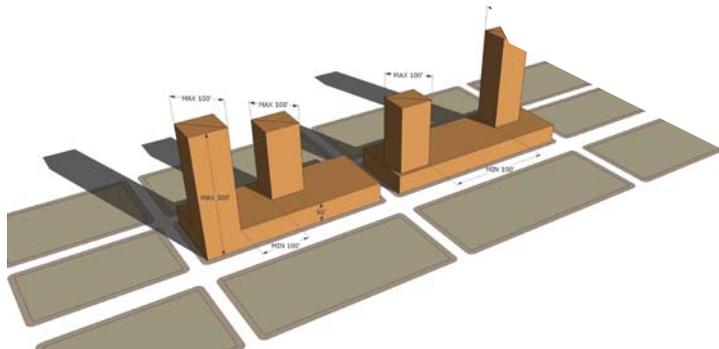




▲ **Figure 17. Existing Bulk Controls: south side of JFK Boulevard should be extended to north side.**



▲ **Figure 19. Vancouver's bulk standards require slimmer towers as height increases to allow more openness and daylight along the street.**



▲ **Figure 18. Possible Building Bulk Controls, RC 4 zones in Parkway North area.**

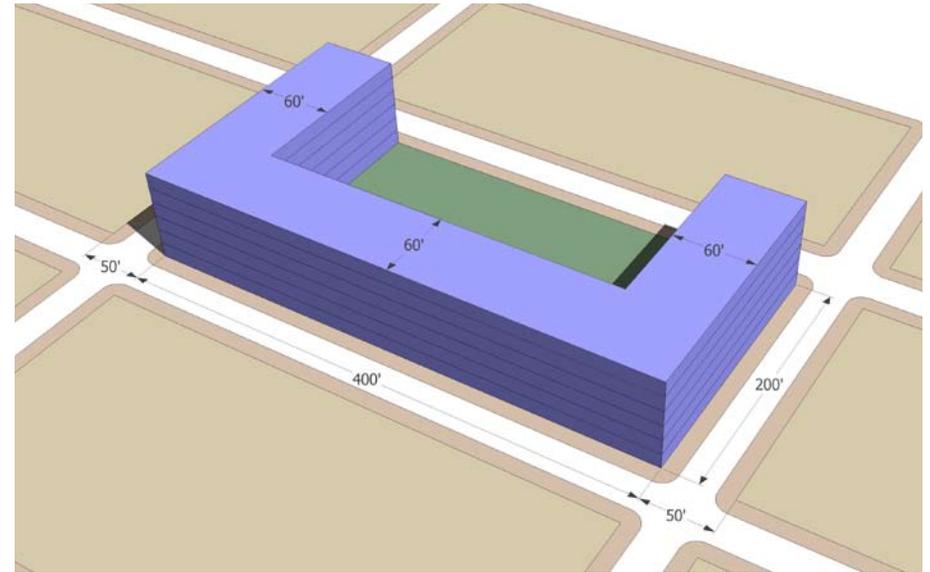


▲ **Image 85. Massing of building should maximize daylight access and views.**

Inappropriate bonuses for building setbacks in R15 zones.

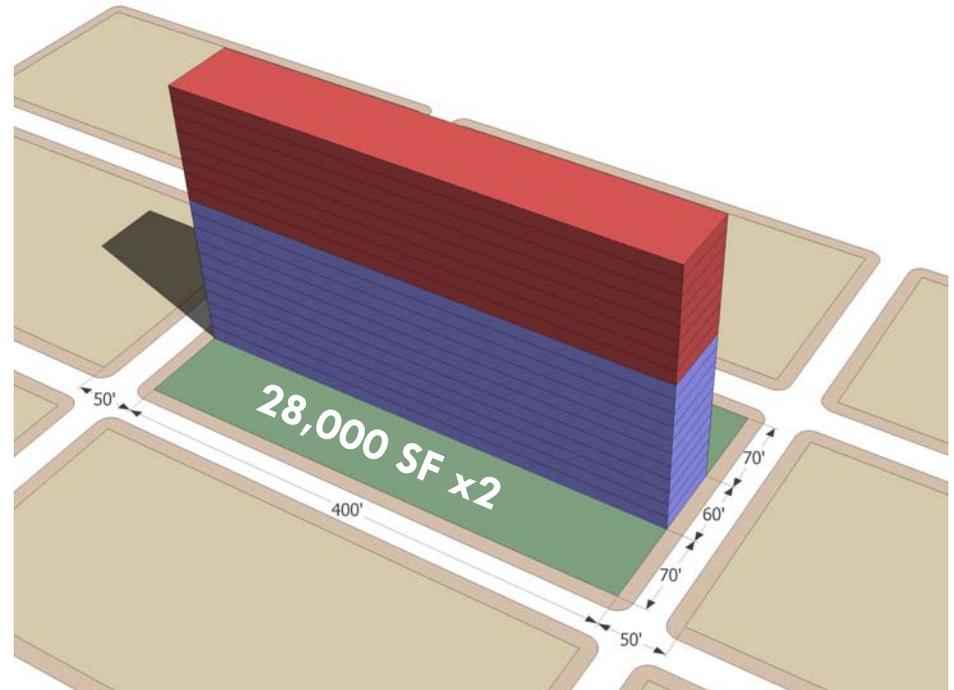
PERIMETER BUILDING

Base FAR = 3.5
 Lot Area = 80,000 SF
 Buildable SF = **280,000 SF**
 Building Footprint = 40,800 SF
 Number of Floors = 6



SLAB BUILDING

Base FAR = 3.5
 Bonus FAR = 4 SF for every square foot between street line and the building
 Lot Area = 80,000 SF
 Buildable SF = **456,000 SF**
 Building Footprint = 24,000 SF
 Number of Floors = 19*
 * Height limited to 2 times the distance from windowed wall to center line of street



R15 zoning currently permits much higher density for a pedestrian unfriendly high rise building setback from the street

4. Accessory Parking Garages

To eliminate the adverse visual impacts of exposed parking podiums, above-ground parking garages as an accessory use should be prohibited within all zoning classifications in the Logan Square Neighborhood - Parkway, except under all the following conditions:

- The garage is completely wrapped in active uses permitted under the zoning classification for a minimum depth of 25' around the perimeter of the garage, for all facades that face or are visible from a public street, except alleys.
- Parking access should be limited to two openings, maximum 11' wide each.

▼ *Image 86. In appropriate above-ground parking garage at Murano condominium development*



To provide a further incentive for the provision of underground parking, an additional floor area of 350 sq.ft. per space should be provided for each underground parking space (as long as no above-ground parking is provided) in all medium- and high-density zoning classifications (C4/C5, RC4, R15, and R16). This additional floor area would only apply to spaces that do not exceed the minimum parking requirements. Total additional floor area could not exceed the new maximum additional floor area defined in on page 175.

5. Loading

To eliminate the adverse streetscape impacts of loading bays, access openings to loading bays should be limited to one opening, maximum 15 feet wide. Where more than one loadings spaces is required by

▼ *Image 87. Comcast loading dock with excess width*



code, such spaces and access and circulation should be provided within the building.

6. *Non-Accessory Garages*

Under RC4 and C4/C5 zoning, parking garages — both public and private — are permitted as a main use, with a special use permit. Retail commercial uses are required where a garage abuts a public sidewalk. Because of the adverse impacts of all above-ground parking garages on the urban streetscape, all above-ground parking garages, as a principal use, should be prohibited within any zoning classification in the study area, west of 16th Street, and in RC4 zoned parcels throughout the neighborhood.

▼ *Image 88. Unsightly exposed parking garage podium without active uses.*



7. *Open Space Requirements*

The Logan Square Neighborhood - Parkway includes significant open space resources, such as the Parkway, Logan Square, and Franklin Town Park, as well as access to the Schuylkill River Trail. However, much of this space is not well suited to small-scale, family-oriented activity. Many municipalities, such as San Francisco and Chicago, have incorporated requirements for a minimum provision of open space per dwelling unit. This open space may be in the form of private usable open space, such as balconies and private roof terraces, common usable open space, such as roof terraces over parking, or grade level enclosed on open public space, or publicly accessible open space that is directly accessible to residential developments (*Image 89*).

▼ *Image 89. Active uses wrapping the perimeter of a garage.*



Zoning controls for the San Francisco downtown residential district of Rincon Hill require a minimum 75 square feet of open space per dwelling unit (private, common and public). A minimum of 40% of the open area is required to be common to all units in a development, and a minimum of 36 square feet of open space per dwelling unit must be provided on site. Chicago requires a minimum 36 square feet of open space per unit.

All high-density district zoning classifications (C4, C5, RC4, R14, R15, R16) should include a provision that accessible open space be provided at a minimum of 40 square feet per unit for all residential developments. This open space may be in the form of “green” accessible roofs, roof terraces, balconies, and yards, etc. Minimum requirements should be established for planting and landscaping of common and public access areas.

Controls should require a sufficient depth of soil above underground or above-ground, wrapped structures to provide the opportunity for landscaped open space where parking is not covered by development.

In the short term, these zoning requirements should be adopted as part of a zoning overlay for the Logan Square Neighborhood - Parkway

5.3.5 Design Review for Major Developments

The current Philadelphia Zoning Code does not provide for design review, or community input, for major developments that could have a significant impact on the community. Currently, because of Philadelphia’s antiquated zoning code, most major projects require a zoning variance, and a zoning hearing, and thus an opportunity for the community to comment on, and have input into, the design of a development proposal. The goal of zoning reform in Philadelphia is to significantly reduce the need for zoning variances. However carefully a zoning code is drafted, it is necessarily generalized and cannot address the specific nuances of every site. It is likely that the new Philadelphia Zoning Code, currently under development, will incorporate provisions for design review.

Many cities such as Seattle, San Francisco and Boston have design review procedures in place, which provide the opportunity for community input into the design of a project. Seattle has one of the most sophisticated of these design review process. In all downtown mixed-use zones, development of over 20 residential units, or 20,000 square feet of non-residential use, are subject to mandatory design review, against established specific design guidelines

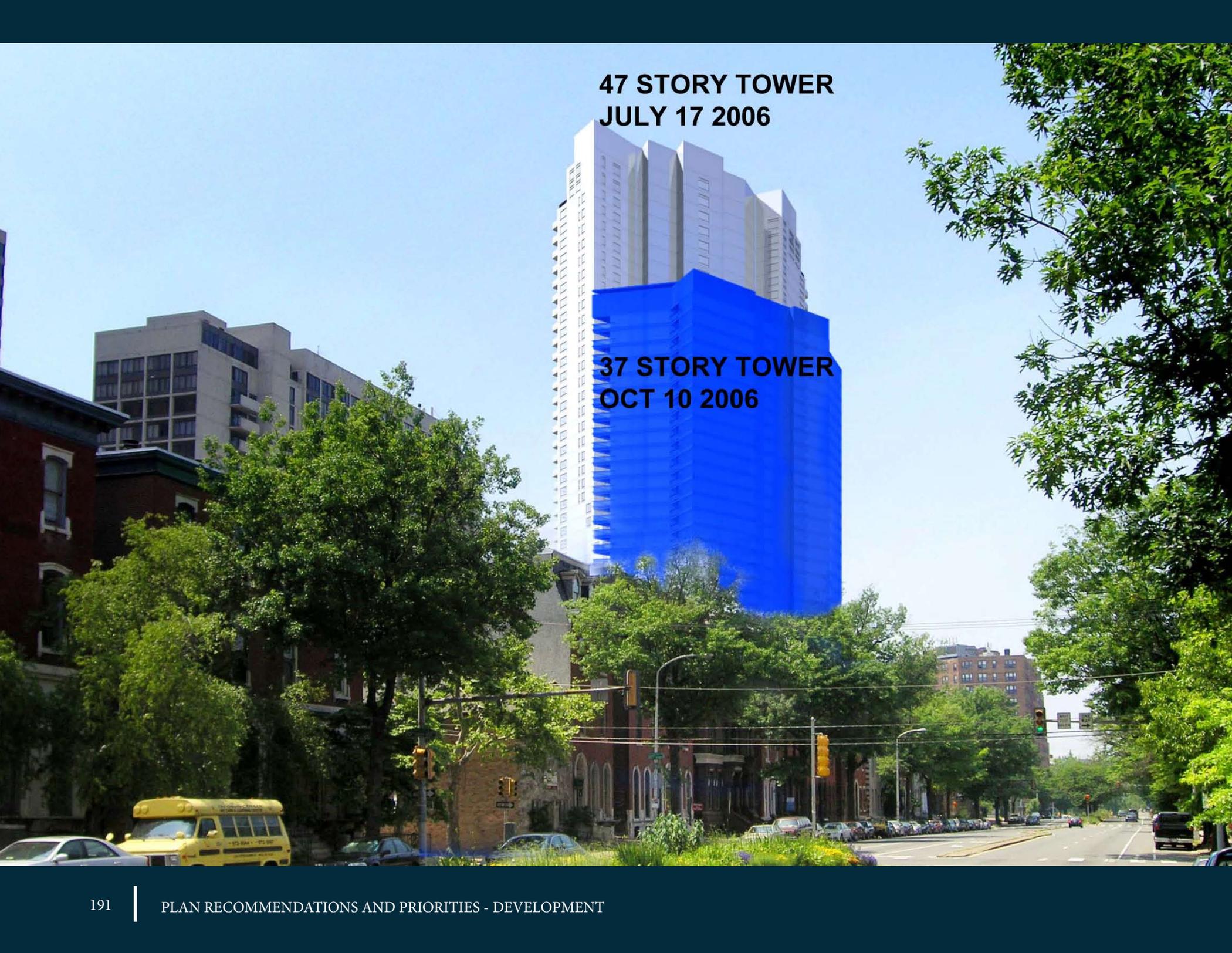
Image 90. Vancouver’s balconies and roof terraces as open space.





**47 STORY TOWER
JULY 17 2006**

**37 STORY TOWER
OCT 10 2006**



to provide predictability and consistency. The design guidelines address site planning, building massing, architectural elements and materials, public amenities, vehicle access and parking, and streetscape and landscaping. The goal of the design review procedure is to encourage better design that enhances the character of the city, ensure development sensitivity fits in the neighborhood, and provide some flexibility in application of development standards. In Philadelphia, the final requirements for a city-wide design review will be established by the City, but likely include a public hearing process that will include an opportunity for community input.

A design review procedure should be established for all major developments in the Logan Square Neighborhood - Parkway that include over 20 residential units, or 20,000 square feet of non-residential space. The applicant should be required to submit an analysis of the sunlight/shadow, daylight, and impacts on views, of the proposed development in addition to standard presentation materials. The design review procedure can provide an opportunity to grant departures from certain land use/zoning code standards, such as structure width and depth,

setbacks, parking, open space, and lot coverage, depending on the specific circumstances of the site, and community benefit of the proposed development.

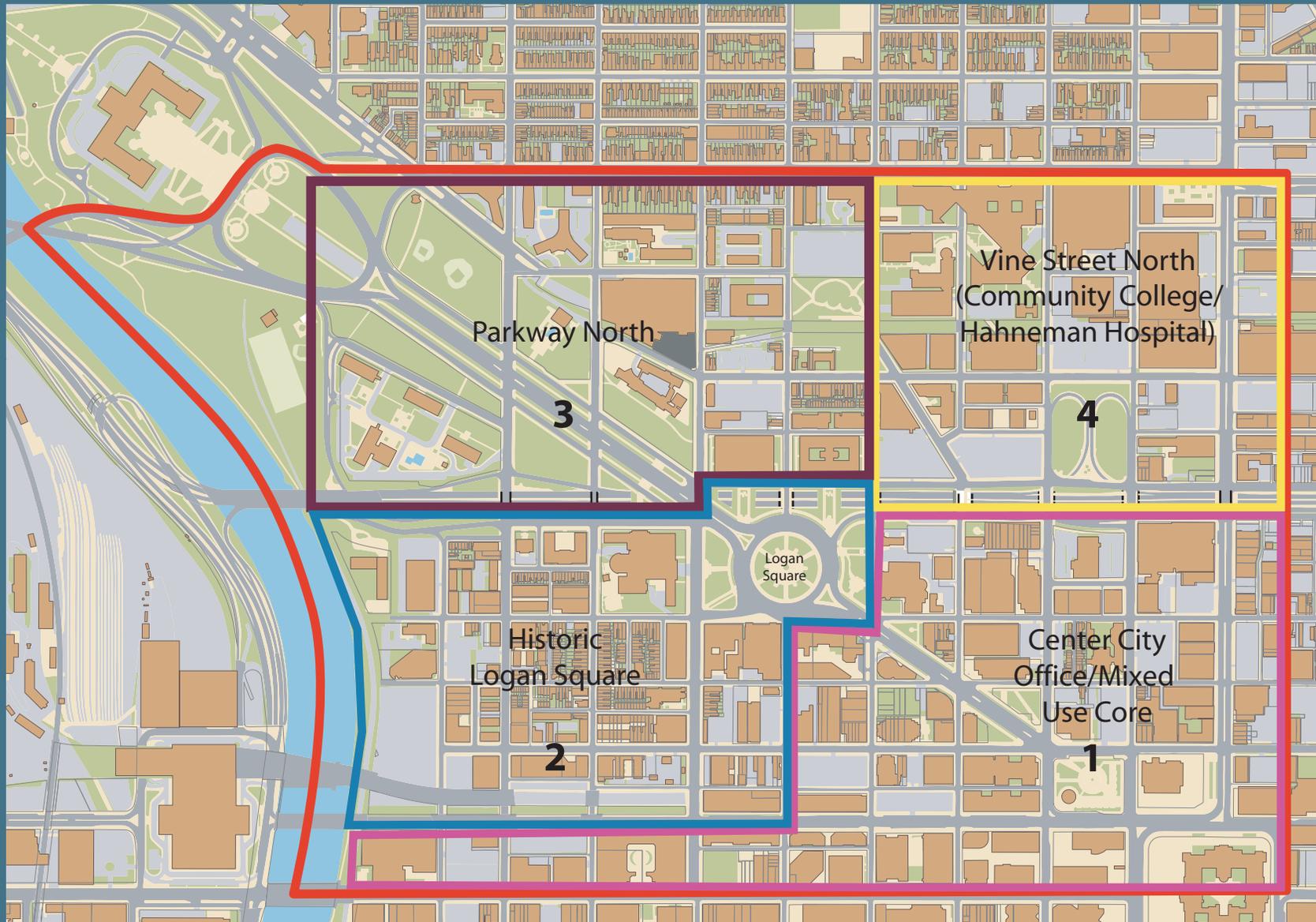
To prepare for community testimony at design review hearings, LSNA should evaluate each property against the design guidelines standards in Section 5.3.6. Separate guidelines have been established for each character quadrant of the neighborhood, as follows:

1. Center City Office Core
2. Historic Logan Square
3. Parkway North
4. Vine Street North Community College/Hahnemann Hospital

The character quadrants are shown on Map 26.

◀ ***Image 91. Reduction of building height as result of community meetings of proposed Parkway 22 Tower***

CHARACTER QUADRANTS



5.3.6 Character Quadrant Design Guidelines

Zone 1 – Center City Office/Mixed Use Core

Zone 1 includes much of Philadelphia’s Center City Office/Mixed Use core, centered around Suburban Station and West Market Street. It includes parcels south of the SEPTA tracks west of 20th Street, and parcels east of 19th Street and south of Vine. It also includes the air rights parcels over the SEPTA tracks, although the air rights parcels west of 21st Street present significant constraints to development. Nineteenth Street is proposed as a line of demarcation between the Center City Office/Mixed Use Core and Logan Square residential neighborhood, creating a buffer of the 1900 block to transition from the large-scale office building to the low-scale historic core. However, 20th Street had also been suggested as the boundary since it is a wider street, and the edge of the square, but residents overwhelmingly preferred 19th Street as the boundary line. Over the years, the center of gravity of the office/mixed use core has moved northwest from Market Street. The

area is predominantly commercial, but with some new high-rise residential development along Market Street, such as the Murano. Many of the buildings are large footprint high-rise office buildings. The area has a high density of pedestrian traffic, mostly office workers, particularly around weekday lunch time. The zone includes some of the highest density high-rise buildings in the city. There is limited open space. Design guidelines are targeted to reinforcing the positive characteristics of the district, while encouraging more 24-hour activity. Major development opportunity sites include parcels on Market and Arch Streets, and the Archdiocese parcels adjacent the Cathedral between 17th and 18th Streets, fronting Logan Square.

A. Building Design Guidelines

- Tall buildings may be appropriate in this zone and should be designed to enhance the skyline, and contribute to the creating of a unique identity. Upper portions of tall buildings should promote visual interest.
- Locations of towers, tower configuration,

◀ *Map 26. Character Quadrants*

and spacing of towers should be adequate to preserve sunlight and daylight penetration. For the C5 zones south of Kennedy Boulevard and east of 19th Street bulk controls should be similar to those already in place for the south sides of Kennedy Boulevard and Market Street. A maximum podium height of 80-100 feet would be appropriate. Maximum tower dimensions should be adequate to permit a typical office building footprint of 25,000 sq.ft., with spacing between towers sufficient to assure daylight penetrations, and allow for views. A maximum building diagonal dimension of 250 feet, and minimum spacing between buildings of 75 feet would be appropriate. A threshold maximum height limit of 400-450' (30-40 floors) is proposed, with the approval for higher buildings at appropriate locations subject to appropriate public review. For air rights parcels over the SEPTA tracks, threshold maximum height limits should not exceed 300', to help create a transition to lower rise development to the north of Arch Street, and reflect the difference in height between JFK Boulevard and the SEPTA tracks, and the cross streets below.

- The base of tower buildings should be designed at the scale of the pedestrian, with materials and detailing that enhance the pedestrian experience.
- Mixed-use buildings and developments are encouraged.
- New structures should provide a transition in form and scale of adjacent existing buildings which are likely to remain.
- New structures on parcels abutting Zone 2 should carefully address the transition in scale and land use.

Image 92. Comcast Center 



B. Street Level Amenities

- Because of the high level of daytime pedestrian traffic, sidewalks should be sufficiently wide to provide the opportunity for sidewalk cafes, seating areas, etc. Buildings on north-south streets with only a 12' sidewalk right-of-way should be setback from the property line a minimum of 5' to create a sufficiently wide sidewalk for sidewalk cafes and other amenities. Buildings should also be setback on other blocks with a narrow sidewalk right-of-way, such as the north side of JFK Boulevard between 20th and 23rd Streets. Here, a 20' sidewalk should be provided, consistent with the overall width of JFK Boulevard. A consistent build-to-line should be provided to create a consistent "street wall" at podium level.
- Provide usable public-access open spaces with public amenities such as retail uses or cafes at appropriate locations.
- Avoid large expanses of blank or opaque facades at all levels. Maintain a minimum 75% transparency at first floor level, and a minimum 30% transparency at upper levels. Articulate blank areas of wall with building materials, art work, landscaping, etc.
- Visually reinforce building entrances.
- Minimize impact of loading docks and parking access. Limit parking access to two (2) openings maximum 11' wide, and one 15' loading access.
- Locate loading and turn around underground with access from service streets or alleys, where feasible, to reduce street level impacts.
- Provide pedestrian amenities, such as public restrooms, seating and building ledges that encourage pedestrian interaction.
- The design of buildings fronting JFK Boulevard should actively address the enhancement of the 21st, 22nd, and 23rd Streets underpasses at JFK Boulevard and the SEPTA tracks, including establishing integral public access connections between JFK Boulevard and the cross streets below, and the street frontage at the cross street level.
- Incorporate street-level pedestrian spaces such as plazas or parcels parks with building setbacks, where appropriate, to support cafes, play areas, etc.

Zone 2 – Historic Logan Square

The Historic Logan Square Zone includes Logan Square itself, the Franklin Institute, Moore College of Art and Academy of Natural Sciences, and the historic rowhouse neighborhood between Race and Arch, 20th and 22nd Streets. It also includes the transition areas of the 1900-2200 blocks of Arch Street, and the 23rd Street corridor. Residents are particularly concerned about the potential westward expansion of the Center City office core uses and the potential for high, bulky buildings along Arch Street, west of 19th Street, where they could have adverse impacts on residential townhouse blocks, as well as on views north from existing high-rise residential buildings, such as Kennedy House. Pedestrian activity is typically by local residents and Parkway museum visitors. Active open space is largely utilized by residents, except the Schuylkill River Trail.

A. Building Design Guidelines

- New buildings within or adjacent to the historic rowhouse core should respond to the physical

context of the adjacent site, in terms of building massing, scale, fenestration, and materials.

- Large-footprint commercial office-core-type high-rise buildings should be prohibited in this zone.
- Development outside the historic core should consist of a mix of townhouses, mid-rise apartments with first floor commercial, and strategically located high-rise small-footprint residential towers and semi-public open space. Overall development density should be in the order of 100-150 units per acre.
- Density within the historic core should not exceed the existing R10 zoning density of approximately 60 units per acre.
- Developments on sites adjacent to Zone 1 should create a transition in bulk of scale,

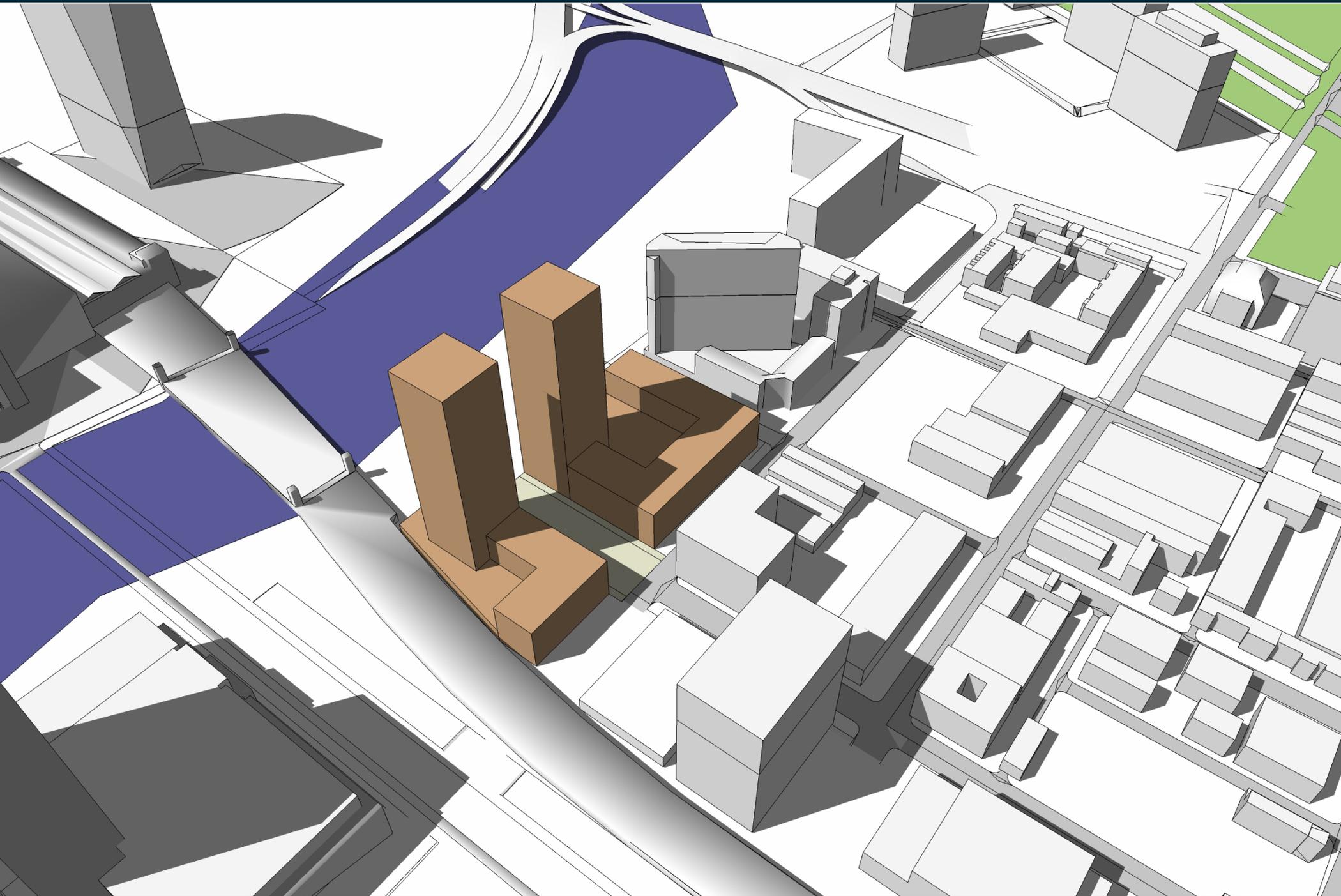
between the high-rise core and townhouse neighborhood, by incorporating elements of each.

- Additional historic resources within the area should be identified, listed on the Philadelphia Register of Historic Places, and integrated into new development proposals.
- The residential townhouse scale of the neighborhood should be reinforced with design elements, such as individual unit entrances, bays, patios, stoops, and balconies that respond to the 16-20' townhouse module, to create a pedestrian-friendly streetscape.
- Arch Street view corridor and potential future access corridor to the Schuylkill River, west of 23rd Street should be maintained in the development of 23rd Street frontage.

- The 23rd Street edge of development of 23rd Street parcels should respond to the townhouse residential scale of the historic townhouse core.
- Small footprint, wide spaced high-rise towers would be appropriate for waterfront sites west of 23rd Street, in conjunction with a low-rise residential/mixed-use podium, 65 to 70' in height. The first floor would be retail, or residential on a raised first floor, above floodplain level.
- Parcels rezoned to RC4 on the south side of Arch Street could appropriately be developed with mid-rise street wall buildings of 8 stories (100') that would not overwhelm this wide street, and would also minimize shadow impacts on low-rise residential blocks to the north, and maximize views. Residents of high-rise Kennedy House have requested that buildings on the south side of the 1900 block of Arch Street be limited to 5 stories to protect

Image 93. Mid-rise Development in Washington, DC that could be appropriate on Arch Street, west of 19th Street 





their uninterrupted views to the north. A possible education campus has been proposed for this block. However, the allowable FAR for this block will likely result in a higher building that would have some impact on views.

- For the transition RC4 area west of 23rd Street adjacent the historic core of Logan Square, no bulk controls are proposed for buildings up to 65 feet in height. For buildings between 65' and 125' in height, the proposed maximum diagonal dimension of a building is 250', with a minimum building spacing of 60'. For buildings between 125' and 300' in height, the proposed maximum diagonal dimension of a building is 120', with a minimum building spacing of 100', to limit high-rise development to small footprint towers.
- Within the historic townhouse core, materials, pattern of solids and voids in the wall plane, and building scale should be consistent with the historic fabric.

B. Street Level Amenities

- Development should create a stimulating pedestrian environment along public streets

through multiple building entrances, for both residential and commercial uses. Residential buildings, including apartment buildings, should be designed with first floor units with direct street access.

- Buildings should be enhanced through landscaping, including planted strips between stoops.
- Developments should include pedestrian-oriented streetscape elements, such as pedestrian-scaled streetlights, porch lights at individual entrances, pedestrian-scaled signage for retail/commercial uses.
- Large developments should incorporate street-level pedestrian spaces such as plazas, or mini parks within building setbacks, to support outdoor seating areas, etc.
- Minimize impact of loading docks and parking access. Limit parking access to two (2) openings maximum 11' wide, and one 15' loading access.

◀ *Figure 23. Possible Development Scenario for 2300 Arch Street with 300' small footprint residential towers above a 65' high podium.*

Zone 3 – Parkway North

Parkway North includes many high-rise residential apartment buildings, such as Park Towne Place, City View Condominiums and Museum Towers, as well as the historic rowhouses along Spring Garden Street. Residential densities of new development are typically in the order of 100 units per acre. Parkway North also includes the Benjamin Franklin Parkway and associated open spaces, as well as the Rodin Museum, the Free Library, Family Court Building, and future Barnes Museum. The area is predominantly residential except for Callowhill Street and the 2000 block of Hamilton Street. Many of the existing high-rise developments are setback far from the street line, and do not create a pedestrian-engaging environment. Residents of City View Condominiums are very concerned about protecting their uninterrupted views of the Center City skyline to the south, and have expressed preference for a mid-rise street wall building of 7-8 stories, as compared to a high-rise development on the abutting Whole Foods site.

The Children’s Crisis Center on the 1800 block of Callowhill Street should be relocated over the long term, to open up the entire north side of the 1800 block of Callowhill Street for mixed-use development with first floor neighborhood retail.

The area has a relatively light density of pedestrian traffic, except for Parkway events. Design guidelines are targeted to reinforce the positive characteristics of the district, including its openness, while encouraging development that is built to the street line, is more pedestrian-friendly, and encourages more active use of existing open space.

A. Building Design Guidelines

- Development can accommodate a variety of forms, within the zoning density of FAR 5, from 7-8 story street wall apartment/mixed-use buildings, to developments that include both 3-4 story townhouses, and a limited number of carefully sited mid- to high-rise small footprint apartment buildings, to mid-to-high-rise small footprint apartment buildings

over a residential/mixed-use podium around public and semi-public protected courtyards, located for sun access. Small footprint high-rise buildings in conjunction with family-oriented townhouses may be most appropriate for parcels abutting Franklinton Park, to the north and south, while high density 7-8 story street wall buildings on the Whole Foods site may more appropriately address abutting residents concerns about protecting views. Overall density should be in the order of 100 units per acre.

- Small footprint, tall buildings should be designed to enhance the skyline, particularly when viewed from the Art Museum steps. Carefully sited tall buildings should define the northern edge of the Parkway, while minimizing any impacts on historic Spring Garden Street, and the Parkway museum buildings.
- Existing high-rise residential apartment buildings have typically been isolated structures

in a park-like setting. New buildings should be oriented to the street and contribute to the streetscape.

- Buildings should be sited to minimize impacts on views from, and sunlight, and daylight access on existing residential buildings.
- Podium buildings should be designed to respond to the traditional rowhouse scale, with individual unit entrances, small landscaped front yards, etc. The Tivoli Condominiums are a good example of this treatment.
- Common open space should be provided in the form of roof decks, terraces, and open space above parking.
- Buildings fronting the 2000 block of Hamilton Street, and 1800 and 1900 blocks of Callowhill Street, should include first floor retail uses.

- Low/mid-rise residential developments, with individual townhouse entrances, with carefully located small footprint high-rise towers, should front Franklin Town Park, to encourage more active use of this underutilized recreation resource.
 - North-south streets at the Parkway crossings should be developed with freestanding small scale, single-story restaurant/café commercial uses as pavilions in a park setting, to encourage pedestrian activity, particularly in evening hours.
 - High-rise/mid-rise residential buildings should have a residential character, with bays, balconies, setbacks and other features to create a residential scale.
 - Developments should include supporting community amenities such as schools, and day care centers.
- B. Street Level Amenities
- Buildings should be setback from the street right-of-way a small distance (5-15'), to create an opportunity for landscaping, patios, etc. and thus contribute to the open, green, park-like setting of this area.
 - Impact of parking and loading access to apartment buildings should be minimized. Openings for parking access should be limited to one 22' wide opening, or two 11' wide openings for each development. Openings for loading access should be limited to one 15' wide opening, and preferable integrated with parking access.
 - Developments such as the Tivoli incorporate modest landscaped setbacks that help

Image 94. Mixed-Use Development in Seattle, WA that could be appropriate in Parkway North 





create an attractive residential scale. Small, approximately 5-foot deep, building setbacks at sidewalks should include landscaping treatments to reinforce the “green” character of the area. Landscaped areas should be protected by low railings.

- First floors of all developments should include pedestrian-oriented treatments, such as stoops, individual unit entries, and windows, to create an enjoyable pedestrian experience.



▲ *Image 95b. Daycare center in Vancouver, BC*

◀ *Image 95a. Townhouse and Tower Development in Vancouver, BC*

*Zone 4 – Vine Street North (Community College/
Hahnemann Hospital)*

Zone 4 north of Vine and east of 18th Street includes the large institutional developments of Community College and Hahnemann University. It also includes the Philadelphia School District offices in the former Philadelphia Inquirer Building and the office development of the former Smith Kline Building. There are a large number of structured parking facilities and surface parking lots. The zone includes large blocks, large footprint buildings with large areas of blank walls and few pedestrian amenities. The streetscape is generally bleak and traffic is heavy. The depressed Vine Expressway and 16th and Callowhill interchange ramps create a barrier to pedestrian activity between the north and south of Vine Street.

A. Building Design Guidelines

- Franklin Town parcels fronting Vine Street should be developed with high-rise, high-intensity mixed-use buildings to begin to draw pedestrian activity north of Vine Street and create a visual link to the high-intensity office core of Zone 1.

- The Vine Street Expressway air-rights parcels between 18th and Broad Streets should be developed for high-rise, high-intensity mixed-use buildings to further enhance the linkages north and south of the expressway.
- The air-rights parcels available at the Vine Street interchange should be developed for high-rise, high-density mixed-use.
- Encourage development of student housing for Pennsylvania Academy of Fine Arts students, possibly as a joint development with Drexel Hahnemann Hospital.
- New development should establish a “street wall” along both Vine Street and Franklin Town Boulevard, without creating the “canyon” effect of the Watermark high-rise.
- Expansion of Community College at appropriate locations should be encouraged. Potential sites would include the 400 block

of N. 18th Street fronting Franklin Town Park, and Vine Expressway air-rights parcels. Such a development on the 18th Street parcel could create an attractive “front door” for the campus, and screen the unsightly college parking structure fronting 18th Street.

B. Pedestrian Amenities

- Improvement of the streetscape is a key concern. Large areas of blank wall at ground level should be avoided. Any areas of blank wall should be articulated with differing building materials, artwork, landscaping, trellises, etc.
- Sidewalks on Vine Street should be a minimum 16’ wide, consistent with those at the Free Library and Family Court. Sidewalks on Franklin Town Boulevard should be 20’ wide to match the width of existing sidewalks north of Wood Street.
- All above-ground parking structures must be wrapped with active uses to create a more

inviting pedestrian environment.

- Encourage first floor retail/restaurants and other service uses to serve institutional employees and students.
- Minimize the impact of loading docks and parking access to 2 openings, maximum 11’ wide, or one 22’ wide opening. Provide one maximum 15’ wide loading access.
- The area lacks landscaping and trees. An aggressive program of street tree planting should be initiated throughout the area.
- Locate loading underground, or entirely enclosed with the building, to reduce street level impacts.
- Support the narrowing of the cartway and widening of the sidewalks on 1400 block of Cherry Street to create pedestrian-oriented street and opportunities for a sidewalk restaurant/café.

5.3.7 Possible Development Scenarios

The following scenarios were developed to illustrate how certain key parcels could be developed in conformance with the proposed zoning overlays, and design guidelines. The scenarios are not meant to provide a prescriptive design for the sites, but only to provide an illustrative example.

23rd and Arch Parcels

- Maximum FAR: 5.0
- Maintain Arch Street public access/view corridor to Schuylkill River, minimum 72' wide.
- Maximum Building Podium Height: 65'
- Maximum Tower Height Above 65': 300'
- Maximum Building Dimension, 65'-125' high: 250' diagonal.
- Maximum Tower Dimensions, 125'-300': 120' diagonal.
- Minimum building spacing, 65'-125' high: 60'.

- Minimum Tower spacing 125'-300' high: 100'
- Residential units with raised stoops and individual unit street front access, or retail fronting 23rd Street
- Residential units with raised stoops and individual unit street front access, or retail fronting Arch and Cherry Street.

20th and Hamilton Parcels

- Maximum FAR: 5.0.
- Maximum Residential Density: 130 units acre.
- Through building public access at railroad right-of-way.
- Retail uses at first floor fronting Hamilton Street.
- Retail uses at first floor on Pennsylvania Avenue.
- Residential uses with individual unit street front access fronting 20th Street.
- Alternate A: Street wall buildings, maximum

height at street frontage of 7-8 stories (90') fronting Pennsylvania Avenue and 20th Street (preferred by adjacent residents).

- Alternate B: Combination of low-rise 4-story buildings (maximum height of 50') fronting Pennsylvania Avenue and 20th Street. Small footprint high-rise residential apartment building at corner of 20th and Pennsylvania Avenue. (Maximum height, 300', maximum tower dimensions, 100' diagonal).

- Minimum Building Spacing, 50'-125' high: 100'.
- Minimum Tower Spacing: 100'.
- Maintain 1800 block of Hamilton Street as public access street between Franklin Town Park and the development.
- Residential units with individual street front access, fronting Hamilton and 19th Street.

18th and Hamilton Streets

- Maximum FAR: 5.0.
- Maximum Residential Density: 130 units acre.
- Maximum Podium Building Height: 50' (4 stories).
- Maximum Tower Height Above 50': 300'.
- Maximum Building Dimension, 50'-125' high: 250' diagonal
- Maximum Tower Dimensions, 125'-300' high: 120' diagonal



▲ *Image 96. Mixed Use Development in Vancouver, BC*



6

PRIORITIES AND NEXT STEPS

6.1 Priorities

Implementation of the plan will require time, coordination with a variety of private and public entities, and funding from a variety of sources. Because of this, it is important to consider a prioritization of the recommendations, in order to ensure that the most important recommendations are enacted.

At the final public meeting for the Logan Square Neighborhood - Parkway Plan, local stakeholders and residents were asked to prioritize the recommendations presented as part of the final plan. In general, the priorities selected by these citizens focused on creating a lively, safe, and engaging public realm for residents and visitors alike.

6.1.1 Quality of Life

The highest priority issue within the Quality of Life section is addressing the chronic street homeless population. The recommendations are listed in full in Section 5.1.3 above, but immediate priorities are to remove all homeless encampments from the neighborhood; cease outdoor food and other distributions to the homeless on the Parkway; and adequately enforce all laws and regulations in the neighborhood, especially along the Parkway and in all parks and plazas. (Residents are concerned, however, that encampments displaced from the Parkway not be permitted to simply relocate into the more residential areas of the neighborhood.) Any and all policies that contribute to the disproportionately large chronic street homeless population in this and adjacent downtown areas should be critically reevaluated and overhauled. These action items will not only enhance the feeling of safety within the neighborhood, but help to preserve the vitality of this downtown hub for culture, tourism, and business.

Other high priorities within Quality of Life include increasing the number of small cafes and providing additional pedestrian enhancements, in the form of benches and upgraded sidewalks along the Parkway. Both of these items will serve residents and visitors. Similarly, the Schuylkill River Trail will benefit from restrooms/water fountains for the active users of the area. The potential dock should also be explored as a priority for the trail.

In terms of streetscape priorities, stakeholders felt that improvements should be made to the JFK/ SEPTA track underpasses at 21st, 22nd and 23rd Streets, and that new or additional pedestrian lighting throughout the neighborhood is also important; this simple change can make the area a more attractive evening destination.

Additional pocket parks (similar to the Coxe Park model) should be installed in the Logan Square area as an amenity for families; stakeholders also prioritized requiring passive open space development from developers building within the neighborhood. Sustainable development practices (such as LEED certification) should be encouraged throughout the neighborhood, and additional tree canopy should

be added. Additional trees will not only provide additional shading to streets, but also help the carbon footprint of the community. The goal is to provide 500 trees within 3 years.

Also in terms of development, a review process should be established for new projects within the neighborhood's boundary; the team should include developers, public agencies, and residents. An LSNA member should also be appointed to the Parkway Foundation Board, to increase communication between organizations and ensure decision making consistency.

6.1.2 Transportation

The highest priority identified within circulation issues was to improve the intersections at 20th and Parkway/15th and Vine, and Broad and Vine. These are crucial to improving pedestrian and vehicular mobility through the neighborhood.

New parking facility north of Vine between 18th and 21st Streets to serve Parkway institutions should be established, while making temporary visitor permits available in other garages. Business parking issues

were not identified as a high priority by residents of the neighborhood, but new underground parking at Barnes Museum or Free Library should be established while those buildings are undergoing their planned construction and renovations. An easy parking fix would be to allow permit-parking holders to bypass roadblocks.

The top priority for pedestrian enhancements is creating 4-way stop at 22nd and Cherry Street; similarly, 22nd Street was targeted for bicycle enhancements, including the creation of a dedicated bike lane.

Coordination with SEPTA will be necessary, but a priority should be to target a 10 minute service target for peak times, and 15 minutes for off-peak. This will encourage public transit use, and possibly reduce auto traffic through the neighborhood. Another transit improvement that could help encourage transit ridership is to create a more welcoming transit gateway at the City Hall Station/Dilworth Plaza area. Bus information should be made available at all bus stops.

6.1.3 Development

The highest priority recommendations overall fall within the development section. These recommendation suggestions are focused on maintaining the existing character and feel of the neighborhood, while allowing for future development. First, the zoning classification should be changed from C4 (Commercial) zoning to RC4 (Residential/Commercial) on Arch Street west of 19th Street. In general terms, the excessive additional FAR provisions in C4 and RC4 zoning classifications should be reduced. Provisions should be put in place to Provide building bulk controls (height, spacing, and footprint dimensions) to assure daylight, sunlight penetration and protect views.

6.2 Implementation Matrix

The following tables summarize the top priority recommendations separated into three separate timelines; short term for within zero to two years, mid-term for within two to five years, and long-term for projects taking five or more years to implement. Based on feedback from the public workshop conducted in October, and the plan's Advisory Committee. The full results of the public workshop are included in the Appendix.

The Implementation Matrix takes the top 20 short term, 10 mid-term, and 5 long-term recommendations and provides guidance on responsible parties and possible funding sources. The list is not comprehensive and is intended to be a guide to the Logan Square Neighborhood - Parkway.

LSNA will continue to work with the Planning Commission to implement recommendations where there are currently differences of opinion between the Planning Commission and LSNA.

Short-Term (0-2 Years) Recommendations

		Responsible Parties	Potential Funding Sources
1.	Reduce excessive additional FAR provisions in C4 and RC4 zoning classifications. <i>(DEV)</i>	A	n/a
2.	Remove all homeless encampments, and enforce all laws and regulations. <i>(QoL)</i>	A, C, D, E, F, H, I	A, E, G, I, J, K
3.	Establish design review procedure, evaluated against design guidelines that incorporate community input, for all major developments. <i>(DEV)</i>	A, D	n/a
4.	Change C4 (Commercial) zoning to RC4 (Residential/Commercial) on Arch Street west of 19th Street. <i>(DEV)</i>	A	n/a
5.	Install pedestrian lighting throughout neighborhood. <i>(QoL)</i>	A, C, D, E	A, D, E, F, I, J, K
6.	Provide building bulk controls (height, spacing, and footprint dimensions) to assure daylight, sunlight penetration and protect views. <i>(DEV)</i>	A	n/a
7.	Cease outdoor food distribution. <i>(QoL)</i>	A, D, E, H, I	A, E, I, J, K
8.	Add LSNA member to Parkway Foundation Board. <i>(QoL)</i>	D, G, H	n/a
9.	Establish review process for projects that includes developers, public agencies, and residents. <i>(QoL)</i>	A, D	n/a
10.	4-way stop at 22nd and Cherry Street. <i>(TSP)</i>	A, B	A, B

 Recommendations where the Planning Commission has a different opinion from LSNA.

Implementation Key

A = City of Philadelphia
 B = PennDOT
 C = Private Developers
 D = LSNA
 E = Center City District
 F = Private Land Owners

G = Institutions
 H = Parkway Council
 I = Non-Profits
 J = State of Pennsylvania
 K = Federal Government
 L = SEPTA

Short-Term (0-2 Years) Recommendations

		Responsible Parties	Potential Funding Sources
11.	Improve JFK/SEPTA track underpasses at 21st, 22nd and 23rd Streets. <i>(QoL)</i>	A, B	A, I, J, K,
12.	Modify existing 125' height limit to permit taller, small footprint buildings, in appropriate locations, to protect views and light. <i>(DEV)</i>	A	n/a
13.	Make temporary visitor parking permits available. <i>(TSP)</i>	A, D, J	A, J
14.	Additional cafés along Parkway. <i>(QoL)</i>	A, C	C
15.	New sidewalks and benches on Parkway. <i>(QoL)</i>	A, B, D, E, H	A, B, E, I, J, K
16.	Off-peak service target of 15 minutes. <i>(TSP)</i>	L	L
17.	Peak service target of 10 minutes on bus routes. <i>(TSP)</i>	L	J, K, L
18.	Prohibit aboveground parking garages for accessory parking unless they are wrapped in active uses. <i>(DEV)</i>	A	n/a
19.	Advocate for use of City Branch Right-of-Way as high frequency transit corridor. <i>(TSP)</i>	A, D, H, L	J, K, L
20.	Require car share and bicycle facilities. <i>(QoL)</i>	A, C, D, H	C, I

Implementation Key

A = City of Philadelphia

B = PennDOT

C = Private Developers

D = LSNA

E = Center City District

F = Private Land Owners

G = Institutions

H = Parkway Council

I = Non-Profits

J = State of Pennsylvania

K = Federal Government

L = SEPTA

Mid-Term (2-5 Years) Recommendations

		Responsible Parties	Potential Funding Sources
1.	New underground parking garage at Barnes Museum or Free Library. <i>(TSP)</i>	C, G	C, G
2.	Limit events to signature events (Thanksgiving Park, July 4th, etc.) <i>(QoL)</i>	A, G, H, I	n/a
3.	Increase tree canopy (500 trees in 3 years) <i>(QoL)</i>	A, C, D, F, G, I	A, C, D, F, G, I, J
4.	New parking facility north of Vine between 18th and 21st Streets to serve Parkway institutions. <i>(TSP)</i>	C, G	C, G
5.	More pocket parks (like Coxe Park) <i>(QoL)</i>	A, D, I	A, D
6.	Bus status information system at all bus stop signs. <i>(TSP)</i>	E, L	J, K, L
7.	Make City Hall Station/Dilworth Plaza a welcoming transit gateway. <i>(TSP)</i>	A, B, E, J, K, L	A, J, K, L
8.	Minimize noise impacts of building mechanical equipment. <i>(QoL)</i>	A, C, F, G	F, G
9.	Add ramp/stair connections to/from JFK at 21st, 22nd, and 23rd Streets. <i>(TSP)</i>	A, B	A, B, J, K
10.	Consider adoption of Neighborhood Conservation District, (20th/23rd, Winter and Arch) <i>(DEV)</i>	A, D	n/a

Implementation Key

A = City of Philadelphia
 B = PennDOT
 C = Private Developers
 D = LSNA
 E = Center City District
 F = Private Land Owners

G = Institutions
 H = Parkway Council
 I = Non-Profits
 J = State of Pennsylvania
 K = Federal Government
 L = SEPTA

Long-Term (5+ Years) Recommendations

		Responsible Parties	Potential Funding Sources
1.	Improve intersections at 20th and Parkway/15th and Vine, and Broad and Vine. <i>(TSP)</i>	A, B	A, B, J, K
2.	Eliminate unneeded outer lanes on Parkway, to reduce pedestrian crossing distances. <i>(TSP)</i>	A, B	A, B, J, K
3.	Infill openings over Vine Expressway at Free Library. <i>(QoL)</i>	A, B	A, B, J, K
4.	Landscaped/widened replacement bridge crossings over the Vine Expressway. <i>(QoL)</i>	A, B	A, B, J, K
5.	Redesign Eakins Oval. <i>(TSP)</i>	A, B,	A, B, G, J, K

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APPENDIX

A.1 Resident Survey

LSN-PP Community Survey

1. What age are you?			
		Response Percent	Response Count
Less than 20 years old		1.7%	8
21 – 40 years old		34.9%	161
41 – 60 years old		29.7%	137
More than 60 years old		33.6%	155
answered question			461
skipped question			7

2. Are you a resident of the Logan Square neighborhood?			
		Response Percent	Response Count
Yes		82.1%	384
No		17.9%	84
answered question			468
skipped question			0

3. In what quadrant of the Logan Square neighborhood do you live?			
		Response Percent	Response Count
Northwest, between Spring Garden, the Parkway, 19th and the Schuykill		15.9%	60
Northeast, between Spring Garden, the Parkway, 19th and Broad		7.9%	30
Southwest, between the Parkway, Market, 19th and the Schuykill		69.3%	262
Southeast, between the Parkway, Market, 19th and Broad		7.1%	27
answered question			378
skipped question			90

4. In what type of building do you live?			
		Response Percent	Response Count
Rowhouse/townhouse		35.1%	128
Conversion of office or other building		7.1%	26
Purpose-built condominium/co-op/apartment		58.1%	212
If you live in a multi-family building, what is the building name/address?			154
answered question			365
skipped question			103

5. Are you an owner or a renter in Logan Square neighborhood?			
		Response Percent	Response Count
Owner		88.0%	330
Renter		12.3%	46
answered question			375
skipped question			93

6. How long have you lived here?			
		Response Percent	Response Count
0-2 years		23.9%	90
3-5 years		22.0%	83
6-10 years		21.5%	81
11+ years		32.9%	124
answered question			377
skipped question			91

7. How would you characterize your household?			
		Response Percent	Response Count
Household with children		14.3%	54
Single, no children		38.5%	145
Married or living with significant other, no children in household		43.5%	164
Other (please specify)		4.0%	15
answered question			377
skipped question			91

8. What is your household size?			
		Response Percent	Response Count
1 person		35.0%	132
2 people		49.3%	186
3 people		9.5%	36
4 people		5.3%	20
5 or more people		1.1%	4
answered question			377
skipped question			91

8. What is your household size?			
		Response Percent	Response Count
1 person		35.0%	132
2 people		49.3%	186
3 people		9.5%	36
4 people		5.3%	20
5 or more people		1.1%	4
answered question			377
skipped question			91

10. In what quadrant of the Logan Square neighborhood do you work?			
		Response Percent	Response Count
Northwest, between Spring Garden, the Parkway, 19th and the Schuylkill		10.7%	13
Northeast, between Spring Garden, the Parkway, 19th and Broad		7.4%	9
Southwest, between the Parkway, Market, 19th and the Schuylkill		32.2%	39
Southeast, between the Parkway, Market, 19th and Broad		49.6%	60
answered question			121
skipped question			347

11. If you choose to stay in the Logan Square neighborhood after work on occasion, for what reasons do you most often stay? (Check all that apply.)			
		Response Percent	Response Count
outdoor events, such as parades		40.0%	46
recreation, such as gym, bike riding, etc		55.7%	64
museum exhibits		67.8%	78
library		44.3%	51
dining		79.1%	91
shopping for food		57.4%	66
shopping for other retail items		40.9%	47
answered question			115
skipped question			353

12. Are you aware of the Logan Square Neighborhood Association?			
		Response Percent	Response Count
Yes		90.5%	417
No		9.5%	44
answered question			461
skipped question			7

13. Are you a member of the Logan Square Neighborhood Association?			
		Response Percent	Response Count
Yes		51.7%	209
No		48.3%	195
answered question			404
skipped question			64

14. Did you know that it has monthly meetings, a website, and newsletter?			
		Response Percent	Response Count
Yes		83.3%	345
No		16.7%	69
answered question			414
skipped question			54

15. Are you a business owner in the Logan Square neighborhood?			
		Response Percent	Response Count
Yes		5.5%	25
No		94.5%	433
answered question			458
skipped question			10

16. In what quadrant is your business located?			
	Response Percent	Response Count	
Northwest, between Spring Garden, the Parkway, 19th and the Schuylkill	6.3%	1	
Northeast, between Spring Garden, the Parkway, 19th and Broad	6.3%	1	
Southwest, between the Parkway, JFK Boulevard, 19th and the Schuylkill	62.5%	10	
Southeast, between the Parkway, JFK Boulevard, 19th and Broad	25.0%	4	
<i>answered question</i>		16	
<i>skipped question</i>		452	

17. How long (in years) have you owned a business in the Logan Square neighborhood?			
	Response Percent	Response Count	
		17	
<i>answered question</i>		17	
<i>skipped question</i>		451	

18. Is this your only location?			
	Response Percent	Response Count	
Yes (Skip to Question 6.)	68.8%	11	
No	31.3%	5	
<i>answered question</i>		16	
<i>skipped question</i>		452	

19. If No, how many other locations do you have?			
	Response Percent	Response Count	
		5	
<i>answered question</i>		5	
<i>skipped question</i>		463	

20. And where are they located?			
	Response Percent	Response Count	
		5	
<i>answered question</i>		5	
<i>skipped question</i>		463	

21. How many employees do you have?			
	Response Percent	Response Count	
Full-time	78.6%	11	
Part-time	50.0%	7	
<i>answered question</i>		14	
<i>skipped question</i>		454	

22. What are your hours of operation?			
	Response Percent	Response Count	
Sunday	40.0%	4	
Monday	80.0%	8	
Tuesday	80.0%	8	
Wednesday	80.0%	8	
Thursday	80.0%	8	
Friday	80.0%	8	
Saturday	30.0%	3	
<i>answered question</i>		10	
<i>skipped question</i>		458	

23. What are your busiest times of operation? Please specify hours			
	Response Percent	Response Count	
Sunday	66.7%	4	
Monday	66.7%	4	
Tuesday	33.3%	2	
Wednesday	33.3%	2	
Thursday	33.3%	2	
Friday	33.3%	2	
Saturday	33.3%	2	
<i>answered question</i>		6	
<i>skipped question</i>		462	

24. Please describe any unique characteristics of your customers.		Response Count
		10
	<i>answered question</i>	10
	<i>skipped question</i>	458

25. Please describe the age and gender characteristics of your customers.		Response Count
		10
	<i>answered question</i>	10
	<i>skipped question</i>	458

26. Do you own or lease your space?			
		Response Percent	Response Count
Own		61.5%	8
Lease		38.5%	5
	<i>answered question</i>		13
	<i>skipped question</i>		455

27. What have been your sales trends over the past year?			
		Response Percent	Response Count
Keeping with inflation		62.5%	5
Declining		0.0%	0
Growing		37.5%	3
	If growing or declining, to what do you attribute this change?		3
	<i>answered question</i>		8
	<i>skipped question</i>		460

28. Within the next 24 months, what do you plan to do?			
		Response Percent	Response Count
Expand		46.2%	6
Stay about the same		38.5%	5
Downsize or close business		15.4%	2
	If you are planning any changes, why?		6
	<i>answered question</i>		13
	<i>skipped question</i>		455

29. Have there been any recent changes in the area that are beneficial to your business?		Response Count
		11
	<i>answered question</i>	11
	<i>skipped question</i>	457

30. Are there any issues in the surrounding area that have a negative impact on your business?		Response Count
		11
	<i>answered question</i>	11
	<i>skipped question</i>	457

31. Is parking an issue for your business?			
		Response Percent	Response Count
Yes		50.0%	7
No		50.0%	7
	<i>answered question</i>		14
	<i>skipped question</i>		454

32. Is parking adequate for your customers?			
		Response Percent	Response Count
Yes		41.7%	5
No		58.3%	7
	<i>answered question</i>		12
	<i>skipped question</i>		456

33. Where do your customers park?		Response Count
		10
	<i>answered question</i>	10
	<i>skipped question</i>	458

34. Where do your employees park?		Response Count
		9
	<i>answered question</i>	9
	<i>skipped question</i>	459

35. What types of activities do you think would help improve your business?			
		Response Percent	Response Count
Physical improvements to neighborhood streets		53.8%	7
Improved maintenance/cleanliness		69.2%	9
Coordinated promotional activities by businesses		15.4%	2
More parking		53.8%	7
Accessibility to financing for your business		7.7%	1
Addition of businesses that would complement the services or products you supply		38.5%	5
Other (please specify)		46.2%	6
answered question			13
skipped question			455

36. What are the best features of your immediate neighborhood? (Choose the 3 best features.)			
		Response Percent	Response Count
Walkability		73.6%	306
Historic character		12.7%	53
Open space along the Parkway		25.2%	105
Access to Schuylkill River Trail		31.0%	129
Close-knit community		10.6%	44
Schools		1.9%	8
Playgrounds		1.9%	8
Proximity to work		26.2%	109
Proximity to museums		27.9%	116
Proximity to cultural events		20.9%	87
Convenience of public transportation		30.8%	128
Convenience of Center City retail		27.6%	115
Other (please specify)		6.0%	25
answered question			416
skipped question			52

37. What are the best features of the larger Logan Square community? (Check the 3 best features.)			
		Response Percent	Response Count
Walkability		57.2%	237
Historic character		17.6%	73
Open space along the Parkway		39.6%	164
Access to Schuylkill River Trail		30.2%	125
Close-knit community		5.1%	21
Schools		1.0%	4
Playgrounds		1.2%	5
Proximity to work		14.5%	60
Proximity to museums		47.8%	198
Proximity to cultural events		29.2%	121
Convenience of public transportation		18.6%	77
Convenience of Center City retail		29.5%	122
Other (please specify)		3.4%	14
answered question			414
skipped question			54

38. What are your primary concerns for your immediate neighborhood? (Choose 3 primary concerns.)			
		Response Percent	Response Count
New development pressures (e.g. River City, Barnes Tower, etc.)		44.2%	182
Inadequacy of open space		13.6%	56
Inadequacy of retail		38.8%	160
Parkway event management		11.7%	48
Number of homeless people		47.3%	195
Parking		27.7%	114
Traffic congestion		22.6%	93
Difficult pedestrian crossings		26.5%	109
Lack of easy access to river trails		6.1%	25
Streetscape issues, such as street trees, sidewalks, and pedestrian lighting		39.8%	164
Other (please specify)		13.1%	54
answered question			412
skipped question			56

39. What are your primary concerns for the larger Logan Square community (Choose 3 primary concerns.)			
		Response Percent	Response Count
New development pressures (e.g. River City, Barnes Tower, etc.)		43.7%	179
Inadequacy of open space		11.2%	46
Inadequacy of retail		37.8%	155
Parkway event management		14.1%	58
Number of homeless people		56.1%	230
Parking		22.4%	92
Traffic congestion		25.9%	106
Difficult pedestrian crossings		30.5%	125
Lack of easy access to river trails		3.9%	16
Streetscape issues, such as street trees, sidewalks, and pedestrian lighting		35.4%	145
Other (please specify)		8.5%	35
answered question			410
skipped question			58

40. What issues on your block concern you most? Briefly list items in less than 20 words.			
		Response Percent	Response Count
a.		100.0%	323
b.		81.4%	263
c.		64.1%	207
answered question			323
skipped question			145

41. What types of new development would you like to see in your immediate neighborhood? (Choose 3 characteristics.)			
		Response Percent	Response Count
Retail		51.1%	204
Grocery or food store		44.4%	177
Restaurants		58.4%	233
New office, residential, or commercial space (10 stories or less)		9.3%	37
New office, residential, or commercial space (11 stories or more)		9.8%	39
New office space mid rise (10 stories or less)		1.8%	7
New office space high rise (11 stories or more)		6.3%	25
New residential condominiums mid rise (10 stories or less)		7.3%	29
New residential condominiums high rise (11 stories or more)		8.5%	34
New townhouses		26.6%	106
Parking garages		16.0%	64
Community center		22.6%	90
Other (please specify)		12.0%	48
answered question			399
skipped question			69

42. What types of new development would you like to see in the larger Logan Square community? (Choose 3 characteristics.)			
		Response Percent	Response Count
Retail		54.1%	216
Grocery or food store		43.9%	175
Restaurants		56.6%	226
New office, residential, or commercial space (10 stories or less)		8.8%	35
New office, residential, or commercial space (11 stories or more)		14.5%	58
New office space mid rise (10 stories or less)		2.0%	8
New office space high rise (11 stories or more)		8.5%	34
New residential condominiums mid rise (10 stories or less)		8.8%	35
New residential condominiums high rise (11 stories or more)		11.0%	44
New townhouses		19.0%	76
Parking garages		19.3%	77
Community center		24.6%	98
Other (please specify)		10.0%	40
<i>answered question</i>			399
<i>skipped question</i>			69

43. Do you think the recent development in Logan Square is			
		Response Percent	Response Count
too much		24.7%	96
about right		49.1%	191
not enough?		26.2%	102
<i>answered question</i>			389
<i>skipped question</i>			79

44. If you have concerns over recent developments in the neighborhood, what are they? (Choose 3 primary concerns.)			
		Response Percent	Response Count
Design of new construction unfriendly to pedestrian environment		30.1%	116
Unsympathetic alterations to historic buildings		12.2%	47
Lack of parking		24.7%	95
Design of parking structure		18.7%	72
Inadequate sidewalks		9.9%	38
Inadequate lighting		23.4%	90
Height or size of the development incompatible with neighborhood		44.4%	171
Lack of landscaping		22.6%	87
Blocking of views		30.6%	118
Blocking of sunlight		27.0%	104
Traffic congestion		24.4%	94
Other (please specify)		9.1%	35
<i>answered question</i>			385
<i>skipped question</i>			83

45. Which recent developments do you like?			
		Response Percent	Response Count
a.		100.0%	238
b.		61.3%	146
c.		44.1%	105
<i>answered question</i>			238
<i>skipped question</i>			230

46. Which recent developments do you not like?		
	Response Percent	Response Count
a.	100.0%	201
b.	45.8%	92
c.	20.4%	41
answered question		201
skipped question		267

47. Do you believe there is enough park space in the neighborhood?		
	Response Percent	Response Count
Yes (Skip to Question 4.)	43.8%	172
No	56.2%	221
answered question		393
skipped question		75

48. If No, what type of open space is needed? (Choose 3 types.)		
	Response Percent	Response Count
Playgrounds/tot lots	33.6%	76
Lawns for informal games	20.8%	47
Playing fields	10.6%	24
Private parklets incorporated into new development	27.0%	61
Basketball courts	8.0%	18
Tennis courts	21.7%	49
Small parks with benches	52.2%	118
Dog parks	28.8%	65
Landscaped garden	48.7%	110
Skate parks	10.2%	23
Other (please specify)	9.3%	21
answered question		226
skipped question		242

49. Where would you locate new neighborhood open space?		
	Response Percent	Response Count
a.	100.0%	141
b.	48.9%	69
c.	22.0%	31
answered question		141
skipped question		327

50. Which parks do you think should be improved? (Choose up to 3.)		
	Response Percent	Response Count
Franklinton Park	21.1%	75
Dilworth Plaza at City Hall	45.1%	160
JFK Plaza/Tourist Hospitality Center ("Love Park")	38.3%	136
Logan Circle in front of Library and Family Court	36.1%	128
Science Park at 21st and Race Streets	17.7%	63
Calder Sculpture Garden at 22nd Street & the Parkway	24.8%	88
Eakin's Oval	22.0%	78
Schuykill River Park	22.3%	79
Three Sisters Park in front of the Cathedral	29.6%	105
Other (please specify)	6.2%	22
answered question		355
skipped question		113

51. Do you believe there are too few, too many, or the right amount of events on the Parkway?		
	Response Percent	Response Count
Too few	10.5%	40
Too many	35.1%	134
Just right	54.5%	208
answered question		382
skipped question		86

52. What event on the Parkway impact the Logan Square neighborhood the most?			
		Response Percent	Response Count
July 4th		61.8%	217
Unity Day		20.2%	71
Thanksgiving Day Parade		10.8%	38
Other (please specify)		7.1%	25
answered question			351
skipped question			117

53. Do you believe that your nearest commercial business and/or cultural institution is a "good neighbor?"			
		Response Percent	Response Count
Yes (Skip to Question 9.)		80.6%	300
No		19.4%	72
answered question			372
skipped question			96

54. If No, what is your biggest concern?			
		Response Percent	Response Count
Parking		29.6%	24
Traffic		9.9%	8
Noise		6.2%	5
Trash		27.2%	22
Other (please specify)		27.2%	22
answered question			81
skipped question			387

55. Are you a member of a neighborhood museum?			
		Response Percent	Response Count
Yes		60.3%	229
No		39.7%	151
answered question			380
skipped question			88

56. Do you think pedestrian lighting is adequate in the neighborhood?			
		Response Percent	Response Count
Yes (Skip to Question 12.)		43.3%	164
No		56.7%	215
answered question			379
skipped question			89

57. If No, where do you recommend lighting be improved?			
		Response Percent	Response Count
a.		100.0%	185
b.		53.0%	98
c.		27.6%	51
answered question			185
skipped question			283

58. Do you feel safe walking at night?			
		Response Percent	Response Count
Yes (Skip to Question 14.)		58.6%	222
No		41.4%	157
answered question			379
skipped question			89

59. If No, please list the places where you feel unsafe at night.			
		Response Percent	Response Count
a.		100.0%	147
b.		45.6%	67
c.		25.2%	37
answered question			147
skipped question			321

60. Do you have school-aged children?			
		Response Percent	Response Count
Yes		10.9%	42
No		89.1%	342
answered question			384
skipped question			84

61. If you have children who attend school in the neighborhood, which school do they attend?		Response Count
		20
answered question		20
skipped question		448

62. If you have children who do not attend school in the neighborhood, which school do they attend?		Response Count
		19
answered question		19
skipped question		449

63. Do you feel safe having your children walk to school?		Response Percent	Response Count
Yes (Skip to Question 5.)		38.7%	12
No		61.3%	19
answered question			31
skipped question			437

64. If No, why not?		Response Percent	Response Count
Speed of traffic		31.8%	7
Strangers		40.9%	9
School too far from home		18.2%	4
Child too young		54.5%	12
Other (please specify)		36.4%	8
answered question			22
skipped question			446

65. Do you have daycare-aged children?		Response Percent	Response Count
Yes		40.0%	16
No (Continue to next page.)		60.0%	24
answered question			40
skipped question			428

66. If Yes, do you feel daycare is adequate in this neighborhood?		Response Percent	Response Count
Yes		29.4%	5
No		70.6%	12
answered question			17
skipped question			451

67. What form of sidewalk behavior, if any, do you believe present the greatest problem for the greater Logan Square?		Response Percent	Response Count
Encampments on public property		32.9%	123
Encampments on private property		3.7%	14
Anti-social behavior (threatening behavior, aggressive panhandling, etc.)		28.6%	107
Sleeping on sidewalks		23.0%	86
None		3.7%	14
Other (please specify)		8.0%	30
answered question			374
skipped question			94

68. Do you believe that the number of homeless people/encampments in the larger Logan Square neighborhood (versus your quadrant thereof) presents a problem beyond humanitarian concerns?		Response Percent	Response Count
Yes		72.1%	271
No (Skip to Question 4.)		11.4%	43
Somewhat		16.5%	62
answered question			376
skipped question			92

69. If Yes or Somewhat, in which locations do you think the presence of homeless people/encampments presents the biggest problem?

	Response Percent	Response Count
The Parkway (including the Youth Study Center, Sister Cities Park in front of St. Peter's Basilica, the park around Swann Fountain, the area in front of the public library, Aviator Park in front of the Franklin Institute, the park in front of the ExceleRx building at 3 Parkway)	73.9%	243
Love Park	8.5%	28
Near the Vine Street Expressway	4.3%	14
Schuykill River Trail in the vicinity of Paine's Park (future skatepark)	3.6%	12
Other (please specify)	9.7%	32
<i>answered question</i>		329
<i>skipped question</i>		139

70. Do you believe that the Parkway is an appropriate venue for feeding programs?

	Response Percent	Response Count
Yes	14.1%	52
No	85.9%	316
<i>answered question</i>		368
<i>skipped question</i>		100

71. Do you believe that the number of homeless people/encampments in your quadrant of the neighborhood (versus the larger Logan Square neighborhood) presents a problem beyond humanitarian concerns?

	Response Percent	Response Count
Yes	58.1%	215
No (Skip to Question 7.)	28.1%	104
Somewhat	13.8%	51
<i>answered question</i>		370
<i>skipped question</i>		98

72. If Yes or Somewhat, in which location do you think the presence of homeless people/encampments presents the biggest problem?

	Response Percent	Response Count
22nd & Summer Streets	18.8%	49
Cuthbert Street (between JFK and Arch)	30.3%	79
Wawa at 21st and Hamilton Streets	27.2%	71
Other (please specify)	23.8%	62
<i>answered question</i>		261
<i>skipped question</i>		207

73. If you believe that the number of homeless people/encampments presents a problem beyond humanitarian concerns, what are the major issues you associate with the problem?

	Response Percent	Response Count
Safety/Crime	29.5%	102
Trash	10.4%	36
Visual impact on visitors and residents	33.8%	117
Monopolization of public spaces and amenities	16.2%	56
Other (please specify)	10.1%	35
<i>answered question</i>		346
<i>skipped question</i>		122

74. Are there any other quality of life issues that impact you? Please briefly list in 20 words or less.

	Response Count
	119
<i>answered question</i>	119
<i>skipped question</i>	349

75. What form of transportation do you most often use to commute to work or school? (Choose 1 that applies most. If you routinely combine modes other than walking, explain your commute pattern on the 'Other' line.)			
	Response Percent	Response Count	
Bus	13.9%	51	
Regional Rail	3.5%	13	
Subway	5.4%	20	
Trolley	1.6%	6	
Car – Drive alone	19.9%	73	
Car - Carpool	0.5%	2	
Bike	3.3%	12	
Walk	37.6%	138	
None, work at home	3.0%	11	
Other (please specify)	11.2%	41	
		answered question	367
		skipped question	101

76. Are you a member of Philly CarShare or FlexCar?			
	Response Percent	Response Count	
Yes	20.1%	75	
No	79.9%	299	
		answered question	374
		skipped question	94

77. Regarding public transit, what are your highest priorities for improvements? (Choose 3 highest priorities.)			
	Response Percent	Response Count	
Improved, cleaner, more attractive shelters at major bus stops	46.5%	154	
Improved information at stops	47.4%	157	
Improved frequency of service at rush hour	21.1%	70	
Improved frequency of service during the evening	32.0%	106	
Improved frequency of service on weekends	40.5%	134	
Faster service (express) to other locations in Center City	30.5%	101	
New routes to specific destinations. If chosen, please list destinations:	13.9%	46	
		answered question	331
		skipped question	137

78. Do you use public transit?			
	Response Percent	Response Count	
Yes (Skip to Question 6.)	76.1%	284	
No	23.9%	89	
		answered question	373
		skipped question	95

79. If No, why not?			
	Response Percent	Response Count	
Does not go where I need to go	25.3%	24	
Too slow	24.2%	23	
Does not run when I need it	5.3%	5	
Schedules are too infrequent	10.5%	10	
Too crowded	10.5%	10	
Unclean vehicles	20.0%	19	
Discourteous service	18.9%	18	
Too expensive	7.4%	7	
Information for schedules and routes not readily available	18.9%	18	
Other (please specify)	34.7%	33	
		answered question	95
		skipped question	373

80. Are there any particular intersections that are difficult to cross? (Choose 3 most difficult crossings.)			
	Response Percent	Response Count	
Spring Garden Street/Pennsylvania Avenue/23rd Street	18.9%	63	
15th/16th Streets at I-676 Ramps	12.9%	43	
Pennsylvania Avenue between 20th and 23rd Streets	12.9%	43	
22nd Street at I-676 ramps	25.8%	86	
The Parkway between 15th and 17th	25.2%	84	
Logan Circle and the Parkway between 18th and 20th	52.6%	175	
The Parkway between 21st and 22nd	38.7%	129	
Eakins Oval and the Parkway	40.5%	135	
Other (please specify)	18.0%	60	
		answered question	333
		skipped question	135

81. Do you own a car?			
		Response Percent	Response Count
Yes		70.7%	265
No		29.3%	110
answered question			375
skipped question			93

82. How many cars are in your household?			
		Response Percent	Response Count
0		0.0%	0
1		78.9%	210
2		20.3%	54
More than 2		0.8%	2
answered question			266
skipped question			202

83. Where do you park it?			
		Response Percent	Response Count
In your own garage on your property		40.4%	107
In a garage off of your property		6.4%	17
In your driveway or parking pad		12.1%	32
In a parking lot off of your property		8.3%	22
On street		23.0%	61
Other (please specify)		9.8%	26
answered question			265
skipped question			203

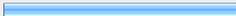
84. If you generally park on-street, how far from your home/destination do you usually park?			
		Response Percent	Response Count
0 to 2 blocks		75.0%	84
2 to 4 blocks		23.2%	26
4 to 6 blocks		0.9%	1
More than 6 blocks		0.9%	1
answered question			112
skipped question			356

85. If you park a car at a private facility, how much do you spend per month to park it?			
		Response Percent	Response Count
Under \$50		8.9%	10
\$51 - \$100		10.7%	12
\$101 - \$150		50.9%	57
\$151 - \$200		17.9%	20
\$201 - \$250		5.4%	6
More than \$250		6.3%	7
answered question			112
skipped question			356

86. Do you consider the train underpasses between 21st and 23rd Streets north of Market Street barriers between the central business district and the larger Logan Square neighborhood?			
		Response Percent	Response Count
Yes		77.4%	278
No (Skip to Question 3.)		22.6%	81
answered question			359
skipped question			109

87. If Yes, what improvements would you recommend to encourage movement through the underpasses?			
		Response Percent	Response Count
Better lighting		87.3%	247
Landscaping		58.3%	165
Cleaning and/or painting the structures		67.5%	191
Commissioning murals or other types of art installations for the abutments		24.7%	70
Other (please specify)		17.7%	50
answered question			283
skipped question			185

88. Do you consider the Parkway to be a barrier between the north and south parts of the Logan Square neighborhood?			
		Response Percent	Response Count
Yes		58.9%	218
No (Continue to next page.)		41.1%	152
answered question			370
skipped question			98

89. If Yes, what improvements would you recommend to improve the connection?			Response Percent	Response Count
Improved signals			64.7%	141
Better crosswalks			68.8%	150
Better signage			25.2%	55
Widening the median for pedestrians			48.6%	106
Widen the sidewalk at the corners (e.g. bumpouts)			34.4%	75
Other (please specify)			21.1%	46
			<i>answered question</i>	218
			<i>skipped question</i>	250

A.2 Public Workshop Priority Results

Table 5a. Quality of Life Community Priorities

		1st Priority	2nd Priority	3rd Priority	
Passive Open Space & Streetscape	Logan Square Parkway	4	1	1	Additional cafés along Parkway.
		3	2		New sidewalks and benches on Parkway.
		3		1	Infill openings over Vine Expressway at Free Library.
		2		1	Better landscape on north-south streets across Parkway.
		1			Landscaped/widened replacement bridge crossings over the Vine Expressway.
	Schuylkill River Park	2	1	1	Restroom/water fountain on River trail.
		2			Public dock on River between Arch and Vine.
		1		1	Bicycle/pedestrian link between Rivers Edge and Cherry Street east of rail tracks.
		1			Wayfinding signage from Art Museum to River trail.
					Bridge over rail tracks to River trail from 24 th Street near Parktowne Place.
	Dilworth Plaza	1			Rehabilitate plaza/improve connections to subway concourse.
					Reduce dense tree cover.
	Streetscape	5	7	2	Install pedestrian lighting throughout neighborhood.
		5	3		Improve JFK/SEPTA track underpasses at 21 st , 22 nd and 23 rd Streets.
		2			Improved shelter/transit information at bus stops.
		1	1		Enhance historic small streets (lighting, paving, trees)
		1			"Green" existing parking garages with "vertical gardens."
			1	1	Enhance neighborhood "gateways" (Vine Expressway to 22 nd and 23 rd Streets).
				1	"Green" Race Street between Convention Center and Schuylkill River.
					"Green" Cherry Street between Convention Center and Parkway.
Other Open Space	4		2	More pocket parks (like Coxe Park)	
	3			Require new development to provide passive open space	
	2	1	1	Provide neighborhood dog parks	
	1			Open Science Park for neighborhood children.	
Active Open Space	1			Community use of rooftop tennis courts at Community College.	
			2	Construct tennis courts (under the Expressway at 24th Street?)	
				Community use of Roman Catholic High playing fields on 24th Street.	
				Complete Paines Park skateboard venue.	
				Picnic grove on trail at Martin Luther King Drive.	
Homelessness	11	4		Remove all homeless encampments.	
	10			Cease outdoor food distribution.	
	2		1	Install public restrooms on Parkway.	
	2			Encourage businesses to employ formerly homeless.	
				Increase public support for addressing issue.	
				Inform community on proposals for homeless/social service facilities in neighborhood.	

Table 5b. Quality of Life Community Priorities

		1st Priority	2nd Priority	3rd Priority	
Education	title received one vote	1			Support appropriate expansion of charter and private schools.
		1			Support appropriate expansion of higher education facilities.
Sustainability		4	2	3	Increase tree canopy (500 trees in 3 years)
		3			Promote LEED for new developments.
		2	3		Require car share and bicycle facilities.
		2			Promote "green streets."
		1		1	Discourage private auto ownership/encourage transportation alternatives.
		1			Encourage alternate energy systems.
			2	1	Community gardens on neighborhood side of CSX tracks.
					Encourage "green roofs."
					Encourage rainwater capture (rain barrels).
					Promote recycling/composting.
Parkway Events					Require trash recycling for multi-family developments.
		7	4		Limit events to signature events (Thanksgiving Park, July 4th, etc.)
		2			Encourage alternatives to driving to events.
		2			Extend standard weekly parking time limits in residential areas to weekends/holidays during events.
		1			Create committee to evaluate number and type of events.
Light/Noise Pollution					Provide shuttles to off-site parking areas.
		4			Minimize noise impacts of building mechanical equipment.
					Minimize impacts of outside light sources on interior residential spaces.
Community Organization		8	1	1	Establish review process for projects that includes developers, public agencies, and residents.
		7	4		Add LSNA member to Parkway Foundation Board.
				1	Build coalition of support for initiatives.

Table 6a. Transportation Community Priorities

		1st Priority	2nd Priority	3rd Priority	
Circulation		7	1		Improve intersections at 20th and Parkway/15th and Vine, and Broad and Vine.
		2			Adjust signal timing at 22nd and Parkway, 16th and Spring Garden, and Broad and Callowhill.
		2			Traffic signal on 22nd Street at Vine Street ramps.
		1			Reestablish 18th Street as through street at Callowhill.
			1	1	Redesign traffic/pedestrian patterns at Eakins Oval.
					Convert Callowhill Street between Franklin Boulevard and 15th Street to 2-way.
					Convert Market Street and JFK Boulevard to 2-way traffic.
Parking	General	4	2	1	New parking facility north of Vine between 18th and 21st Streets to serve Parkway institutions.
		2		1	Replace unneeded traffic lanes with parking lanes.
		1			Remove unnecessary parking restrictions.
		1			Rehabilitate JFK Plaza garage.
					Encourage weekend/evening use of parking resources near Broad Street.
	Residential Parking	4	2		Make temporary visitor parking permits available.
		2			Prohibit garage front townhouses.
		1	1		Maintain existing minimum 0.7 space per unit parking ratio. Maximum 1.0 space per unit.
			4	1	Require 1 car share space per 50 units.
					Reduction of 4 required parking spaces for every 1 car share space.
					Reconfigure Pennsylvania Avenue to accommodate additional parking.
					<i>Added: 24 Hour Parking for Visitors; 3 votes at 1st priority</i>
	Business Parking				Increase on-street meter rates, and reduce short-term garage rates, (2005 Center City Parking Study).
					2-hr. meter parking on Callowhill Street for retail.
					Provide parking for Fed Ex, UPS, to minimize double parking.
		9	10	1	New underground parking at Barnes Museum or Free Library
		3			Eliminate surface parking at Eakins Oval.
		1			Parking lanes on Parkway for institutional uses. Better signage to parking facilities.
	Special Event Parking			1	Allow permit-parking holders to bypass roadblocks.
		1			Permit residential parking on event days.
					Extend permit-parking restrictions to weekends/holidays. Make use of Community College parking.

Table 6b. Transportation Community Priorities

	1st Priority	2nd Priority	3rd Priority	
Pedestrians	7	2		4-way stop at 22nd and Cherry Street.
	2	1	3	Add ramp/stair connections to/from JFK at 21st, 22nd, and 23rd Streets.
	2	1	2	Eliminate unneeded outer lanes on Parkway, to reduce pedestrian crossing distances.
	1			Calm traffic on Callowhill Street.
	1			Install pedestrian count down timers at all crossings with more than 3 traffic lanes.
	1			Redesign Eakins Oval.
	1			Tighten curb radii and add bulb outs at 21st and 22nd Streets and Parkway, to calm traffic.
				Eliminate free turn at 23rd Street and Vine Expressway.
				Eliminate free turn at 24th Street and Eakins Oval.
				Eliminate right angle parking at WaWa and restore sidewalk.
Bicycles	2		1	Complete bicycle lanes on 22nd Street.
	1		1	Relocate bike lanes to outer lanes of Parkway.
		1		Install bicycle lanes on 20th Street south of Parkway.
		1		Install bicycle rental/parking facility on Race Street at Schuylkill River Park, Free Library, and JFK Plaza.
				Establish Pennsylvania Avenue/Callowhill Street as bike route.
				Provide bike parking on Callowhill Street, Van Colln Field, and Spring Garden station.
Transit	3	2		Peak service target of 10 minutes on bus routes.
	3	2		Off-peak service target of 15 minutes.
		1		Encourage coordination of private shuttle routes and schedules.
				Replace 40-foot buses with 60-foot articulated buses.
Infrastructure Investment	4			Make City Hall Station/Dilworth Plaza a welcoming transit gateway.
	3	1	1	Advocate for use of City Branch Right-of-Way as high frequency transit corridor.
		1		Frequent, year round service linking tourist attractions.
Informational Improvements	2	1	5	Bus status information system at all bus stop signs.
				Install permanent signs showing Parkway event detour routes.

Table 7. Development Community Priorities

	1st Priority	2nd Priority	3rd Priority	
Neighborhood Conservation	2	2	1	Consider adoption of Neighborhood Conservation District, (20th/23rd, Winter and Arch)
Historic Resources	2		1	Complete survey of historic resources for Local Register.
	1			Encourage use of Transfer of Development Rights for historic preservation
Zoning Map	9	4	1	Change C4 (Commercial) zoning to RC4 (Residential/Commercial) on Arch Street west of 19th Street.
	1			Update zoning map to eliminate obsolete industrial.
			2	Rezone parcels along Broad Street corridor for C5 higher intensity commercial zoning
			1	Rezone 1900 and 2000 blocks of Spring Garden Street from R15 to R10, to reflect historic district.
				Rezone parks (Franklintown Park, JFK Plaza) for recreation.
Specific Zoning District Controls	17	4	2	Reduce excessive additional FAR provisions in C4 and RC4 zoning classifications.
	6	6	1	Provide building bulk controls (height, spacing, and footprint dimensions) to assure daylight, sunlight penetration and protect views.
	4	4		Modify existing 125' height limit to permit taller, small footprint buildings, in appropriate locations, to protect views and light.
	2	2	3	Prohibit aboveground parking garages for accessory parking unless they are wrapped in active uses.
	1		1	Require that all new residential developments provide open space at minimum 40 sq.ft. per unit.
			1	Prohibit parking garages as a main use west of 16th Street.
				Eliminate inappropriate FAR bonuses for building setbacks in R15 zone.
Design Review for Major Developments	11	1	3	Establish design review procedure, evaluated against design guidelines that incorporate community input, for all major developments.